

Memo

To: Springboro Residents & Other Interested Parties

From: Dan Boron, Staff Liaison Springboro Planning Commission

Date: April 7, 2021

Re: Instructions for Wednesday, April 14, 2021 Planning Commission Work Session

The Wednesday, April 14, 2021 Planning Commission work session will be conducted remotely by teleconference using Zoom. The meeting will begin at 6:00 p.m., however the virtual meeting room will be available for you to join beginning at 5:45 p.m. Wednesday night.

- 1) Beginning at 5:45 p.m. log on to www.zoom.us. You do this by going to www.zoom.us.
- 2) Once you are on the Zoom website, click "Join a Meeting" at the top right hand corner, which will bring you to the "Meeting ID" prompt. Enter Meeting ID and click button provided below.
- 3) At this point, it may prompt you to download the Zoom app (if you haven't used Zoom on that laptop / computer before). Download the app and click the .exe file in the bottom left hand corner once it is downloaded.
- 4) Then it may say "Open Zoom". Click "Open Zoom."
- 5) Then it should ask you for a meeting password. Enter meeting password and click Join Meeting.
- 6) Make sure at some point you click the "Join with Video" button. The meeting host will then accept you into the meeting. This may take a few seconds, so don't worry if you aren't joined right away.

City of Springboro is inviting you to a scheduled Zoom meeting.

Topic: Springboro Planning Commission Meeting

Time: Apr 14, 2021 05:45 PM Eastern Time (US and Canada)

Join Zoom Meeting

<https://us02web.zoom.us/j/85144969719?pwd=QXJyVXRnMHVpcEU3VGNEK1FGODVqZz09>

Meeting ID: **851 4496 9719**

Passcode: **052770**

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+1 346 248 7799 US (Houston)

+1 669 900 9128 US (San Jose)

Meeting ID: **851 4496 9719**

Passcode: **052770**

Find your local number: <https://us02web.zoom.us/j/kcxj51t8MI>

Agenda
City of Springboro Planning Commission Meeting
Wednesday, April 14, 2021, 6:00 p.m.

PUBLIC NOTICE:

Per Ohio Substitute House Bill 404 made effective November 22, 2020 in response to the COVID-19 state of emergency (Sec. 12), Section 12(A), the Springboro Planning Commission will conduct its Wednesday, April 14, 2021 Meeting via video conference at 6:00 p.m. EDT. Visit the City of Springboro website at <https://www.cityofspringboro.com/CivicAlerts.aspx?CID=6,1> for a link to connect to the meeting.

- I. Call to Order
- II. Approval of Minutes
 - A. March 10, 2021 Planning Commission Meeting
- III. Agenda Items
 - A. Preliminary Review, Site Plan Review, 285 South Pioneer Boulevard, The Tooling Zone, building addition
 - B. Preliminary Review, Rezoning, Easton Farm, 605 North Main Street, from R-1, Estate-Type Residential District, to PUD-MU, Planned Unit Development-Mixed Use, retail and residential development
 - C. Preliminary Review, General Plan, Easton Farm, 605 North Main Street, from R-1, Estate-Type Residential District, to PUD-MU, Planned Unit Development-Mixed Use, retail and residential development
- IV. Guest Comments
- V. Planning Commission and Staff Comments
- VI. Adjournment

City of Springboro
320 West Central Avenue, Springboro, Ohio 45066
Planning Commission Meeting
Wednesday, March 10, 2021

I. Call to Order

Chairperson Becky Iverson called the Springboro Planning Commission Meeting to order at 6:00 p.m. by video conference.

Present: Becky Iverson, Chair, Chris Pearson, Vice-Chair, Mark Davis, Robert Dimmitt, Steve Harding, Mike Thompson, and John Sillies.

Staff: Chris Pozzuto, City Manager; Dan Boron, City Planner; Elmer Dudas, Development Director; Chad Dixon, City Engineer, Ann Burns, Planning Commission Secretary.

Also present were Larry Dillin, Dillin Corp., Doug Borrer, Borrer Development, Wendy Moeller with Compass Point Planning, and Robin Hall, representing the Hall family.

II. Approval of Minutes

A. February 10, 2021 Planning Commission Minutes

Ms. Iverson asked for corrections or additions to the minutes.

There were none.

Mr. Harding motioned to approve the February 10, 2021 Planning Commission minutes as submitted. Mr. Sillies seconded the motion.

Vote: Harding, yes; Davis, yes; Sillies, yes; Dimmitt, yes; Iverson, yes; Pearson, yes; Thompson, yes (7-0)

Ms. Iverson took this time to explain the process and the order of speakers for the meeting. She reported that 19 e-mails were submitted to share comments and 4 citizens requested to speak. The staff will provide their report first, then the applicant, next the other speakers and lastly, the planning commission members.

III. Agenda Items

A. Final Approval, Planning & Zoning Code Text Amendment, sign code

Mr. Boron reported that this agenda item was reviewed at length at the February meeting where Wendy Moeller from Compass Point Planning provided her presentation. Staff is requesting formal approval to forward this text amendment to council. Mr. Boron explained that this amendment is to incorporate changes to the code due to a recent U.S. Supreme Court decision that impacts how signs are regulated. He briefly reviewed the amendments and the

one major change that was discussed, mostly related to the flag and flag pole restrictions, which are reflected on page 6 of the draft ordinance.

Ms. Moeller had nothing to add.

Ms. Iverson asked for a motion for approval Planning & Zoning Code Text Amendment, Sign Code.

Mr. Thompson motioned to approve. Mr. Harding seconded the motion.

Vote: Davis, yes; Sillies, yes; Dimmitt, yes; Iverson, yes; Pearson, yes; Thompson, yes; Harding, yes; (7-0)

B. Preliminary Review

Rezoning, Easton Farm, 605 North Main Street, from R-1, Estate-Type Residential District, to PUD-MU, Planned Unit Development-Mixed Use, retail and residential development

C. General Plan, Easton Farm, 605 North Main Street, from R-1, Estate-Type Residential District, to PUD-MU, Planned Unit Development-Mixed Use, retail and residential development

Background Information

These agenda items are based on a request filed by Easton Farm Partners, Springboro, seeking rezoning and general plan approval for the Easton Farm, 103.31-acre located at 605 North Main Street. The applicant is requesting rezoning and general plan approval under the City's Planned Unit Development (PUD) process from R-1, Estate-Type Residential District, to PUD-MU, Planned Unit Development-Mixed Use. The applicant proposes to develop a mix of commercial, single-family and multi-family residential development. While included in the PUD-MU rezoning, the applicant proposes to retain the 16.82-acre historic farmstead located on the west side of the property.

The proposed rezoning/general plan appears as two separate items on the Planning Commission work session agenda. The first stage of the PUD process, rezoning and general plan review and approval, will involve two separate recommendations to City Council, and later two separate pieces of legislation considered by City Council.

The subject property is located southwest of the intersection of Anna Drive/Little-Five Points Road and North Main Street. The subject property is presently farmed and includes two single-family residential units on the west side of the property within a historic farmstead. Vehicular access is presently provided by a single driveway from North Main Street.

The subject property is presently zoned R-1, Estate-Type Residential District. The R-1 District allows residential development at a density of 2 dwelling units per acre on 20,000 square foot lots. The R-1 District was applied to this property in 2015 as part of the implementation of the current Planning & Zoning Code.

The applicant has requested rezoning to PUD-MU, Planned Unit Development-Mixed Use, with three components: mixed-use, multi-family, and residential indicated on sheet C1.0 in the submitted materials.

Rezoning together with general plan review and approval are the first step in the three-step PUD review and approval process. Approval by both Planning Commission and City Council are required. Final development plan, similar to the City's site plan review process, review and approval by Planning Commission is the second stage in the process. Final development plan may be submitted in a number of sections in conjunction with a site's incremental development. Record plan review and approval by both Planning Commission and City Council is the last step in the PUD approval process, this allowing for the subdivision of lots and the dedication of right-of-way and open spaces. As with final development plans, record plans may be submitted in a number of sections as the development is completed.

Adjacent land uses include single-family residential development to the northwest within the Hunter Springs subdivision that includes homes on Deer Trail Drive. Open space in the form of the City of Springboro's Gardner Park, office and retail development to the north within the Village Park PUD-MU, Planned Unit Development-Mixed Use, retail development to the northeast within the Marketplace of Settlers Walk shopping center, a part of the Settlers Walk PUD, northeast of the intersection of Lytle-Five Points Road and North Main Street; retail and office development to the east on the east side of North Main Street; and retail and office development to the south including a day care facility and real estate office. To the south, residential development including condominiums within Springbrook Commons/Spice Rack subdivision, and the City of Springboro's North Park. To the west is single-family residential within the Tamarack Hills and Royal Tamarack subdivisions.

Adjacent zoning includes to the north R-2, Low-Density Residential District corresponding to the Hunter Springs subdivision, and PUD-MU corresponding to the Village Park development. PUD to the northwest associated with the Settlers Walk PUD. LBD, Local Business District, O, Office District, and O-R, Office-Residential District, to the east associated with the existing pattern of retail and office development. O-R District to the south, and transitioning to PUD and R-3, Medium-Density Residential District, associated to the condominium development to the south, and then transitioning to R-2 District corresponding to the single-family area along Tamarack Trail and into North Park. This R-2 District pattern continues to the west and the Tamarack Hills/Royal Tamarack subdivisions.

The *Springboro Land Use Plan*, adopted by City Council in April 2009, includes recommendations for the long-range development of the community.

It is divided into 16 policy areas that make specific recommendations for smaller portions of the community and are grouped together because of proximity, land use patterns, date of development and other general characteristics. Policy Area #3, North SR 741 Corridor, includes the subject area and land including Hunter Springs, Village Park, the non-residential portions of Settlers Walk and retail/office areas on east side of North Main Street. Preferred Land Uses identified in the plan include convenience retail, personal service, retail uses limited to a maximum of 75,000 square feet in floor area, among other uses. Residential development is preferred at an overall density of 6-8 dwelling units per acre.

The applicant's General Plan concept drawing proposes the following:

- An 18.75-acre mixed use commercial component on the northeast corner of the property fronting North Main Street. This component includes the following:
 - A 113-unit independent living facility.

- An 84,400-square foot assisted living/memory care facility.
- Outparcels for a fire station, restaurants and other retail facilities totaling 16,800 square feet.
- Two commercial buildings including 37,900 square feet of space.
- A 10.12-acre multi-family residential component on the southeast corner of the property fronting North Main Street that includes multiple buildings including 324 apartments, a 9,500-square foot restaurant, and 3.0 acres of open space comprised of storm water detention ponds.
- A 74.40-acre residential component covering the remainder of the property including the following:
 - Retaining the historic farmstead including 2 homes and preserving most farm buildings.
 - 24 townhomes.
 - 251 single-family lots most of that are served by garages accessed by private drives. The site of lots proposed for this large area ranges from large lots adjacent to the Hunter Springs neighborhood on the north end of the component to smaller lots to the south.
 - 12.82 acres of open space including two small parks, storm water detention ponds, a linear park, and a town green-type open space abutting the mixed use and multi-family residential component.

For proposed residential development areas, a gross density of 6.83 dwelling units per acre (577 dwelling units on 84.52 acres) is proposed.

Access to the proposed development would be provided by an extension of the existing Anna Drive through the development south to Tamarack Trail near the entrance to North Park, an extension of Fox Trail Drive from the Hunter Springs subdivision south into the interior of the site, and an access point onto North Main Street from the proposed Easton Farm Boulevard.

Staff Comments

City staff has the following comments regarding the proposed rezoning/general plan application:

1. Rename the mixed-use component of the PUD to commercial or other to avoid confusion with the overall rezoning request, and provide a component to address historic farmstead design/development standards and proposed permitted uses.
2. Revise full-color illustrative plan to match b/w plan proposal.
3. Revise the submittal for the next review to include the following for each component area: design and development standards including but not limited to setbacks, building heights, dwelling unit sizes, lot coverage, and a list of land uses proposed for each component area. As a companion to this information, provide a color-coded version of the general plan.
4. Easton farmstead is listed as open space. Unless the farm is available for use as common space it should not be included in the calculation.
5. An additional 5.47 acres of open space is needed to satisfy the 25% minimum open space requirement for residential PUDs.
6. Indicate who will manage open spaces proposed in the development.
7. For trails proposed on common areas, if any, include no restrictions for their use by any person with the exception of areas specifically set aside for the members of an association such as pool areas.

8. The trail along Anna Drive to be designed to meet minimum standards for width, turning radius, and to avoid obstructions.
9. City to review Anna Drive extension to intersection with Tamarack Trail and North Park entrance.
10. Indicate proposed phasing including road connections and other improvements with surrounding developments.
11. Provide typical plans for buildings indicating materials, and other details for each component in order to determine concurrence between general plan's concept and specific building plans when final development plans are prepared for review and approval by the Planning Commission.
12. Sidewalks (or trail) to be located on all streets, both sides. This should be verified. The general plan is difficult to tell and a statement would address it.
13. Connect Alley 4 and Alley 8, provided it can be engineered due to elevations.
14. Central mailbox units need to be located on general plan (and approved by the post office). Include this information in the design guidelines.
15. Flag lots not permitted (lots off of cul-de-sac on Red Hawk View). Remove flag lots or extend roadway.
16. Road name proposals to be reviewed by City Engineer in consultation with the police and fire departments.
17. Anna Drive to extend off of existing Anna Drive, and not relocated as shown.
18. Provide Noel Drive typical section.
19. Add a possible drop right turn lane into North Park at the Tamarack Trail connection.
20. Traffic Study to be submitted for review and approved prior to final approval of General Plan/Rezone by planning commission.
21. Right-of-way along North Main Street to be dedicated per city specifications.
22. No construction access permitted from Tamarack Trail or Fox Trail Drive.
23. Engineering design details to be reviewed at the Final Development Plans stage, including but not limited to utility design, storm water management plan including detention/retention design, and roadway design.
24. Road intersections to be at 90 degree angles.
25. HOA documents need to be created for review.
26. All private alleys to have a public access easement description.
27. The Clearcreek Fire District has no comments at this time.

City staff has the following comments regarding the proposed design guidelines booklet included in the rezoning/general plan submission:

1. City staff recommends a review with applicants on the intent of the design guidelines. Are they covenants or are these intended to be incorporated into the general plan approval?
2. Remove references to ARC in design guidelines.
3. Change references to occupancies to permitted uses.
4. Beginning on page 7, define personal care services.
5. Page 7, (p) states no upper floor level use restrictions within mixed-use areas. This needs to be defined.
6. Page 9, explain reference to architectural guidelines.
7. Page 10-11, utility easements are to be located outside of the right-of-way in a 10-foot utility easement, and not within the proposed right-of-way. (page 10, (c); page 11 (e)).
8. Page 13, for loading/unloading provisions, cross-reference City code on hours of operation.
9. Page 14, explain proposed open space area requirements. Are these setbacks?
10. Beginning on page 15, there are many references to traditional design. Is this concept tied down to examples?
11. Page 18, (h) tie-down proposed building heights.
12. For landscaping provisions on page 19, provide cross reference to City requirements in Chapter 1280, Landscaping.
13. Page 22, for exterior lighting, provide cross reference to City requirements in Chapter 1273, Exterior Lighting.
14. Page 30, explain accessory structure uses.
15. Page 31, single-family lots table states 4 story permitted – is this correct? In same table, footnote 1 states front porch encroachment up to 5 feet maximum is permitted. This should be removed and the table should reflect actual need/want.
16. Page 31, define where Village Center, Neighborhood Lane, etc., are in this proposal.
17. Page 31-32, state no parking in alleys permitted (on page 31 (b) and Page 32, 9(a)).
18. Page 32, are parking spaces defined as in a garage?
19. Page 33, are antennas permitted? (page 33, 11 (b)).
20. Page 34, explain Residential Typologies beginning on this page. Are these going to be supported by other design metrics?
21. Parking Dimensions to match city code. Change Stall Length on 0 degree parking from 22 feet to 24 feet.

Discussion:

Mr. Boron provided some background information for the Planning Commission's approval process for Planned Unit Developments (PUD). He referred to the development plan that was proposed back in 2017 from a different development team. This is a three step process that involves rezoning, final development plan, and then the record plan. Mr. Boron noted that the meeting tonight is for preliminary review and there will be no action taken.

Mr. Boron introduced Ms. Robin Hall who will be reading a statement on behalf of the Easton Family Trust.

Mr. Boron referred to the vicinity map that the area includes the entire property, all 108 acres, which does differ slightly from the proposal in 2017, which was around 85 acres. This is a Planned Unit Development-Mixed Use (PUD-MU) project which would be an amendment to our Planning and Zoning Code. The proposal includes a mixed use area, a multi-family area as well as a single family area.

Mr. Larry Dillin from the applicant team stated that their team has been speaking with the Hall family since late 2017 on the development of this plan and it reflects the wishes of the family. He explained how this site was modeled after two other existing sites in other parts of the state.

Mr. Doug Borrer provided some background information on his company which is a construction management and development company that specializes in mixed-use buildings in Central Ohio. He shared the artist rendering emphasizing the entrance from S.R. 741 and the statement that it creates.

Mr. Dillin reviewed their goal to create an environment that promotes walkability. He explained how the commercial core is connected to the residential sections as well as the central park on the inside of the development. He also shared plans for extending the multiuse trail, connections to both Gardner Park and North Park by way of a linear park.

Mr. Borrer reviewed how they incorporated other concepts from around the world where mixed use and residential can be development together to accomplish the goals of the Hall family. Two similar examples of these developments are Norton Commons in Louisville Kentucky and Baldwin Park in Orlando, Florida.

Mr. Borrer explained how they are creating a unique destination for the Springboro Community by developing prominent and inviting neighborhoods with a blend of urban and suburban neighborhoods. He reviewed how the development will incorporate a variety of heights, materials and textures to create an upscale presence. Mr. Borrer shared plans for the upscale apartments, the two story parking and the amenity packages that are designed to attract young professions to this area. He also reviewed the different types of residential living which include townhouses, urban lots, alley fed lots, edge lots and estate lots. All of these types of homes will follow the strict standards of the PUD.

Mr. Dillin noted that the residential sections are uniquely designed to cover all aspects as well as minimize curb cuts. He also reviewed the plans and amenities for the independent living building which is designed to accommodate the unmet needs of the 75 and older population that is still mobile and active. Mr. Dillin also reviewed the plans and locations of other restaurants and commercial sites within the development, which will include outdoor dining. He explained how the design focuses on connectivity between buildings which will encourage walkability between residential, multi-family and commercial space. Mr. Dillin stated they have worked to design a unique and special development while being respectful of the wishes of the Hall family.

Ms. Iverson thanked them for the presentation. She then welcomed the 3 other speakers who have registered to share their statements.

Justin Weidle of 164 Deer Trail Drive, read his prepared statement which shared his opposition to the development and voiced his concerns about density, greenspace and the insufficient revenue that will be generated. His statement is provided as part of this record.

David Beckman, 168 Deer Trail Drive, read his prepared statement which shared his concerns about overcrowding schools, overcrowding existing parks, increased traffic and the additional burden on police and fire services. His statement is provided as part of this record.

Steve Smith, 30 Jacamar Court, read his prepared statement in opposition of the development voicing concerns about increased traffic, noise, effects on wildlife and the return on investment. His statement is provided as part of this record.

Ms. Iverson thanked these residents for their comments which will be entered into the record.

Mr. Boron provided some background on the land use plan and its relationship to existing zoning on the site, which is currently zoned R-1, Rural Residential District. He explained that in the state of Ohio, and the City, there is no requirement that our land use plan be specifically matched up with the zoning of the property. Around 2015, the City also eliminated the agricultural zoning category since the community was no longer mostly agrarian, and is now a more suburban community with a large industrial base on the west end of town. Mr. Boron noted that the 2017 plan presented by Hills was not rejected, it was withdrawn by Hills Development before the Planning Commission had the opportunity to review it. The staff recommendation was to deny it, but the Planning Commission never had the opportunity to vote on it.

Mr. Harding thanked all the residents for their comments as well as the staff and developer for all of their input. He asked Mr. Dillin to provide some additional information on the school issue, and what the estimated average age of children will be in these type of residential units.

Mr. Borror explained that this semi-urban design is similar to another development near downtown Columbus. In this development, under half have any children at all, and there are no school age children. In the suburban apartments, there are less than 10% of children. In the unit mixes that are 2 bedrooms, we estimate a minimum amount of school age children. In the single-family townhouses, there are likely to have young couples with no children. Finally, in the single family homes, these tend to attract the urban professionals not likely to have children. Mr. Borror stated that equals about 45% of the homes that will likely not have many children. He stated that they would expect to see some children within the 95 larger single family lots. Mr. Borror noted that this will be significantly less children than if they were all standard single family homes and the development was created with sensitivity to the impact on schools.

Mr. Harding noted that the majority of these children would likely go to Dennis Elementary, and he assumes the building would be gradual.

Mr. Borror stated that the multi-family units would be completed in two phases, beginning in summer, 2022, with an absorption of 50 units annually and completion timeline of 6½ years.

Mr. Thompson asked if this was the same acreage as the proposal submitted in 2017.

Mr. Boron explained that the acreage of the heritage farm was excluded in 2017 so it was roughly 86 acres and the full acreage of the property is 103. It is staff's understanding that it is included, and the family may have plans to change the use to include some commercial use.

Mr. Sillies asked why the proposal was withdrawn in 2017, and what changed on this current proposal that make the City more receptive.

Mr. Boron stated that after 5 meetings, over a 10 month period of time Hills Development withdrew after staff recommendations regarding density relative to Deer Trail and acreage being consistent with Hunter Springs.

Mr. Boron explained that with the current preliminary review, staff has had the opportunity to review at length, and we did not see this level of detail back in 2017. There will like be changes based on comments and feedback from both the public and members of Planning Commission.

Ms. Iverson agreed that Hills Development did not provide this level of detail or take into consideration the input from residents or the Planning Commission. This plan also seems to be taking into consideration the need to stay in line with the existing Deer Trail area.

Mr. Sillies asked if the 3 and 4 story apartment buildings are necessary to make this project cost justifiable for both the developer and land owners.

Mr. Borrer explained that the goal is to create an up-scale experience that is different than anything in Springboro and create a destination where residents can be involved in the neighborhood. He reviewed the details, security and amenities that come with these buildings, which will be located on the lowest point of the property.

Mr. Sillies asked if they were needed to make the project cost justified and would the project move forward without the apartments.

Mr. Dillin explained that all the scenarios, market analysis and estimates include the multi-family apartments which also allows the multi-family use on smaller acreage.

Mr. Harding also asked if the project can move forward without the apartments, or if they could be reduced to 3 story rather than 4.

Mr. Dillin explained that economically, they would not move forward without the apartments and feel they could not reach the quality and character that they proposed.

Ms. Iverson also asked if there could be a consideration to lower the building to 3 stories, and what would that impact be.

Mr. Dillin stated they could review the possibility of changing to a 3 story, but would likely result in less amenities.

Mr. Borrer agreed that it would take away from much of the luxury of the project, the demographics would change and could also eliminate the ability to build the parking garage. They feel the market is there to support the 4 story building and the project would not be as appealing and the end result would not be the same.

Mr. Dillin stated he would likely need to pull the application if it is changed to a more suburban style product.

Mr. Thompson stated the entrance is intended to catch your eye, and he did not feel one story either way will make much of a difference.

Ms. Iverson stated there has been much consideration and surveys, and this seems to be the best location to promote a complex of this size with this level of amenities.

Mr. Pearson also shared concerns about a 4 story building.

There was further discussion on the options and the pros and cons of changing the plans for the multi-family apartment building.

Mr. Davis asked for clarification on the process, and if the rezoning and the development need to move forward together, or can staff focus on the re-zoning separately.

Mr. Boron explained that the rezoning and the general plan would need to move forward together, and ultimately be forwarded to Council for legislation. There would be an ordinance for the re-zoning and a resolution for the general plan.

Mr. Boron thanked everyone for their comments and participation. There will be additional discussion on the months to come, and the next meeting will be April 14, 2021. Staff will be reviewing all the comments and issues that were raised today. The next deadline for submittal is Friday, March 29th, so staff will know then if anything is submitted for further review.

IV. Guest Comments

There were no guest comments. Ms. Iverson encouraged citizens to submit comments in advance through e-mail.

V. Planning Commission and Staff Comments

Mr. Boron noted that the Master Plan Update is still in process and the surveys can be submitted through March 28th.

Adjournment

*Ms. Sillies motioned to adjourn the March 10, 2021 Planning Commission Regular Meeting at 8:05 p.m.
Mr. Thompson seconded the motion.*

Vote: Sillies, yes; Dimmitt, yes; Iverson, yes; Pearson, yes; Harding, yes; Thompson, yes; Davis, yes. (7-0)

Becky Iverson, Planning Commission Chairperson

Dan Boron, Planning Consultant

Ann Burns, Planning Commission Secretary

Ann Burns

From: Justin Wiedle <justin.wiedle@gmail.com>
Sent: Tuesday, March 09, 2021 12:17 PM
To: Ann Burns
Cc: Dan Boron; D B
Subject: Re: 2021 Easton Farm Plan Written Submission - Wiedle
Attachments: 1 Easton Farm - Letter to Planning Commission.pdf; 2 Easton Farm - Financial Illustration.pdf

Hi Ann,

Please see the attached written submission for the 3/10 Springboro Planning Commission Meeting.

Additionally, I would like to register to speak during the 10 March planning meeting zoom call.

Thank you,

Justin Wiedle, 164 Deer Trail

March 9, 2021

Dear Springboro Planning Commission,

I ask you to deeply consider the proposal in front of you for Easton Farm. While I acknowledge the Developer put together a thoughtful plan, it still falls short of what I and my fellow taxpaying citizens want for the City.

Density is clearly an issue, no different from the 2017 plan the Commission turned away. Greenspace was another issue in 2017 and it would appear that there is less greenspace in this proposal.

Hunter Springs residents remain concerned about public safety and privacy as it relates to this high-density proposal. Of greatest importance is the safety of our youth and family, as connecting the PUD at Fox Trail Drive would create a nearly mile-long strip where speed would become an issue in a neighborhood full of children. It is undeniable that these are areas of detriment for Hunter Springs residents when compared to our current climate without said PUD in existence.

Sadly, this development brings nothing to Springboro that isn't already offered to our citizens within a 1-mile radius. In fact, I can make the argument that this development leaves our City worse off. Using projections (see attached illustration), I am able to draw a few general conclusions:

- Property Taxes on Single-Family Homes
 - Assuming two children per household, the property taxes on the single-family homes will only generate \$1,500 per student – meager compared to local funding levels needed to fund the education of each child annually.
 - \$221K in property taxes generated by the single-family homes for the Township/Fire Services would not be sufficient to fund the personnel and capital expenditures needed for a new fire station.
 - \$26K in property taxes generated for the City is the equivalent of 25% of one full time officer's salary and benefits, or just a few curbs and sidewalks.
- Income Taxes on Single Family Homes & Commercial Jobs
 - City income tax generated from new residents and the commercial jobs created could amount to \$570K.
 - With a current estimated population of 18,196, the new housing can be expected to increase Springboro's population by 7%.
 - With a current general fund budget of \$20.5M (2019), incremental income tax of \$570K would increase City revenue by 2.8%.
 - Therefore, population increases by 7% but funding increases by 2.8%. Per capita revenue decreases. This development is not self-sustaining.

In conclusion - It is understandable that the Easton Farm owners would be interested in selling their valuable and historic property. But there is no law that states they be allowed to change the zoning laws in order to accommodate a sale. If they were to list their property at current market value for R-1 zoned farm/residential, I feel confident that buyers would be lined up. Proof of this lies all around Springboro in Clearcreek Township, where similar sized parcels are being scooped up and subdivided into communities of \$400,000 to \$800,000 homes on 1 to 3 acre lots.

Respectfully Submitted,

Justin Wiedle, 164 Deer Trail

Easton Farm Financial Illustration March 2021

	Units	Acres	Density
Single Family/Townhomes	253	74.44	3.40
Apartments	324	10.12	32.02
Ind. Living	110	18.75	5.87
	687	103.31	6.65

	Distribution %
County	10.52%
Township	18.04%
School	61.32%
Vocational	3.92%
City	2.17%
Special	4.03%

PROPERTY TAXES

Property Tax Distribution						
			Prop Tax			
Est. Value	Tru Value	Assessed	Eff. Rate	# Homes	Generated	
Single Family/Townhomes \$ 340,000	\$ 275,000	\$ 96,250	50.393808	253	\$ 1,227,152	
						County \$ 129,096
						Township \$ 221,378
						School \$ 752,490
						Vocational \$ 48,104
						City \$ 26,629
						Special \$ 49,454

Conclusions: At 2 kids per single family home, property taxes only generate \$1,500 per student for the school district.
 \$221K generated for the township (Fire Department) will not be enough to operate a new Fire Station and fund capital equipment purchases.
 \$26K in City taxes is the equivalent of half a new hire Police Officer, or enough to fund new sidewalks for 2-3 houses.

INCOME TAXES

		Income per		Income of New		City Income	
		Capita		Residents		Tax Rate	
Units	# per unit	Persons	Assessed	Income	Resident	City Income	City Income
Single Family/Townhomes	253	633	\$ 43,270	\$ 27,368,275	0.50%	\$ 136,841	
Apartments	324	648	\$ 43,270	\$ 28,038,960	0.50%	\$ 140,195	
						\$ 277,036	

New residents maintain current employment outside Springboro, thus they receive 1% reciprocal city income tax credit

		Annual		City Income	
		Hours		Tax Rate	
# Jobs	Avg. Pay	Hours	Taxable Income	City Income	City Income
Commercial Jobs	\$15	1300	\$19,500,000	1.50%	\$ 292,500
assumptions - 1000 new jobs average 25 hours a week.					

Conclusions: City Income tax generated equates to approx. \$570K for new residents and new jobholders.
 With a current est. population of 18,196, this development would increase population by 7%.

With a current City General Fund Budget of \$20.5M (2019), incremental income tax would increase the budget 2.8%.
 Therefore, 7% increase in population only increases the budget approximately 2.8% - this development is not self-sustaining.

Ann Burns

From: D B <theusualshady@gmail.com>
Sent: Monday, March 08, 2021 9:26 PM
To: Ann Burns
Cc: Dan Boron; justin.wiedle@gmail.com
Subject: 2021 Easton Farm Plan Written Submission - Beckman
Attachments: 2021 Easton Farm Proposal Assessment.pdf

Ann,

Please see the attached written submission for the Springboro Planning Commission. I would appreciate your assistance in distributing it to the members for review and consideration.

Additionally, I would like to register my interest in speaking during the 10 March planning meeting zoom call.

I would appreciate it if you could confirm receipt of this message. Thank you.

V/R,
David Beckman

1168 Deer Trail

08 MARCH 2021

To: Springboro Planning Commission
From: David Beckman, on behalf of Hunter Springs Residents

2021 Easton Farm Proposal: High Density, Aggressive Growth Plan Conflicts With Zoning and Community Views on Growth

1. Summary

The 2021 Easton Farm development proposal (hereafter referred to as "the 2021 plan") is a high density, rapid growth vision that clashes with current zoning and community opinion on growth while also contrasting heavily with the current culture and identity of the Springboro Community. In 2017, a similar plan was recommended for denial by the planning commission due to high density rates as well as a mismatch with surrounding development patterns. The 2021 plan is more dense, has less open space, is more uniformly urban and will create more traffic than the 2017 plan. This analysis shows the 2021 plan represents aggressive growth, requests a drastic change in zoning and is incongruous with the existing community's culture and, as such, denial of this plan is strongly recommended.

- In 2017, the Springboro Planning Commission recommend the less dense 2017 plan for denial because its "densities exceed those identified in the city's land use plan and the proposed development is not consistent with development patterns of this portion of the community".
- The Easton Farm parcel is zoned R-1, low density housing at 2 units per acre designed to "stabilize, protect and encourage the residential character of the district", according to Springboro's codified ordinances. Existing Springboro residents moved here expecting the spirit of this zoning would be upheld into the future to maintain the look and feel of the city in which they invested.
- The 2021 plan is more dense and has a total of at least 224 more living units (nearly 50% more!) than the 2017 plan, exacerbating documented density concerns.
- The 2021 plan has 5 acres less open space than the 2017 plan which, combined with higher total unit volume, will likely contribute to overcrowding of nearby parks and a crowded urban-like atmosphere.
- Five community surveys spanning over 12 years document that over 90% of Springboro residents want moderate to no growth, in direct conflict with the 2021 plan's aggressive growth proposal.
- Over 90% of Springboro residents are homeowners, judging from community surveys. Construction of the 2021 plan would fundamentally change the fabric of the Springboro demographic.
- A variety of other quality of life concerns will result from this type of development and have a detrimental effect on the greater community. These concerns include: traffic, schools, parks, safety, more transient and less community invested inhabitants, out-of-place building heights, and a change in character that will erode Springboro's small-town aura.

2. Zoning and Density

The Springboro Planning Commission recommended the 2017 Easton Farm plan for denial based, primarily, on density concerns:

"The proposed residential development densities exceed those identified in the city's land use plan and the proposed development is not consistent with development patterns of this portion of the community, specifically the relationship of proposed higher density multi-family residential to existing lower density residential neighborhoods."

-- Springboro Planning Commission reasoning for recommending denial of the 2017 Easton Farm Plan

These same concerns remain. As documented in Table 1 the 2021 plan is more dense and has more (nearly 50%) total residential units. Note, in this analysis independent living is considered a residential unit because residents will live there. Even after removing the independent living units from consideration, the 2021 plan has at least 114 (or 25%) more residential units than the 2017 plan.

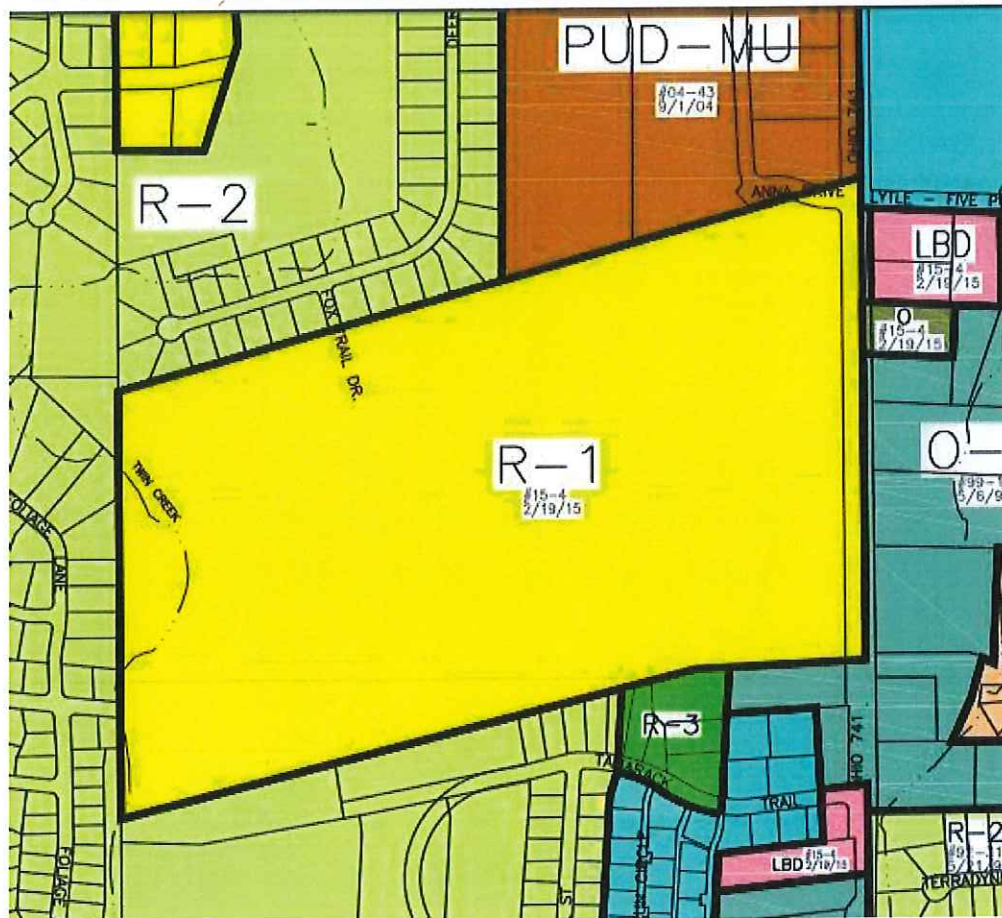
Reference	Single Family Homes	Multi Family Units	Independent Living	Total Units	Comments
2017 Easton Farm Plan	107	356	N/A	463	This plan was recommended for denial due to density concerns.
2021 Easton Farm Plan	253 or 275	324	110 or 113	690 or 712	<p>This plan has 224 or 249 more units, or 48%/54% more units than the 2017 plan. There are 150% more single family homes – homes that are more likely to have a higher number of inhabitants than multi-family units. Thus it is reasonable to expect with more units and more inhabitants traffic and density will be higher than the 2017 plan.</p> <p>NOTE: the multiple values in the columns are due to errors in the submitted development plan. Page 11 and page 54 of the plan provide differing unit numbers.</p>

Table 1: Comparison of Recent Easton Farm Development Plans by Unit Type and Volume

This density in the 2021 plan is not only higher than the 2017 plan, but continues to exceed the current R-1 zoning for the Easton parcel of 2 units per acre. Table 2 shows the density of the Easton Farm plans compared to current zoning, which can also be viewed in Graphic 1.

Reference	Overall Density	Comments
2017 Easton Farm Plan	6.66 u/ac	Recommended by city for denial due to high density , withdrawn plan (25OCT2017) density cited.
2021 Easton Farm Plan	6.83 u/ac, as listed OR 7.08 u/ac, based on 599 unit total on page 11 of the plan (not including independent living units)	This plan has a <u>higher density than the 2017 plan</u> . NOTE: The submitted plan density calculation may be inaccurate. The 6.83 u/ac calculation is based on 577 units, but page 11 of the 2021 plan (reference 8) states there are 599 units not including the independent living units. Page 54 of the plan states there are 577 units not including independent living. It is unclear which number is accurate. If independent living units are included, the density is likely even higher.
Current Easton Farm Zoning	2.0 u/ac	Current zoning is well below the proposed density.

Table 2: Comparison of Recent Easton Farm Development Plans to Current Zoning



Graphic 1: Current Zoning per Springboro Zoning Map

The Easton Farm land is zoned R-1, which carries a maximum density of 2.0 units per acre. The intent of R1, as stated in Springboro's codified ordinances is: "to stabilize, protect and encourage the residential character of the district and **prohibit activities not compatible with a low density residential neighborhood. Development is limited to single-family dwellings** plus Public Uses and Quasipublic Uses". All residents living in the vicinity of the Easton Farm have moved here with the expectation that the R-1 zoning set forth in Springboro's zoning would be honored as R-1 into the future. We have chosen to make our most expensive investment – our homes – and invest our livelihood through taxes and trust in this city. Changing the zoning by converting this parcel into Planned Use Development – Mixed Use (PUD-MU) to enable this construction would break this trust and set a concerning precedent for all residents in the city.

Closely related to community, but a component enabled by zoning changes are buildings with three or more stories. Even when the tallest buildings are located closer to state route (SR) 741 three or more stories will feel like they are towering over surrounding residential neighborhoods such as Tamarack Trail – it would feel like they are on the edge of a city. As an example, see the way the Village Park townhomes tower over the back yards of the eastern most residents of Deer Trail Drive. These are only three stories. Four would be worse and would mean the towering feeling is applicable over farther distances. The proposed three and four story buildings would look out of place along SR 741 and change the character of the corridor. There is no precedent for a 3 or four story buildings along this corridor. Construction of buildings this height would permanently change the atmosphere along SR 741 and compromise the low-rise small town feeling throughout Springboro.

An important aspect of a development plan that is open (green) space. Open space is inversely related to density and the total volume of developed land. The 2021 plan has less open space than the 2017 plan by 5 acres which, when combined with high density multi-family housing and 31' residential lots, will likely make this development feel very dense and urban.

Comparison of Open Space in Easton Farm Development Plans		
Reference Plan	Open space (acres)	Comments
2017 Easton Farm Plan	20.76	This plan was recommend for denial by the planning commission in 2017.
2021 Easton Farm Plan	15.66	The 2021 plan has less open green space than the 2017 plan owing to the density and land use volume of the development.

3. Community

Community surveys provide insight into what the city is doing well and where the community would like to see the city go in the future. Table 3 tabulates these views and compares them with actual observed growth through new construction permits. It is overwhelming clear through 12 years of community surveys that over 90% of the city's residents want moderate or no growth. In other words: they like things how they are. The rate of construction reflects this view and also reflects a culmination of good zoning and smart planning in line with the community's desires. If the Easton Farm plan is approved there will be an explosion of development creating more new housing units than all new construction in Springboro over the last 14 years (estimating 56 homes in 2020)! This amount of construction could not be categorized as moderate to no growth, this is aggressive growth. This volume of construction along with the traffic and congestion it will create will remove the small town feel that motivated current residents to move here in the first place and is a reason residents like living in Springboro.

Year	Single Family Home New Construction Permits	Percent of Residents Wanting Moderate or No Growth	Comment
2008	12	92.00%	Per 2008 Community Survey
2009	19	---	No survey conducted
2010	25	---	No survey conducted
2011	24	89.00%	Per 2011 Community Survey
2012	67	---	No survey conducted
2013	95	---	No survey conducted
2014	65	94.00%	Per 2014 Community Survey
2015	67	---	No survey conducted
2016	61	---	No survey conducted
2017	70	90.00%	Per 2017 Community Survey
2018	54	---	No survey conducted
2019	56	---	No survey conducted
2020	49 (Mar-Dec)	90.00%	Per 2020 Community Survey

Table 3: Comparison of Springboro Development with Community Views on Growth Preferences

This plan appears to be creating a small city within the city of Springboro. Certain features such as a two-story parking garage, street side parallel parking and very narrow 33' lots are only found in cities or dense urban inspired mixed used developments such as Austin Landing. We already have an Austin Landing just one mile north on 741 and Austin Landing South will create more multi-family housing in a city-like environment. This type of development near residents in the small town of Springboro is completely the opposite of community views on growth and the current R-1 zoning of this property.

The city of Springboro is where people buy a home in which they will raise a family or retire. Over 90% of Springboro residents are homeowners, according to community surveys. This makes Springboro very unique compared to many surrounding cities and likely contributes greatly to the small town feel community so many residents cherish. Table 4 displays the large percentage of homeowners in the community. Creation of hundreds of rental units (more than all new homes constructed in the last 6 years) would create a change in the very fabric of the city. It is more likely a homeowner will be more invested in a city's future and more likely to reside in Springboro long term, contributing positively to the small town culture of the city.

Year	Percent of Home Ownership, based on community surveys
2008	96.00%
2009	no survey
2010	no survey
2011	94.00%
2012	no survey
2013	no survey
2014	96.00%
2015	no survey
2016	no survey
2017	95.00%
2018	no survey
2019	no survey
2020	94.00%

Table 4: Percent of Springboro Residents That Are Homeowners, Based on Community Surveys

4.0 Quality of Life Concerns

4.1 Traffic

4.1.1. Traffic concerns fall into two broad categories: road congestion and throughfare. A development of this size and density will place many new vehicles on SR 741. Additionally, the connectivity to existing residential neighborhoods will lead to increased throughfare.

4.2.1. Low density single family housing, in accordance with current zoning, would exponentially lower the traffic impact.

4.1.2. Six hundred eighty seven units (687) with an estimated two cars per unit will place 1374 new vehicles on SR 741. This would result in over 2500 new vehicle trips a day if each vehicle conducts only one round trip a day. Since most people make more than one trip of a day this number is a low estimate. This figure does not include employees, customers or visitors. Congestion on SR741 will increase significantly and the creation of a new light on SR 741 @ Easton Farm Blvd will slow traffic throughout the SR 741 corridor.

4.1.3. Throughfare is a reality. It is human nature to seek the fastest distance between two points. While it is difficult to quantify throughfare it will be significant and will increase noise, reduce safety for children at play and lower quality of life in surrounding neighborhoods, particularly Deer Trail Drive. When motorists cut through a neighborhood they are less likely to follow the speed limit than residents of that neighborhood and less likely to drive through as if they lived there. As the high density of the development congests SR 741, motorists will find access through Deer Trail Drive and Tamarack Trail to be a favorable alternatives that allow motorists to bypass 741 congestion (along with multiple stop lights) and congestion associated with transiting access roads in the eastern most portion of the 2021 plan. Throughfare from Pennyroyal Drive to Deer Trail Drive to

Paddock Trail/Village Park Blvd is already common and an easy example showing that motorists will always find the fastest way to their destination. Tamarack Trail residents can surely relate: throughfare between SR 741 and SR 73/North Park creates a constant high volume of traffic that would only worsen with the 2021 plan. The proposed on-street parking in the 2021 plan will slow down motorists as well, leaving them to seek an easy way to reach the western portion of the development. In short, throughfare is a significant concern and expected side effect from high density, high congestion development that will have an outsize impact on surrounding neighborhood's safety and quality of life. It is unrealistic to expect 687 units worth of traffic to flow orderly to the East and transit only the two SR 741 access points.

4.2 Schools

- 4.2.1. Springboro schools are crowded and the budget is low (601st out of 608 school districts in Ohio in spending per pupil). The Springboro school system is in re-occurring annual deficit spending pattern. A new levy is needed and planned but passage is uncertain. The school district's average teacher to student ratio is 24:1 (some classes exceed 27 students!) which is much higher than the Ohio average of 18:1. Our school district can't afford or handle a large influx of students from this development, especially if factoring in the growing influx of students from new communities in Clearcreek Township. Low density single family housing, in accordance with current zoning, would lower the burden on our schools.

4.3 Parks

- 4.3.1. Near the Easton Farm area there are two parks: North Park and Gardner Park. North Park is the only park in Northwest Springboro with a playground. The high density of units and low area of open space in the 2021 plan is problematic for the city's parks. North Park's playground, tennis courts and walking trails are already crowded on nice days. Gardner park's walking trails are already popular and increasingly crowded. While the easy access to these parks from the proposed development is convenient for future inhabitants it will place a lot of stress on two parks already nearing capacity on nice days. Simply put, there are too many people and not enough park space. The open space proposed in this plan AND the city's two parks are not enough to absorb another potential 1200 inhabitants.

4.4 Safety

- 4.4.1. Springboro's police force is exemplary and current residents want it to remain that way. Six hundred and eighty seven more units, and likely 1200 more residents, living in close quarters is a recipe for more calls to the police department. The building of this dense of a development will require more police incident responses and patrols versus a lower density development. 4.2.1. Low density single family housing, in accordance with current zoning, would lower the burden on our police force.

4.5 A More Transient Community

- 4.5.1. Rental properties will create a more transient community. A more transient community is not ideal for creating long term investments in the community like a homeowner might and makes it harder to maintain a tight knit neighborly small town feeling, which is contrary to an attractive quality residents positively rated in community surveys.

4.6 Changing Springboro's Character

4.6.1. The "city within a city" concept of the 2021 plan is a departure from Springboro's existing character. The following features or outcomes are significant differences from the city residents know and love:

- Tall buildings along SR 741, creating a "skyline" and forever changing the small town feel of Springboro
- Small city size lots 31' in width creating a urban environment
- A parking garage within city limits
- Parallel parking along streets outside of downtown
- Multi-family housing with hundreds of units
- An increase of thousands of vehicle trips a day resulting in more congestion on SR 741
- Throughfare of surrounding neighborhoods, reducing safety, increasing noise and reducing quality of life.

5.0 References

1. Springboro Community Satisfaction Surveys 2008-2020. <https://www.cityofspringboro.com/378/Community-Satisfaction-Survey>
2. Springboro Adopted Land Use Plan
<https://www.cityofspringboro.com/DocumentCenter/View/178/Adopted-Land-Use-Plan-PDF>
3. Springboro City Newsletters <https://www.cityofspringboro.com/206/City-Newsletter>
4. Springboro Codified Ordinances
<https://codelibrary.amlegal.com/codes/springboro/latest/overview>
5. Springboro Zoning Map
<https://www.cityofspringboro.com/DocumentCenter/View/1023/Zoning-Map>
6. Dayton Daily News
<https://www.daytondailynews.com/news/local-govt--politics/springboro-staff-recommends-rejection-easton-farm-development/Me4rKKRzxLjYk0g7qmrq6N/>
7. 2017 Easton Farm Plan (10/25/2017 revision)
<https://www.cityofspringboro.com/DocumentCenter/View/533/Easton-Farm-605-North-Main-Street-Rezoning-General-Plan-Design-Guidelines-10-25-17-WITHDRAWN?bidId=>
8. 2021 Easton Farm Plan (3/10/2021 Planning Meeting)
<https://www.cityofspringboro.com/DocumentCenter/View/1283/2021-03-10-Planning-Commission-Meeting-Materials>
9. Springboro Schools Five Year Forecast (May 2020 Update)
<https://www.springboro.org/Downloads/Five%20Year%20Forecast%20and%20Assumptions%20May%2020202.pdf>
10. City of Springboro Residential Permit Reports (Mar-Dec 2020)
<https://www.cityofspringboro.com/Archive.aspx?AMID=37>
11. City of Springboro Comprehensive Annual Financial Report
<https://www.cityofspringboro.com/ArchiveCenter/ViewFile/Item/378>

Ann Burns

From: steven_b_smith@earthlink.net
Sent: Friday, March 05, 2021 12:12 PM
To: Dan Boron
Cc: Ann Burns
Subject: RE: March 10th Planning Commission Meeting Information

Dan, Thanks for sending over. I spoke to Ann earlier today in f/u to our brief conversation yesterday. I would formally like to request time to comment/present at next Wednesday's meeting via Zoom. What is the time allotment for a given speaker? Will we be able to make use of slides or images? I understand these sessions are recorded.

Thanks for everything.

Steve

Address - 30 Jacamar Ct

From: Dan Boron <danb@cityofspringboro.com>
Sent: Friday, March 5, 2021 11:33 AM
To: steven_b_smith@earthlink.net
Cc: Ann Burns <ann.burns@cityofspringboro.com>
Subject: March 10th Planning Commission Meeting Information

Mr. Smith,

Good morning. Attached is the letter I mentioned during our phone conversation yesterday.

Dan

*Daniel J. Boron, AICP
Planner, City of Springboro
320 West Central Avenue
Springboro, Ohio 45066
(937) 748-6183*

March 10, 2021

Thanks for time today.

Appreciate audience-thanks to Dan Boron & Ann for their help in getting on agenda.

By way of background, I'm a 28-year resident of Springboro-I've seen a lot of development in that time, most of which has been done well, and we have been able to maintain the small-town feel/flavor.

Exception: N. Park, removal of wetlands, and loss of red-tipped blackbird.

Today, I stand in opposition to the Easton Farms proposal. I view it as the straw that broke the camel's back. This development will be a horror show of epic proportions & will significantly impact QOL for residents living near this development.

Let me remind newer residents that this proposal was previously dressed up in a different form and was twice voted down by your fellow citizens-so this is NOT a done deal.

I've reviewed the plans in some detail, and while I'm not AICP accredited, I've looked at this from a 'common sense' basis. Well done, thorough, and clearly a lot of thought and energy has been poured into this draft.

What is missing, though, IMO, is a statement of need-a rationale, a purpose, a justification. This is nowhere to be found in the proposal anywhere-has this been done separately? What is the ROI?

I am particularly concerned about how this will impact North Park and its ambiance, notwithstanding the crushing load of new people 'discovering' the Park for the 1st time. As somebody who faithfully walks the Park 2x/day, this is of serious concern.

The connection of Anna Dr. to Tamarack is a serious opportunity for cut-through traffic to avoid the TWO red lights that you would otherwise need to navigate. As pointed out, one of drawings mislabeled.

My specific areas of concern:

1. Inalterable changes of the nature and ambiance of N. Springboro, including North Park
2. Traffic and wait times will increase significantly with the development and new stop light.
3. Effects of noise-current background is about 35db, what will this be post-development?
4. FEMA FLOODPLANE sits directly adjacent to development-how will this be addressed?
5. Effects on wildlife that use this area to feed-small mammals, and 3 deer in particular.
6. What is the ROI on the proposed development, both short-term and longer-term? \$265MMM

I'd like to see each of these issues addressed, along with a rationale for WHY we need this?

Thanks for your time today.

Respectfully,

Steven B. Smith, PharmD

30 Jacamar Ct.

Springboro, OH 45066

Ann Burns

From: robin45066 <robin45066@gmail.com>
Sent: Tuesday, March 09, 2021 3:14 PM
To: Ann Burns
Subject: planning commission meeting

Good Afternoon,

I would like to register to read a statement on behalf of the Easton/Hall family for the Planning Commission meeting tomorrow evening.

I will also pre register via Zoom asap.

If you have any questions please feel free to contact me.

Thank You,
Robin Hall
937-545-6953

605 N. Main

Sent from my Verizon, Samsung Galaxy smartphone

Ann Burns

From: Jamie Duck <jdrakeduck@gmail.com>
Sent: Tuesday, March 09, 2021 7:51 PM
To: Ann Burns
Subject: Easton development concerns

I am writing in concern for the development of the Easton Farm property. I currently live at 172 Deer Trail Dr. My property is directly next to the field. The ability to have a less busy place but still be in Springboro, is one of the reasons I loved this property in the first place. In reviewing the plan for the farm, I have many concerns beyond the view from my backyard. First, I am concerned about the density of the people who could occupy this space. There are many single family homes, but also an apartment complex. The increase in this volume of people would certainly overrun the schools. This would also make the day to day in Hunter Springs much more hectic with increased traffic, of not only the residence of this proposed plan, but also people who are using the commercial developments of this land. This density of the plan proposed not only affects the load on the school system, but it would also affect the character and availability of existing city resources of the residence of Springboro.

The connection to Fox Trail would also create an area of street that would put our children at danger. This road is proposed to create a mile long 25mph zone. I live right next to this road. The children of Hunter Springs are quite visual during the spring, summer and fall. The residents of this community also have a heavy presence day to day. Having the characteristic of limited traffic in our community has created a safe place for our families. I also question the thoughts and desires of the taxpayers of Springboro as well. If the tax payers of the City of Springboro do not prefer this land to be developed into a very densely populated area, why would this be supported? Would a lower density and more green space area be more preferred? As this is more dense than the 2017 proposal, the residents of Springboro have not gotten that chance to evaluate a plan such as this. From the social media traffic in the Springboro area, the residents seem to prefer some green space and park areas that are family friendly and quite areas. Perhaps this would be a future consideration.

Also of note, the proposal to develop the business commercial areas of the front of the property seems to be unnecessary as there are several commercial properties in the area just north on 741 that have stood open for several years.

Thank you for listening to my concerns.

Jamie Duck

Ann Burns

From: Rod Knight <retiredyoung2havefun@yahoo.com>
Sent: Tuesday, March 09, 2021 8:14 PM - Late Submittal
To: Ann Burns
Subject: Easton Farm Development

The proposed development of Easton Farm is a example of folly. First the density of Hunter Springs is NOT .25 acres. It is about half again this at minimum 1/3 acre. The cut through at Fox Trail is a HUGE mistake giving people a way to avoid lights at Pennyroyal Rd. and at the 5th 3rd bank. Putting loads more traffic on Deer Trail. We were assured before that would not happen. I see it every day. All the new infrastructure necessary to support the growth who pays for that increase in fire protection street improvements, police protection, schools? Who benefits? It surely won't be the taxpayers. I guess as I look from Deer Trail I will not be able to see 4 story buildings on or near Main St. just like we can not see the hideous 3 story condos behind us. What businesses are going to want there except for retirement homes, a restaurant or two a doctors offices. Easton Farm should NOT be rezoned so that a few can make money at everyone else's expense.

Address - 201 Deer Trail

Ann Burns

From: Springboro, OH - The Goddard School® <SpringboroOH@goddardschools.com>
Sent: Tuesday, March 09, 2021 4:20 PM
To: Ann Burns
Subject: Easton Farm Development

Ms. Burns,

We would like to express our concerns regarding the development of Easton Farm. We have 2 major concerns:

1. We are concerned about the construction dust and the certainty that it will blow onto the school grounds. The concern is not only that it will blow onto the building and playground equipment, but while the students are outside, it will blow onto them and there is the potential of their lungs being irritated by the debris. We request that some type of high barrier be installed between the development site and The Goddard School.
2. We are concerned about the proximity of the fire station to the school and the noise impact it will have. Children are often startled by loud sounds and we are concerned that it will cause undue stress as well as decrease the quality of their naps. We respectfully request that the fire station be relocated so it will be less of a noise impact to The Goddard School.

We appreciate the opportunity to express our concerns to the City.

Best,
Leena Rekhi-Salmon
Banita Rekhi

705 Gardner Rd.

Ann Burns

From: edalepack@aol.com
Sent: Tuesday, March 09, 2021 4:02 PM
To: Ann Burns
Subject: Easton Farm - Concerns

Good afternoon Ms Burns.

I'm writing in regards to the upcoming Planning Mtg discussion involving the Easton Farm proposed real estate development project. My house is located on Tamarack Trail and is one of the homes that borders the South side of the Easton Farm. That said, I have some real concerns with the plan and I think my neighbors all share some of the same concerns.

The incredibly small width of the residential lots that requires alleyways to access a garage from the rear is a real concern. This will obviously lead to issues with parking in these cramped alleyways and will quickly become an eyesore along with reducing the value of the surrounding properties. While the overall average headcount per acre may not appear to be extreme the areas that have <50' wide lots certainly has a much higher headcount per acre than the surrounding neighborhoods.

The style of homes in the proposal do not appear to be what you would expect when you think of the Springboro community. These are mostly very small homes crammed together and from the illustrations the homes appear to be made with very low end building products.

The additional traffic dumping out onto Tamarack Trail is also a concern. This street is already very busy and at times it takes several minutes just to get out of the driveway because the traffic is continuous at certain times of the day. Adding several hundred families to the Easton Farm development and a road that dumps traffic out onto Tamarack Trail near the North Park entrance is going to feed additional traffic through other residential neighborhoods and in areas where small children are more likely to be.

Please consider a different plan that doesn't overload the area with traffic and degrade the value of the surrounding properties.

Thank you,

D. Pack

150 Tamarack Trail

Ann Burns

From: Mike Hemmert <hemmert1400@aol.com>
Sent: Tuesday, March 09, 2021 3:55 PM
To: Ann Burns
Subject: I ask that you please read this email into the Record at the Planning Commission Meeting for the Easton Farm Development on 3-10-21

Good Evening,

My name is Mike Hemmert, I reside at [50 Royal Highlands Dr., Springboro](#). I have lived in the Springboro area for 37 years and first came to Springboro in 1984 to purchase a small business. I operated it for 33 years before recently selling and retiring.

During this time, I have seen an orderly development both on a Business and Residential basis.

I also feel the City has always operated in a positive forum to balance development and I feel confident they will also do so in the Easton Farm Proposal.

That being said, I am a PROPONENT of the Application on file for the rezoning to a requested PUD-MU. This is a perfect parcel to have the mixed use of Retail across the frontage closest to St. Rt. 741 and residential housing.

As we move to the Preliminary/ General Plan...I am an OPPONENT to the Residential portion as currently submitted. The density is too high.

According to a recent Survey Plan...a Consultant that the City of Springboro hired to address the needs of the community...a Presentation was made on Dec. 21, 2020 via a zoom meeting...I attended.

I have attached Screen Shots of the Housing that people wished to see for your review.

I would like to draw attention to the Slide that states 91% of the population in Springboro would like to see single family housing (detached and attached.)

When I reviewed all of the Slides there seemed to be NO interest OR demand for Apartment housing.

The Apartment building would also be non desirable due to proven high crime in our City close to apartment buildings.

I would like to remind Planners that when the Meijer grocery store at Clearcreek-Franklin Rd. and St. Rt. 73 was proposed...Police Dept. Records were introduced that the highest crime area in Springboro was the Meadow View Apartments and Townhouses at [15 Woodhill Dr.](#) Those were in very close proximity to the proposal.

There is also a Slide that mentions working with the Township on housing options. In neighboring Clearcreek Township, we are seeing Life Style Homes that seem to be a good fit for 55+. Developers in that area have presented research that there is a growing need for this and there is a shortage of downsized homes as there is a growing desire for Grandparents to move closer to family.

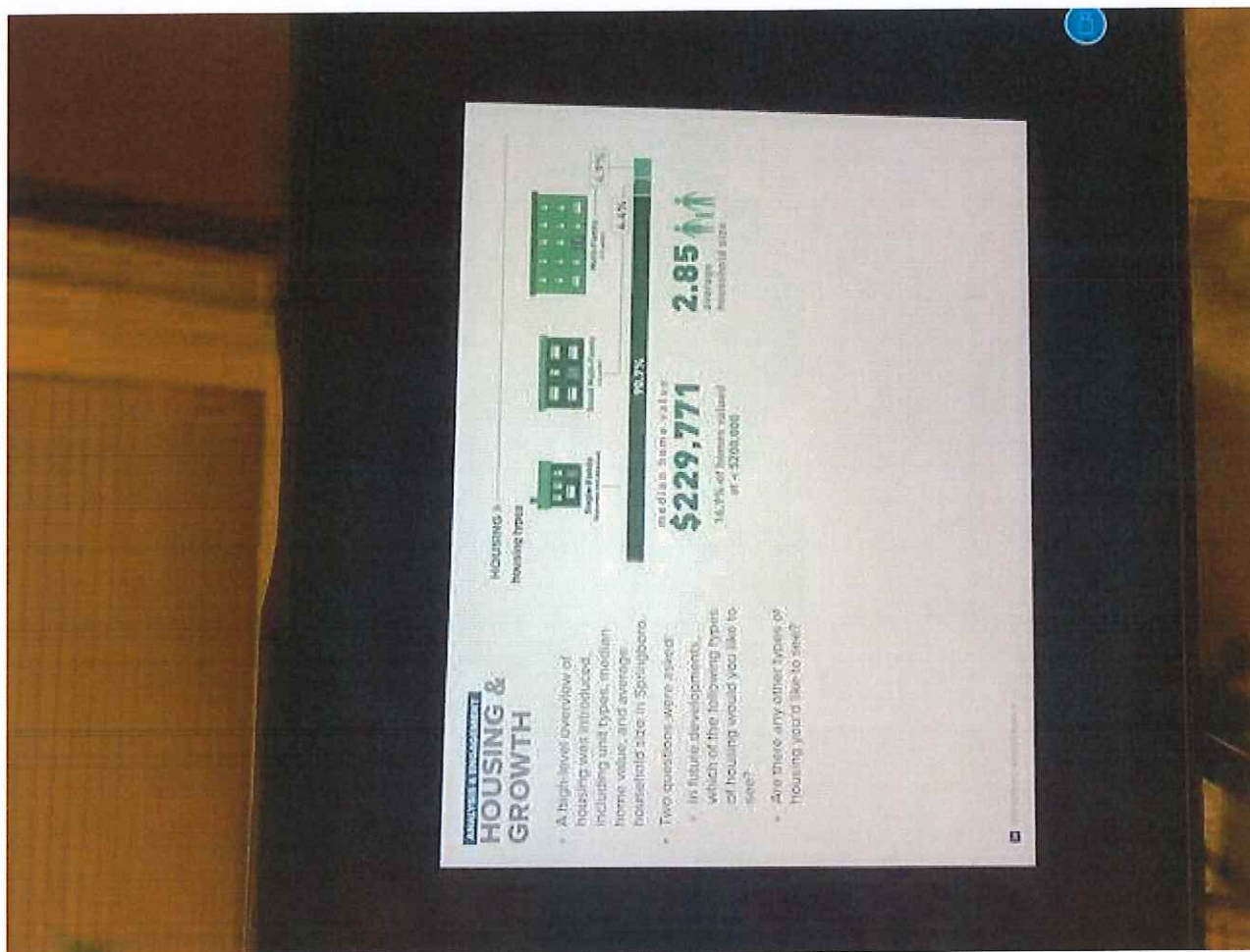
Just last week in Clearcreek Township, the Silver Mill Subdivision received Phase 1 approval for a Mixed Use residential Development of 117 single family and 235 Lifestyle Homes. Easton Farms seems like this could be a similar fit and also address the housing desires of what our community is asking for.

I would like to note that the 55+ targeted demographic also had little to no strain on the School District.

In summary, I believe the Easton Farm property should be repositioned to have the inclusion of the type housing our community has asked for in the Dec. 21, 2020 Presentation by a wide margin of 91%... they are asking for single family attached and detached.

Respectfully Submitted,

Mike Hemmert



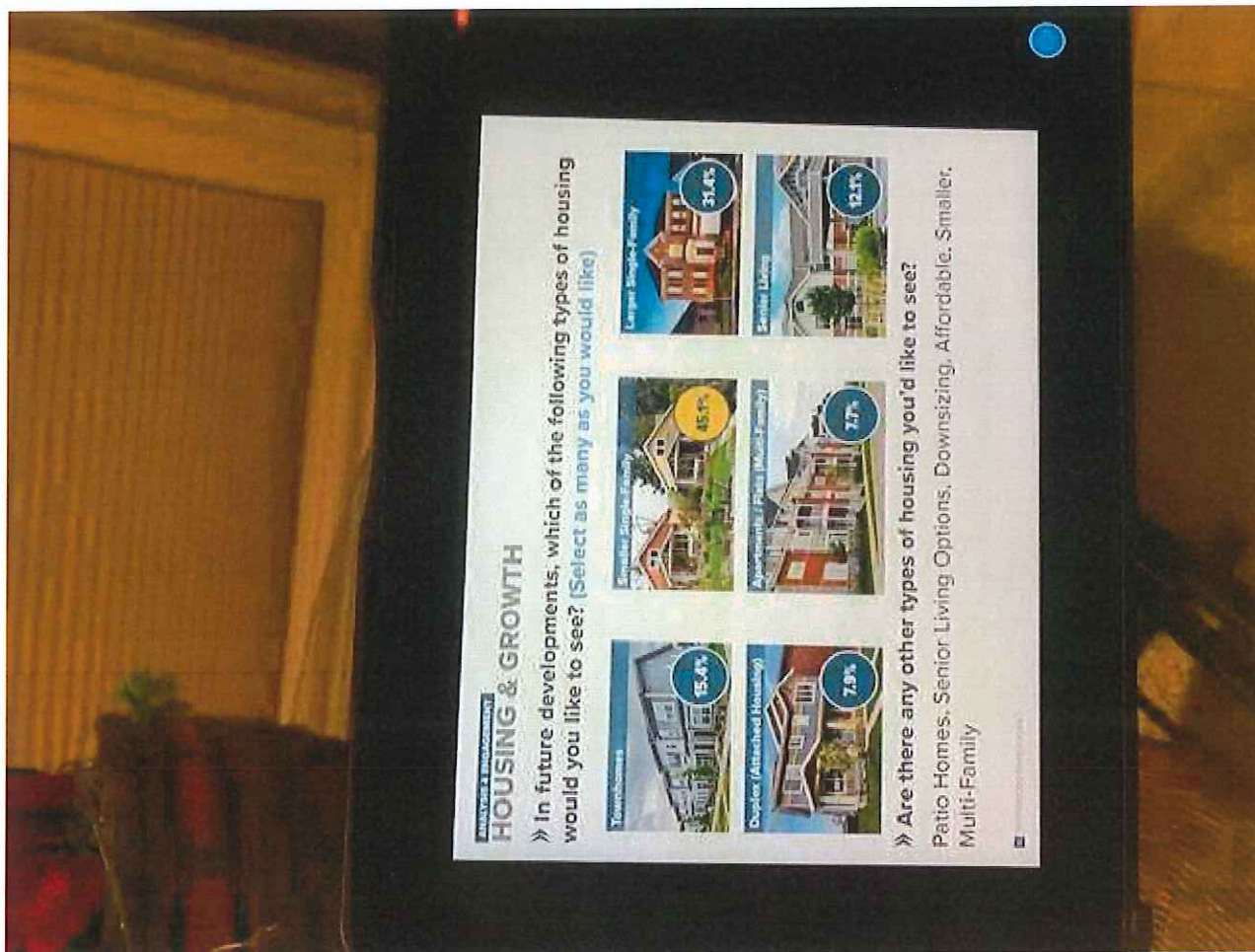
HOUSING & GROWTH

» If you are planning to move in the next 5 years, do you intend to stay in Springboro?



» If you plan to move in the next 5 years, what is the reason?





Sent from my iPad

Ann Burns

From: Tyrchniewicz, Michael Lorne <tyrchniewicz.2@wright.edu>
Sent: Tuesday, March 09, 2021 2:25 PM
To: Ann Burns
Subject: Easton farm development

Mz. Burns,

I live at 209 deer trail, near the proposed connection into the Easton farm development. I have concerns with the density of the project being proposed. 4 story apartments and tall assisted living seem to go against the flavor of the housing currently in the area. Looking over the proposal map it seems that the plan is to shoehorn in as much as possible without regards to the significant available and empty land that is around it.

Even if my reservations do not mean much I would much rather see a lower density plan similar to the hunter springs development or the neighborhood on the west side.

Thank you,
Mike Tyrchniewicz
937-554-8526

Ann Burns

From: amy Shivener <amyshivener@yahoo.com>
Sent: Tuesday, March 09, 2021 1:03 PM
To: Ann Burns
Subject: Easton Farm Proposal

Good afternoon,

My name is Amy Shivener and I live at 5 Fox Trail Dr. in the Hunter Springs subdivision. My husband, Dale and I have some concerns about the new proposal for Easton Farm development and would like to have them made known at the Commission Meeting scheduled for 03/10/21.

The new proposal for single family homes with a density of 4.36 units per acre does not align with what we currently have on Deer Trail/Fox Trail as the letter we received stated. Our concerns that the density proposed has too many units per acre. This proposal is greater than the density proposed in 2017 which city council denied at that time. We wish to maintain the character and value of our neighborhood by not having homes stacked on top of each other. The increased density also raises safety concerns for our neighborhood-increase in traffic with cut through Fox Trail. We have many children that play on this 25 mile per hour stretch and I am concerned that Fox Trail/ Deer Trail will be used as a short cut for travelers as Paddock Trail has become for people that do not live in our neighborhood. The increased density also poses a risk of strain on our school system, EMS availability as well. I understand the need to grow and develop the land. I am suggesting that a new revised density proposal be drawn up that is more in line with the Hunter Springs neighborhood.

I am also suggesting that the tree line be maintained for houses on Deer Trail that back up to the new proposed development to maintain some privacy.

Thank you for the opportunity to have our voices heard,

Amy and Dale Shivener
5 Fox Trail Dr.
937-830-3213

Ann Burns

From: Cheryl Sheffield <golfnbake@aol.com>
Sent: Tuesday, March 09, 2021 10:47 AM
To: Ann Burns
Subject: Easton Farm proposal

>
> Hi Ann-
> We would like to express our concern and opposition to the proposed development plan for the Easton Farm.
>
> The major objection is the density per acre of this project. There is no need for 3 and 4 story buildings that would pile people on top of each other to make this financially feasible for the owners and developers. We are also concerned with traffic issues, school capacity, safety and other strains on resources. We are also very concerned about what kind of single family home can be built on a 30 foot wide lot and the parking issues that would come with that.
>
> Respectfully,
> Steve and Cheryl Sheffield
> 228 Deer Trail Dr.
> 937-603-1969
>
> Sent from my iPhone

Ann Burns

From: mark nelson <mark4725@sbcglobal.net>
Sent: Tuesday, March 09, 2021 10:41 AM
To: Ann Burns
Cc: Justin Wiedle
Subject: March 10 PUD Zoom meeting
Attachments: THE PURPOSE OF A PUD IS BUILDING AND DEVELOPMENT WITHOUT DETRIMENT TO THE COMMUNITfinal.docx

Ann,

We realize there is not much information released on the PUD on specifics yet and much will become known soon. We have many concerns but we believe you must address the access to Hunters Springs as a priority.

We are a one-mile residential street with a pretty substantial curve, a 25 mile speed limit, and sun blinded areas at various times of the day. We are residential only and we have been watching with horror all the accidents and near misses that are continually happening on Anna in a business area with all the traffic currently cutting through there even before the development of a PUD.

We have attached a document with an illustration that we would like addressed at the March 10 Zoom meeting.

Thank you,

Mark and Linda Nelson
221 Deer Trail Dr.

THE PURPOSE OF A PUD IS BUILDING AND DEVELOPMENT WITHOUT DETRIMENT TO THE COMMUNITY

The presented plans to the Easton Farm demonstrate the city and the developer's efforts to hear the residents of Deer Trail Dr. We appreciate what you did; BUT YOU ARE NOT DONE YET. The residents of Hunter Springs cannot accept the proposal as shown.

Specifically, Fox Trail Dr. cannot be connected to the PUD. You cannot use it as a through street of the PUD. Hunter Springs does not benefit in any way from the PUD, yet the current plan would cause financial loss to our property and become an extreme traffic problem for the safety of our children. When the southern half of Hunter Springs was developed and sold as part two of Hunter Springs, homes averaged .48 acre lots when open space and lots are considered. We were aware the next phase of Hunter Springs could be developed with two homes per acre estate homes, not a PUD with the density currently proposed. At the time when they were considering changing the use of the property behind us, we were assured the Deer Trail residents would be taken care of (by the Mayor and accompanied by Tom LaDu, former councilman) and we expect you will stay true to their promises. We propose an easy fix to the problem.

A solution to help Hunter Springs maintain its current character is by turning the PUD side of Fox Tail Dr. and the two adjacent PUD alleyways to dead ends; making the last two properties into equal sized lots to the others on the others on that road and turning the last alley outlet onto Anna Dr. Please see the illustration below. This would truly benefit your PUD by being able to use Anna to get to Deer Trail for any emergency while keeping Deer Trail from being a direct access point allowing us to maintain our intended residential neighborhood feel; not a pass through for a PUD.

We are aware that with this PUD all of Springboro will potentially incur increase taxes for schools, police, fire, as well as potential increases due to water and sewer improvements and others that may be needed. But we should not take a hit on our property value or our safety. This will only cost you a few units and the cost could be made up on the premiums on all the homes built on that street as it would elevate the appeal for all those lots as well.



Ann Burns

From: The Murphy Family <hileighmurphy@gmail.com>
Sent: Thursday, March 04, 2021 4:35 PM
To: Ann Burns
Subject: New Development School Impact Suggestion

To the Springboro Planning Commission,

With the sale and scheduled development of another Springboro farm, there is a serious concern regarding the impact to our schools and the need for increased taxation.

Most of our community feel it is "unfair" to increase taxes and pass levies to accommodate more families and students from these new communities when they do not have a vote to approve this type of development. One solution that is fairly common in the Western United States, is to create a Developers Fee. This fee is a set amount the developers pay for the right to develop the land. The particular legal arrangements vary by state, but it protects cities from dramatic increases in taxation while allowing growth. This fee would allow the city and school district to create a fund to cover the expense of the improvements, expansions and new schools that **will be required** because of the creation of these new developments.

Our community cannot afford to be ignorant or short sighted regarding the inevitable expansion expenses that will be required to our schools and infrastructure with these new developments. We must be proactive and not reactionary.

Thank you for your time,
Hileigh Murphy

7443 Silver Lake Dr
Waynesville, OH 45068

Ann Burns

From: Jim Milthaler <jamthaler1@gmail.com>
Sent: Monday, March 08, 2021 12:27 PM
To: Ann Burns
Subject: Development proposal for Easton Farm.

We are negatively impacted with the development proposal and do not support the proposed ideas. We would not be opposed to a residential home development that are nice single family homes and yards with approximately 1/3 to 1/2 acre lots and or parks, bike path, walk paths to and from North Park and areas for kids to play. Totally against the condo or high-rise apartment buildings for residential living. The Springboro school systems are already overcrowded. The idea of building something similar to "The Green or Austin Landing seems unreasonable with respect to losing the quiet peaceful neighborhood we presently enjoy on and around Deer Trail Drive. Even if the project proposal is partially adopted there is strong opposition to a connecting road to existing Fox Trail off of Deer Trail Dr.

Address - 132 Deer Trail

Ann Burns

From: Zachary Palmer <zppalmer90@gmail.com>
Sent: Monday, March 08, 2021 9:02 PM
To: Ann Burns
Subject: Comments for Planning Commission for 10 March 2021

Ann,

My wife and I would like to submit the following comments for the Planning commission regarding the proposed PUD-MU development which will be discussed at the March 10 meeting.

--
Regards,

Zachary and Laura Palmer

321 Park Ln

To Whom it May Concern,

We are a Springboro family submitting comments on the proposed 84.6-acre PUD-MU development at 605 North Main Street. After reviewing the zoning plans, we believe several steps would be required to ensure the protection of existing businesses and capacity of existing facilities.

Construction Adjacent to The Goddard School

1. Our son attends the Goddard School on the North side of the proposed development. We do not want my young son, who requires naps, to attend daycare adjacent to a noisy construction site. We also do not want him playing directly adjacent to a construction site that kicks up dust. We imagine many customers of the Daycare will feel similarly and could be driven away if precautions are not taken. It would be negligent for the community to allow development to hurt a well-established Springboro business already impacted by the pandemic.

We would propose leaving the existing treeline by the Goddard School to remain, expanding the buffer between the Assisted Living parking and the Goddard School, and constructing a temporary barrier such as a wall or berm between the school and construction site. Any grading and tree planting adjacent to the school should be completed early in the construction process then left alone except for care for the trees, sod planting or connecting to the existing road. This would be a hassle for the developer, but the buffer would help parents such as myself feel more assured about their child's well-being and may save an existing Springboro business.

Limiting the access of construction equipment via Anna Drive would also help decrease noise for the business.

2. The proposed fire station is very close to the existing Goddard School daycare. While this is located at a good access point to State Route 741, fire sirens are very loud. We would propose moving it further south to avoid permanent noise impacts to a building full of napping children.

Existing Park Capacity

1. Parking: The parking at North Park is inadequate in existing condition and would need to be expanded for any additional residential developments. During soccer season, concerts and other events held at the park exceed available parking causing visitors to fill up the street at the South end of the park as well.

2. Playground: Despite living adjacent to North Park, we will sometimes forgo taking our young son to play on the playground on nice days due to the possibility of him being stepped on during the busy days. Any additional residential development should include the development of an additional playground to avoid added overcrowding. The playground should be located within the development or by Gardner park to prevent further strain on parking and facilities at North Park.

3. Concert Venue: The city has worked to improve capacity at the North Park Amphitheater for the summer concert series. Even with these improvements, prior to COVID it was recommended to arrive significantly in advance to avoid sitting where a hill blocked the view and sound. Even with COVID there has been competition to obtain a spot that does not block sound and view. The concert is especially popular with seniors. The proposed development will further crowd the event with the addition of the assisted living facility. A further expansion of the seating area at the concert venue would be required to alleviate the added demand.

Existing Interstate Intersection Capacity

1. The I-75 exit at Austin Landing has been developed to allow for capacity for additional development. However, the Franklin/Springboro exit has not been improved. Although the new development would likely have easy access to the Austin Landing intersection, we suspect that many new residents would prefer to use the Franklin/Springboro intersection to avoid the litany of traffic lights at the Austin Landing intersection, or to travel south. Including this location in the traffic study to ensure continued capacity would be recommended.

Thank you for the opportunity to submit my thoughts on the proposed development. We look forward to hearing the city's thoughts on the submitted comments at the Zoning meeting on March 10th.

Thank you,

Zachary and Laura Palmer

Ann Burns

From: Robb Ervin <robbervin@gmail.com>
Sent: Monday, March 08, 2021 5:32 PM
To: Ann Burns
Cc: Rhonda Ervin
Subject: Concerns of Proposed Rezoning & Development of Easton Farms Property

Hi Ann,

My name is Robb Ervin, and my wife Rhonda and I would like to voice our concerns regarding the proposed rezoning and development for Easton Farms.

Rhonda was born and raised here in Springboro and called our community home here entire life. I moved here with my family in 1987 and have been proud to call Boro home ever since then. We both graduated from Springboro in 1990 and have been blessed enough to raise our boys in our hometown, have them educated in our old schools (sometimes by our former teachers and classmates) and ultimately to see them graduate from our alma mater. Springboro has always been, and will always be, a big part of our lives and we are proud of how our community has grown over the years.

We live at 124 Deer Trail Drive directly behind Gardner Park. Since we moved here in 2008 we have watched the development of residential townhomes and commercial/office development behind our home increase. We understand change and growth can often be a positive, but we believe the proposed rezoning and development of the Easton Farms property will actually have a negative impact on our community.

When we reviewed the proposed plan that includes more commercial and multi-family home development we feel this is just overkill and will not bring short or long term benefits to our community. What it will bring is a much larger development than was previously rejected not too long ago. A development that the community voiced strong opposition to once before. One that will have a negative ripple effect on our schools and the number of students attending, and likely raise a need for more schools and future levies. We already watch cars speeding up and down the cul de sac and around thru Paddock ever since the streets were connected. Based on the proposed connection of Fox Trail we believe it will only continue to get worse and be even more of a safety risk to the children on our street.

Ultimately we as taxpayers have not and will not support this proposed rezoning and development of Easton Farms. We have called Springboro home for almost all of our lives, but if the City chooses to ignore the voices of the residents of our community we may have to start to look elsewhere to call home.

Thank you for your time.

Best Regards,

Robb & Rhonda Ervin
robbervin@gmail.com
937-901-0724

Ann Burns

From: Don Cummings <dcummings2385@gmail.com>
Sent: Monday, March 08, 2021 10:18 PM
To: Ann Burns
Subject: Rejection of Easton Farm development proposal

Dear members of our Springboro Planning Commission,

Thank you for recognizing that we members of the Springboro Community that live adjacent to the property that is proposed to be overly developed, overly commercialized, overly densified and to have an overbearing effect on our homes and lifestyles, are entitled to voice our dismay.

It should come as no surprise to you all that we homeowners of Hunter Springs on Deer Trail Drive feel as if the proposed development of the "Easton Farm" is a direct attack upon the identity and characteristics of our lifestyles and neighborhood. So often I boast about the many privileges and comforts that we are provided while living on our "dead end" street. They are the very reasons that my wife and I decided to buy our current home in 2016 and to start our family here. Now, once again, these privileges and comforts of safety are being threatened by what seems to be most easily summed up as "greed and disregard".

In a world where people are losing sight of a community's responsibility to protect each other and our children, we like-minded friends and neighbors of Hunter Springs continue to value each other's safety and wellbeing. The intimacy that our families share with one another has largely been due to the fact that Deer Trail Drive is a dead end. There is an immeasurable amount of comfort in knowing each car and face that travels down our lightly traveled road, especially as my two and half year-old son chases a ball across the driveway. Enough can't be said about how important it is to us all to know that each driver behind the wheel of each vehicle has a high sense of ownership and responsibility to each other as neighbors, that would be immediately removed if our road was to be connected and accessible to any development on "Easton Farms". Especially one of this size and population density.

I would like you all to know that I am 100% against and non-budging on my stance and wish that Hunter Springs and Deer Trail drive continue to remain a dead end, and to not be connected to any proposed development through Fox Trail Drive.

Sincerely,
Donald Cummings
173 Deer Trail Drive

Ann Burns

From: Dan Boron
Sent: Tuesday, March 09, 2021 10:30 AM
To: Ann Burns
Cc: Chris Pozzuto
Subject: FW: City Planning Commission Meeting - March 10, 2021 - T. Price Concerns - REVISED 3.9.2021

Ann,

Revised comments from Tricia Price, I didn't check to see what's changed. Please add to those going to members of Planning Commission.

Dan

From: Tricia Price <tprice@sunesiscc.com>
Sent: Tuesday, March 9, 2021 8:24 AM
To: Dan Boron <danb@cityofspringboro.com>
Subject: FW: City Planning Commission Meeting - March 10, 2021 - T. Price Concerns - REVISED 3.9.2021

Dan, thought of a few more items last evening..... please have Ann pass this email on and not include the other one from yesterday.

Thank you.

Tricia L. Price
Project Manager

Sunesis Construction Co
2610 Crescentville Rd.
West Chester, OH 45069
O: 513-326-6000
C: 937-218-3638
tprice@sunesiscc.com
[web](#) | [facebook](#) | [insta](#) | [twitter](#)



"Improving Our Communities"

From: Tricia Price
Sent: Monday, March 8, 2021 1:58 PM
To: danb@cityofspringboro.com
Subject: City Planning Commission Meeting - March 10, 2021 - T. Price Concerns

Dan,

I would prefer to have a conversation rather than sending an email before the meeting but I know your schedule is tight. Below are a few comments I have for the meeting Wednesday night.

I live in the Springbrook area – 130 Allspice Court, I have thoughtful concerns as to why I do not want the development to go forth for our Springboro community. I joined the ARB to help maintain the integrity of our community, to keep our rich history and I am rethinking my appointment at this time to the ARB. I did some soul/general searching and found this property throughout the years has been looked at for development. I fully respect the Hall family and know they do have first, the right to sell their land, and second, know they have the best interest in our community, there is not doubt there. But as a resident, construction person, and member of the ARB, I have a hard time with this development moving forward as Dillin, LLC has presented in the 51 page presentation. The amount of potential issues this development brings to our Community I feel extremely out weigh the positive impacts.

- Interesting facts on Springboro – how will this potentially change our community if we continue to build, build, build, build? Dayton/Moraine/West Carrolton/Miamisburg/Springboro we will all be one big city!
 - <https://worldpopulationreview.com/us-cities/springboro-oh-population>
- Fire/EMS needs (future and current)
- Post office/delivery needs (future and current)
- Police needs (future and current)
- School system (future and current)
- Crime (potential) now we currently have none
- City workers/engineers (they are having trouble keeping up now with the amount of permits and inspections needed)
- Traffic (is unbearable now)
 - Has a Traffic Study been done? Plan/concern for Crockett Crossing being a feeder to Tamarack Trail with traffic/pedestrians and existing residents of the Springbrook area. The traffic is horrendous as it is now, what is the plan for safety/road concerns in the Springbrook/North Park area?
- Pump/sewage system (future and current). Our current wastewater treatment facility is outdated and to handle such a new development most likely would need its' own pump station.
- Water/erosion concerns for existing/new resident homes and not to mention the land itself and the creek running behind the property
- Water pressure issues potentially if no new station is installed
- What are the phases/timeframe for construction if this moves forward?
- Has there been a geotechnical engineering investigation report done?
- Has a Storm Water Quality Control plan been done along with wetland, road plans and storm water management plan assessed?
- What is the plan for informing resident/commercial residents they are in the direct flight pattern of the Dayton Wright Brothers Airport?
- With concern to C4.0 Grading Plan, what thought has been given to disturbing the existing farm land with erosion control measures and future preservation for the Easton Homestead that will be left with concern of run off, sewage/septic and positive drainage?
- What is phasing in regards to utilities, infrastructure planned and existing systems around the site. How will that be managed during the construction phases and how will they be addressed.
- I read thru the City of Springboro Land Use Plan dated January 27, 2009. Oddly enough, from 2009 to 2021 the concerns may still be valid.
- Reviewed the March 8, 2017 Planning Commission Work Session minutes as well. These residence still live in the addresses noted below. Wondering how they will feel about the development almost five years later?

still current resident Justin Wiedle, 164 Deer Trail Drive, agrees with Mr. Siefferlen. He believes this is a rule change in the middle of the game. The Deer Trail neighborhood is adjacent to Easton Farm at 2 per acre, and to look at as many as 12 units per acre, it would change the environment. He doesn't agree with the developer regarding the claim that the amenities would benefit his community. He likes the bike path idea, but he doesn't think it would add any value to their homes. He is concerned about the ability for resale if there is 10 years of construction in their backyard. He commented that there will be an additional strain on police force due to the added population, and the small amount of tax dollars that Springboro would gain would not be enough to cover additional officers. He said that it could be great for future citizens, but it would not benefit the current citizens. He stated the density issue has not really changed from the last meeting with only 17 units being omitted. He commented that safety issues weren't even discussed. He added that he wouldn't like to see the huge apartment buildings next to very nice houses.

still current resident David Beckham, 168 Deer Trail Drive, gave density comparisons with other apartment dwellings in the surrounding areas. He commented that it would have a huge impact on traffic, and a drive on 741 would add an additional 1,000-1,250 cars. He doesn't believe that the development will be mostly empty nesters and worried about the additional impact on the school system with additional students. He stated that North Park is already crowded, and if another 1,000 people are added, the park would become unenjoyable. He said that he moved here because he likes single family homes, and people who invest in the long-term, but you would not have that with this development. He believed there would be a change in the demographics and said that it would feel like the density would be doubled behind their homes and even though the developer was told last month to go back to the drawing board to eliminate units, they only cut out 18 units. He doesn't agree with a gas station or oil change facility at the entrance to the development and stated that drive-throughs are not within the characteristics of Springboro.

still current resident Chris Watt, 220 Tamarack Trail, stated that his main concern was with the traffic. He stated that he has a daughter that likes to play outside and he is concerned about the road connecting to Tamarack, and the additional traffic that will bring. He said that it is busy in the summer and even though it is a great amenity being able to live that close to the park, his house is the closest to Tamarack, and it would affect him the most. He said he understands the reasoning behind eliminating some of the traffic elsewhere, but added that Tamarack is already very busy.

still current resident Kathy Stonecash, 435 Tanglewood Drive, said that the points made about the apartments that are supposedly in Springboro were incorrect. She stated the Falls is in Miamisburg, Timber Creek is in Franklin, and the apartments on Gilpin is before we had a responsible zoning board. Easton Farms, North Park, and the Underground Railroad history are treasures in this community. We don't need any more national retailers, with chain after chain, and more people on top of more people because that is not why people move here. Her suggestion is to look into a federal tax incentive that Congress initiated for conservation of easement donations and it helps thousands of land owners conserve their land. She said it can help maintain the private property right and have federal tax benefits. She believed that the land should be kept as a nature center, park, or a working farm where children can go on field trips. She worried about the impact of the development in the future.

still current resident Regina Crane, 138 Timberside Court, wanted to point out that at North Park, we got rid of the little pond there because of mosquitoes. She is concerned about the water containments in the proposal where fountains are shown, but what if the fountains are not operating. She stated that traffic issues make it impossible to get through at certain times, and if 1,000 plus cars are added, it won't make it any more convenient for the citizens of Springboro, it will make it worse. She is concerned

March 8, 2017

City of Springboro Planning Commission Work Session
Meeting Minutes

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about water issues after the development of the land regarding the surrounding properties, and wondered if more basements would flood. She stated she likes walking through old Springboro with the mom and pop shops, and we don't need national retailers, because she thinks it is important to keep the economy local.

Rod Crane, 138 Timberside Court, commented that at Centerville's recent planning session, they happened to be celebrating that they got another piece of land that they could make a park, and they were so proud that they found land because it is so hard to come by. He stated that for the long term benefits of the people, this committee should think about that as well. He pointed out the land on S.R. 63 in Monroe where Butler Tech keeps and runs a farm that supports the community and wondered if there were something else we could do to compromise to share the value of the land.

Dan, I know you have been in our community a long time as well, but how much is enough? When and why do we need all this development to make us better? Will it bring enrichment to our community and lives? The packet the Dillin Group presented is all fluff, pretty skin on the development. But what about the bones, the structure, the heart of the

development, if the infrastructure is not put into place, this development could potentially cause issues in the years to come for the surrounding residence/businesses and land? I know all too well from my construction background, these developments are built and the City thinks income but little thought is given to the underground systems that carry the water, the sewage, the data/electric and fiber. What about the land we are disturbing and how will it be affected for 5, 10, 20, 25 years to come? We have a small swamp that lays in our back yard when it rains too much because the infrastructure was not installed properly from the condos behind my home, but no one wants to take ownership of it. Our streets in Springbrook still need to be repaved, our sidewalks are the original sidewalks installed in the 1970's/80's when the development was put in. It would be appreciated to do some maintenance on what we do have in our community, enhance what we have and not potentially bring greater issues to resolve.

I have literally been sick this week and even tearing up at points off and on this past week when I think about this development going in my back yard. I have lived in Springboro since 1973 and I realize change is inevitable, that is my life (construction), but I do not feel in my soul this development would better our Springboro community. I know this is a lot to take in and I am sure you are getting bombarded but I needed to share my concerns. I have spoken with a handful of residents in my neighborhood and they are extremely upset with the development as well. If you want to chat, I am always willing but know you are one busy man!!! ☺ Thanks Dan, I do appreciate all you and the commission do, I realize it is not an easy job.

Tricia L. Price
Project Manager

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Background Information & Staff Comments
City of Springboro Planning Commission Meeting
Wednesday, April 14, 2021, 6:00 p.m.

PUBLIC NOTICE:

Per Ohio Substitute House Bill 404 made effective November 22, 2020 in response to the COVID-19 state of emergency (Sec. 12), Section 12(A), the Springboro Planning Commission will conduct its Wednesday, April 14, 2021 Meeting via video conference at 6:00 p.m. EDT. Visit the City of Springboro website at <https://www.cityofspringboro.com/CivicAlerts.aspx?CID=6,1> for a link to connect to the meeting.

III. Agenda Items

A. Preliminary Review

Site Plan Review, 285 South Pioneer Drive, Tooling Zone, building addition

Background Information

This agenda item is a request for site plan review approval for an addition to the existing building that houses The Tooling Zone located at 285 South Pioneer Drive. As indicated in the submitted plans, the applicant for the property and business owner, Ferguson Construction, is seeking approval to construct an 11,400-square foot addition to the existing 37,180-square foot structure. The addition will be located on north side of the property on the rear/east side of the existing building in a portion of the property currently used for parking and circulation and building access.

The 3.1668-acre subject property has vehicular access by way of an easement to South Pioneer Drive to the west through property owned and occupied by Nations Roof. The property also has frontage onto West Factory Road on the east side of the property, however no access is available to the roadway. The majority of the property is located in the City of Springboro, however a 30-foot strip of land on the east side of the property is located in Franklin Township. The east property line coincides with the boundary between Springboro/Franklin Township on the west, and Clearcreek Township to the east.

Adjacent property to the northwest, west, and south have frontage and/or vehicular access from South Pioneer Drive and include Numed Pharma (265 South Pioneer), Nations Roof (275 South Pioneer), and a multi-tenant building located at 295-333 South Pioneer Drive. To the northeast is Master's Touch Lawn Care located at 2754 West Factory Road, and to the east are single-family residences on the east side of West Factory Road, all in Clearcreek Township.

The subject property is zoned ED, Employment Center District, a designation that permits light manufacturing, office, warehouse/distribution, and a number of other uses. The existing and proposed use are permitted in the ED District. The ED District also includes adjacent property to the north, west, and south. Property to the east located in Franklin Township is zoned R2, Two-Family Residential Zone, a zoning category that permits residential development up to three units per acre, as well as other uses. Rural Residence District; property to the east in Clearcreek Township is zoned SR-1, Suburban Residence Zone, a zoning category that permits residential development up to two units per acre when connected to a central sanitary sewer system, as well as other uses.

Staff Comments

City staff has the following comments regarding the site plan review application:

1. Setbacks and other design and development standards for the proposed addition to be set at the time of site plan review approval by Planning Commission as provided for in the ED District.
2. Provide a 25-foot multiuse easement along the east end of the property abutting West Factory Road.
3. Indicate how the proposed building exterior matches or compliments the existing building.
4. Following preliminary review by Planning Commission, provide specifications for proposed lighting. Also indicate if proposed lighting complies with maximum 3500° Kelvin color-temperature standard.
5. Provide a separate storm water pollution prevention plan.
6. Elevations and contours shall be based upon USGS datum and identify benchmark utilized.
7. Storm water calculations currently under review.
8. Provide revised final plan set incorporating staff comments and signed by owner or duly authorized officer.
9. Dimension parking lot from lot line.
10. Provide proposed sanitary sewer and water lateral information, if any.
11. Clearcreek Township Fire District has no comments at this time.

B. Preliminary Review

Rezoning, Easton Farm, 605 North Main Street, from R-1, Estate-Type Residential District, to PUD-MU, Planned Unit Development-Mixed Use, retail and residential development

C. Preliminary Review

General Plan, Easton Farm, 605 North Main Street, from R-1, Estate-Type Residential District, to PUD-MU, Planned Unit Development-Mixed Use, retail and residential development

Background Information

These agenda items are based on a request filed by Easton Farm Partners, Springboro, seeking rezoning and general plan approval for the Easton Farm, 103.31-acre located at 605 North Main Street. The applicant is requesting rezoning and general plan approval under the City's Planned Unit Development (PUD) process from R-1, Estate-Type Residential District, to PUD-MU, Planned Unit Development-Mixed Use. The applicant proposes to develop a mix of commercial, single-family and multi-family residential development. While included in the PUD-MU rezoning, the applicant proposes to retain the 16.82-acre historic farmstead located on the west side of the property.

The proposed rezoning/general plan appears as two separate items on the Planning Commission work session agenda. The first stage of the PUD process, rezoning and general plan review and approval, will involve two separate recommendations to City Council, and later two separate pieces of legislation considered by City Council.

These agenda items appeared on the March 10th Planning Commission agenda for preliminary review. As with the March 10th review, no formal approval has been requested or will be made at

the April 14th Planning Commission meeting. The applicants have submitted a revised design guidelines booklet for the April 14th review by Planning Commission. The background information below and staff comments reflect that change, however comments regarding the general plan map and other details remain largely the same from the March 10th meeting review. City staff anticipates that the applicants will submit revised plans for a future Planning Commission meeting, as early as the May 12th meeting.

The subject property is located southwest of the intersection of Anna Drive/Lytle-Five Points Road and North Main Street. The subject property is presently farmed and includes two single-family residential units on the west side of the property within a historic farmstead. Vehicular access is presently provided by a single driveway from North Main Street.

The subject property is presently zoned R-1, Estate-Type Residential District. The R-1 District allows residential development at a density of 2 dwelling units per acre on 20,000 square foot lots. The R-1 District was applied to this property in 2015 as part of the implementation of the current Planning & Zoning Code.

The applicant has requested rezoning to PUD-MU, Planned Unit Development-Mixed Use, with three components: mixed-use, multi-family, and residential indicated on sheet C1.0 in the submitted materials.

Rezoning together with general plan review and approval are the first step in the three-step PUD review and approval process. Approval by both Planning Commission and City Council are required. Final development plan, similar to the City's site plan review process, review and approval by Planning Commission is the second stage in the process. Final development plan may be submitted in a number of sections in conjunction with a site's incremental development. Record plan review and approval by both Planning Commission and City Council is the last step in the PUD approval process, this allowing for the subdivision of lots and the dedication of right-of-way and open spaces. As with final development plans, record plans may be submitted in a number of sections as the development is completed.

Adjacent land uses include single-family residential development to the northwest within the Hunter Springs subdivision that includes homes on Deer Trail Drive. Open space in the form of the City of Springboro's Gardner Park, office and retail development to the north within the Village Park PUD-MU, Planned Unit Development-Mixed Use, retail development to the northeast within the Marketplace of Settlers Walk shopping center, a part of the Settlers Walk PUD, northeast of the intersection of Lytle-Five Points Road and North Main Street; retail and office development to the east on the east side of North Main Street; and retail and office development to the south including a day care facility and real estate office. To the south, residential development including condominiums within Springbrook Commons/Spice Rack subdivision, and the City of Springboro's North Park. To the west is single-family residential within the Tamarack Hills and Royal Tamarack subdivisions.

Adjacent zoning includes to the north R-2, Low-Density Residential District corresponding to the Hunter Springs subdivision, and PUD-MU corresponding to the Village Park development. PUD to the northwest associated with the Settlers Walk PUD. LBD, Local Business District, O, Office District, and O-R, Office-Residential District, to the east associated with the existing pattern of retail and office development. O-R District to the south, and transitioning to PUD and R-3, Medium-Density Residential District, associated to the condominium development to the south, and then transitioning to R-2 District corresponding to the single-family area along Tamarack Trail and into

North Park. This R-2 District pattern continues to the west and the Tamarack Hills/Royal Tamarack subdivisions.

The *Springboro Land Use Plan*, adopted by City Council in April 2009, includes recommendations for the long-range development of the community. It is divided into 16 policy areas that make specific recommendations for smaller portions of the community and are grouped together because of proximity, land use patterns, date of development and other general characteristics. Policy Area #3, North SR 741 Corridor, includes the subject area and land including Hunter Springs, Village Park, the non-residential portions of Settlers Walk and retail/office areas on east side of North Main Street. Preferred Land Uses identified in the plan include convenience retail, personal service, retail uses limited to a maximum of 75,000 square feet in floor area, among other uses. Residential development is preferred at an overall density of 6-8 dwelling units per acre.

The applicant's General Plan concept drawing proposes the following:

- An 18.75-acre mixed use commercial component on the northeast corner of the property fronting North Main Street. This component includes the following:
 - A 113-unit independent living facility.
 - An 84,400-square foot assisted living/memory care facility.
 - Outparcels for a fire station, restaurants and other retail facilities totaling 16,800 square feet.
 - Two commercial buildings including 37,900 square feet of space.
- A 10.12-acre multi-family residential component on the southeast corner of the property fronting North Main Street that includes multiple buildings including 324 apartments, a 9,500-square foot restaurant, and 3.0 acres of open space comprised of storm water detention ponds.
- A 74.40-acre residential component covering the remainder of the property including the following:
 - Retaining the historic farmstead including 2 homes and preserving most farm buildings.
 - 24 townhomes.
 - 251 single-family lots most of that are served by garages accessed by private drives. The site of lots proposed for this large area ranges from large lots adjacent to the Hunter Springs neighborhood on the north end of the component to smaller lots to the south.
 - 12.82 acres of open space including two small parks, storm water detention ponds, a linear park, and a town green-type open space abutting the mixed use and multi-family residential component.

For proposed residential development areas, a gross density of 6.83 dwelling units per acre (577 dwelling units on 84.52 acres) is proposed.

Access to the proposed development would be provided by an extension of the existing Anna Drive through the development south to Tamarack Trail near the entrance to North Park, an extension of Fox Trail Drive from the Hunter Springs subdivision south into the interior of the site, and an access point onto North Main Street from the proposed Easton Farm Boulevard.

Staff Comments

City staff has the following comments regarding the proposed rezoning/general plan application reviewed at the March 10th Planning Commission meeting:

1. Rename the mixed-use component of the PUD to commercial or other to avoid confusion with the overall rezoning request, and provide a component to address historic farmstead design/development standards and proposed permitted uses.
2. Revise full-color illustrative plan to match b/w plan proposal.
3. Revise the submittal for the next review to include the following for each component area: design and development standards including but not limited to setbacks, building heights, dwelling unit sizes, lot coverage, and a list of land uses proposed for each component area. As a companion to this information, provide a color-coded version of the general plan.
4. Easton farmstead is listed as open space. Unless the farm is available for use as common space it should not be included in the calculation.
5. An additional 5.47 acres of open space is needed to satisfy the 25% minimum open space requirement for residential PUDs.
6. Indicate who will manage open spaces proposed in the development.
7. For trails proposed on common areas, if any, include no restrictions for their use by any person with the exception of areas specifically set aside for the members of an association such as pool areas.
8. The trail along Anna Drive to be designed to meet minimum standards for width, turning radius, and to avoid obstructions.
9. City to review Anna Drive extension to intersection with Tamarack Trail and North Park entrance.
10. Indicate proposed phasing including road connections and other improvements with surrounding developments.
11. Provide typical plans for buildings indicating materials, and other details for each component in order to determine concurrence between general plan's concept and specific building plans when final development plans are prepared for review and approval by the Planning Commission.
12. Sidewalks (or trail) to be located on all streets, both sides. This should be verified. The general plan is difficult to tell and a statement would address it.
13. Central mailbox units need to be located on general plan (and approved by the post office). Include this information in the design guidelines.
14. Flag lots not permitted (lots off of cul-de-sac on Red Hawk View). Remove flag lots or extend roadway.
15. Road name proposals to be reviewed by City Engineer in consultation with the police and fire departments.
16. Anna Drive to extend off of existing Anna Drive, and not relocated as shown.
17. Provide Noel Drive typical section.
18. Add a possible roundabout at the Tamarack Trail connection.
19. Traffic Study to be submitted for review and approved prior to final approval of General Plan/Rezone by planning commission.
20. Right-of-way along North Main Street to be dedicated per city specifications.
21. No construction access permitted from Tamarack Trail or Fox Trail Drive.
22. Engineering design details to be reviewed at the Final Development Plans stage, including but not limited to utility design, storm water management plan including detention/retention design, and roadway design.
23. Road intersections to be at 90 degree angles.

24. HOA documents need to be created for review.
25. The Clearcreek Fire District has no comments at this time.

City staff has the following comments regarding the proposed design guidelines booklet, as revised for the April 14th Planning Commission meeting, included in the rezoning/general plan submission:

1. City staff recommends a review with applicants on the intent of the design guidelines. Are they covenants or are these intended to be incorporated into the general plan approval?
2. Page 8, in the table, the minimum open space is 25% for the residential portion only.
3. Page 10-11, utility easements are to be located outside of the right-of-way in a 10-foot utility easement, and not within the proposed right-of-way. (page 10, (c); page 11 (e)).
4. Page 13, for loading/unloading provisions, cross-reference City Code Section 660.19 for hours of operation for non-residential areas. Design standards may be more restrictive, if desired.
5. Page 14, explain proposed open space area requirements. Are these setbacks?
6. Page 16, (e) Building Materials – Prohibit the use of plywood, unadorned corrugated siding, vinyl siding and plastic panels all together.
7. Page 18, (i) - Clarify Enclosed unoccupied building elements vs purely decorative unoccupied elements.
8. For landscaping provisions on page 19, provide cross reference to City requirements in Chapter 1280, Landscaping.
9. Page 22, for exterior lighting, provide cross reference to City requirements in Chapter 1273, Exterior Lighting.
10. Page 23, 8. (a) – Prohibit the use of chain-link fencing with inserted slats, or plastic coated walls and/or support wood posts all together.
11. Page 26, Are the proposed parking standards intended to supersede those of the City?
12. Page 27 (d) – Increase the minimum setback for off street parking along SR 741
13. Provide Public Access Easements over service ways.
26. For the table on page 31, define in a map where Village Center, Neighborhood Lane, etc., are in this proposal.
27. For the same table, for residential areas, provide a table showing minimum setbacks, minimum lot size (SF), minimum lot width, and minimum dwelling size for each type.
28. For the same table, Footnote 1 states front porch encroachment up to 5 feet maximum is permitted. This should be removed and the table should reflect actual need/want. For which residential area does footnote 1 relate?
14. Page 33, 11. (a) – Prohibit the use of chain link, barbed wire, or plain wire mesh, or rough-textured/timber or “fortress style” wood fences.
15. Beginning on page 34, explain Residential Typologies beginning on this page. Are these going to be supported by other design metrics?
16. Page 42 – Increase trail width from 8 feet to 10 feet.

The information contained in this report is based on material provided to the City of Springboro as of Monday, April 5, 2021 at 5:00 p.m.

APPLICATION—SITE PLANS, SUBDIVISIONS & RECORD PLANS

CITY OF SPRINGBORO PLANNING COMMISSION

☒ SITE PLAN ☐ REVISION TO APPROVED SITE PLAN ☐ CONCEPT PLAN ☐ PRELIMINARY SUBDIVISION ☐ RECORD PLAN

The undersigned requests the approval identified above. Site Plan Review approvals subject to expiration provided for in Section 1284.18 of the Planning and Zoning Code. For all approvals under this application, it is understood that it shall only authorize the approval described in this application, subject to any conditions or safeguards required by the Planning Commission, and/or City Council.

- ☐ Owner
☒ Agent
☐ Lessee
☐ Signed Purchase Contract

APPLICANT NAME:

Brandon Rose

Address

400 Canal St.

Sidney, OH 45365

Telephone No.

(937) 498-2381

Fax No.

(937) 498-2243

Email Address

brose@ferguson-construction.com

PROPERTY OWNER NAME (IF OTHER):

WHO DEY INVESTMENTS, LLC

Address:

198 Poplar Grove Ct.

Springboro, OH 45066

Telephone No.

(937) 286-2688

Property Address or General Location:

285 S. Pioneer Blvd., Springboro, OH 45066

Parcel Number(s):

0419276033

Zoning District:

"ED"-Employment Center District

Proposed Use:

Continued use of the property/facility for manufacturing tools. New 76'x150' building addition at the north-east corner of the property and a new parking area to the South-west.

The applicant or representative who is authorized to speak on behalf of the request must also be present at all meetings.

Brandon Rose

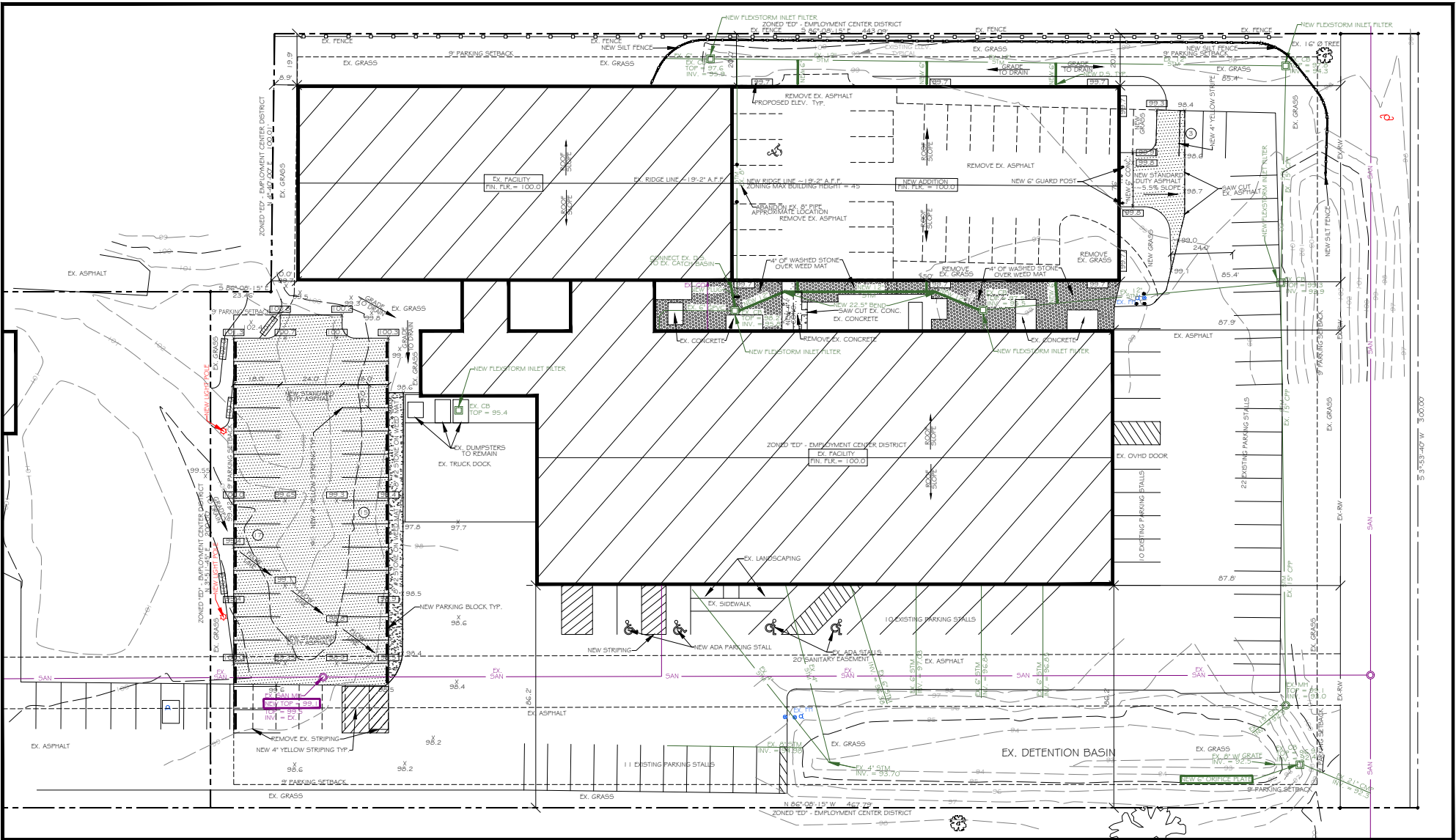
(Signature of Applicant and/or Agent)

3-26-21

(Date)

Brandon Rose

Printed Name

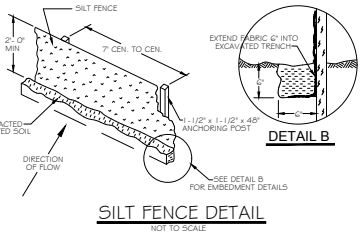
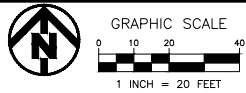


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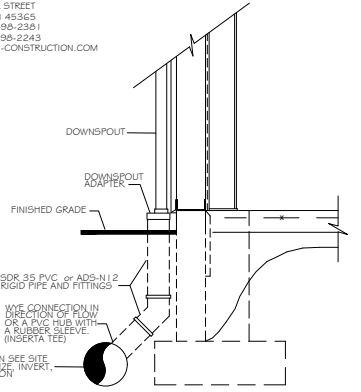
OWNER
THE TOOLING ZONE
STEVE HANS (PRESIDENT & CEO)
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SPRINGBORO, OHIO 45066
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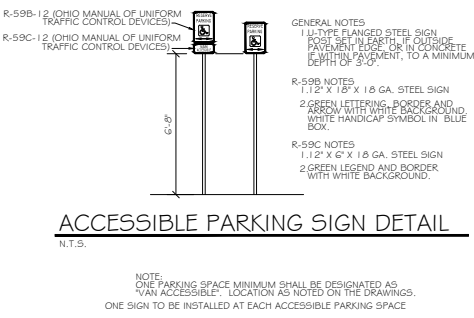
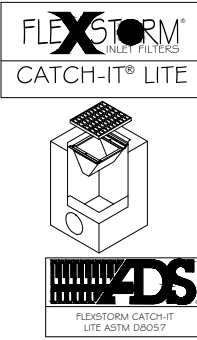
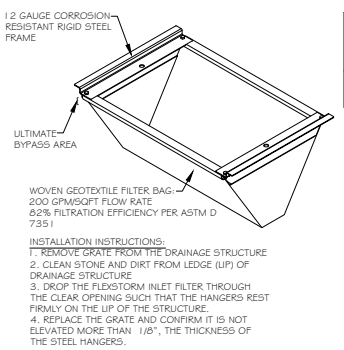
SITE PLAN
SCALE 1" = 20'-0"
PROPOSED GRADES ARE FINISH ASPHALT OR FINISH GRADE ELEVATIONS



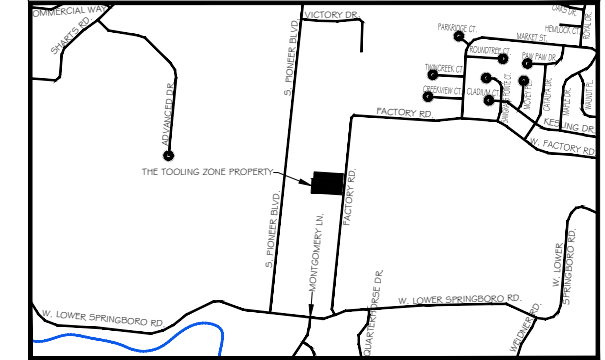
SILT FENCE DETAIL
NOT TO SCALE



DOWNSPOUT DETAIL



ACCESSIBLE PARKING SIGN DETAIL
N.T.S.



VICINITY MAP

GENERAL NOTES

EROSION CONTROL

SITE CONTRACTOR SHALL TAKE APPROPRIATE MEASURES TO CONTROL SOIL EROSION AND SEDIMENTATION THROUGHOUT THE PROJECT. THIS SHALL INCLUDE THE USE OF STRAW BALES, SILT FENCING, #2 STONE AT CONSTRUCTION ENTRANCES AND ANY OTHER METHODS TO PREVENT EROSION OR SEDIMENT FROM LEAVING THE CONSTRUCTION SITE. ANY DIRT OR DEBRIS TRACKED ONTO THE ROADWAY SHALL BE REMOVED WITHIN 24 HOURS OR LESS.

EROSION CONTROL

SITE CONTRACTOR SHALL TAKE APPROPRIATE MEASURES TO CONTROL SOIL EROSION AND SEDIMENTATION THROUGHOUT THE PROJECT. THIS SHALL INCLUDE THE USE OF STRAW BALES, SILT FENCING, #2 STONE AT CONSTRUCTION ENTRANCES AND ANY OTHER METHODS TO PREVENT EROSION OR SEDIMENT FROM LEAVING THE CONSTRUCTION SITE. ANY DIRT OR DEBRIS TRACKED ONTO THE ROADWAY SHALL BE REMOVED WITHIN 24 HOURS OR LESS.

ALL DETENTION BASING SHALL BE USED AS SEDIMENTATION BASING DURING THE CONSTRUCTION PERIOD. THE BASING MUST BE INSPECTED ONCE A WEEK AND AFTER EVERY RAINFALL OR WHENEVER CITY STAFF DEEMS NECESSARY. CAREFULLY CHECK OUTLETS AND MAKE TIMELY REPAIRS AS NECESSARY. REMOVE SEDIMENT FROM THE BASIN WHEN APPROXIMATELY HALF OF THE STORAGE VOLUME HAS BEEN FILLED.

SITE PREPARATION AND GRADING

REMOVE EXISTING ASPHALT AND STRIP TOPSOIL FROM BUILDING AND PAVEMENT AREAS, STOCK PILE AND REESPREAD. AFTER THE COMPLETION OF STRIPPING OPERATIONS, THE EXPOSED SUBGRADE AREAS SHALL BE PROOFROLLED WITH SUITABLE HEAVY EQUIPMENT (20-30 TON LOADED DUMP TRUCK). GRADE AND MAKE REQUIRED CUTS AND COMPACTED FILLS FOR NEW ELEVATIONS SHOWN. HAUL EXCESS MATERIAL OFF-SITE AND HAUL IN ENGINEERED FILL AS NEEDED.

THE CONTRACT MUST BE ADJUSTED FOR ANY COST INCURRED IN MOVING, REROUTING OR REPAIRING BELOW GRADE ITEMS SUCH AS TANKS AND PIPING ENCOUNTERED DURING CONSTRUCTION UNLESS SPECIFICALLY NOTED ON CONTRACT DRAWINGS. ROCK EXCAVATION AND REMOVAL IS EXCLUDED. ROCK EXCAVATION IS DEFINED AS ROCK WHICH CANNOT BE REMOVED BY ORDINARY MEANS. (TRACK HOE OR RUBBER TIRE BACK HOE)

BUILDING PAD

NEW FLOOR SLAB IS TO BE 6" CONCRETE WITH (1) LAYER OF 42# WWF ON 4" 304 CUSHION FILL ON COMPACTED SUBGRADE. SUBGRADE IS TO BE COMPACTED TO A MINIMUM OF 100% STANDARD PROCTOR AND APPROVED BY A TESTING LABORATORY.

SITE CONCRETE

ALL OUTSIDE HVAC UNITS SHALL BE ON A 6" CONCRETE PAD.

SITE CONCRETE SHALL BE 6" THICK (4,000 PSI - AIR ENTRAINED) WITH ONE LAYER OF 42# WWF ON 4" OF COMPACTED 304 AGGREGATE ON COMPACTED SUBGRADE.

UTILITIES

EXISTING UTILITIES ARE SHOWN IN THEIR APPROXIMATE LOCATION ACCORDING TO THE BEST AVAILABLE DATA. THE CONTRACTOR WILL BE RESPONSIBLE FOR LOCATING THEM IN THE FIELD PRIOR TO CONSTRUCTION AND WILL BE RESPONSIBLE FOR ANY DAMAGE DONE TO THEM. CONTRACTOR TO CONTACT OHIO UTILITIES PROTECTION SERVICE (1-800-362-2764) 48 HOURS PRIOR TO CONSTRUCTION.

COMPACTED GRANULAR BACKFILL SHALL BE INSTALLED ON ALL PIPING UNDER BUILDINGS, PAVEMENT AREAS AND ANY PIPING WITHIN 5 FEET OF EDGE OF PAVEMENT.

AT CROSSING THE WATERLINE SHALL HAVE A MINIMUM VERTICAL DISTANCE OF 18" FROM STORM AND SANITARY SEWERS. ALSO ONE FULL LENGTH OF WATER MAIN SHALL BE LOCATED SO THE JOINTS ARE AS FAR FROM THE STORM SANITARY SEWERS AS POSSIBLE.

ALL WORK MUST COMPLY WITH CITY STANDARDS.

STORM WATER MANAGEMENT

NEW STORM PIPING TO BE ADS N-12 18" ID PIPE OR EQUAL AND INSTALLED PER PIPE MANUFACTURERS SPECIFICATIONS.

FURNISH AND INSTALL A COMPLETE SITE DRAINAGE SYSTEM CONSISTING OF PIPING AS SHOWN.

DOWNSPOUTS TO BE CONNECTED TO SITE DRAINAGE SYSTEM USING PVC BOOTS, PVC PIPE AND PVC 45° FITTINGS. VERIFY DOWNSPOUT LOCATIONS ON THE BUILDING ELEVATION DRAWINGS.

THE EXISTING DETENTION BASIN IS TO BE UPDATED TO MEET THE REQUIREMENTS FOR SEDIMENT CONTROL AND THE ADDITIONAL STORM WATER DETENTION REQUIREMENTS.

ASPHALT NOTES

STANDARD ASPHALT PAVEMENT SHALL CONSIST OF 1-1/2" OF ASPHALT SURFACE COURSE TYPE 1, PG 64-22 ON 1-1/2" OF ASPHALT INTERMEDIATE COURSE, TYPE 2, PG 64-22 ON 7" OF COMPACTED 304 AGGREGATE ON COMPACTED SUBGRADE.

PAVING DESIGN IS BASED UPON A MAXIMUM DEFLECTION OF THE SUBGRADE SOILS OF 1/2" DURING PROOF ROLLING OPERATIONS. ADDITIONAL COSTS FOR REMOVAL OF UNSTABLE SOILS FOUND DURING PROOF ROLLING OPERATIONS MUST BE ADDED TO CONTRACT BY CHANGE ORDER.

CLEANING OF ASPHALT AND ITEM 407 TACK COAT IS REQUIRED IF MORE THAN (3) WORKING DAYS PASS BETWEEN LAYING OF ASPHALT SURFACE COURSE TYPE 1 AND ASPHALT INTERMEDIATE COURSE TYPE 2. (0.1 GAL / SQ. YD.).

ASPHALT CONTRACTOR IS RESPONSIBLE FOR SAW-CUTS AND A.C. SEALER WHERE PROPOSED ASPHALT MEETS EXISTING ASPHALT.

PARKING LOT STRIPING IS TO RECEIVE ONE COAT OF YELLOW PAINT 4" WIDE AS PER LAYOUT ON SITE PLAN. (BY ASPHALT CONTRACTOR)

HANDICAP PARKING

PROVIDE SYMBOL OF ACCESSIBILITY PARKING SIGNS AT ALL HANDICAP ACCESS/PARKING SPACES PER DBC SECTION 111.0 AND ADAAG REQUIREMENTS. UNIVERSAL PARKING SPACE DESIGN USED PER ADAAG A4.6.3 REQUIREMENTS.

SEEDING AND LANDSCAPING

AREAS DISTURBED BY CONSTRUCTION SHALL BE SEEDDED, FERTILIZER, STRAW, ROCK PICK-UP, MULCH & TACKIFIER, ETC. REQUIRED)

LANDSCAPING SHALL BE BY THE OWNER.

CONTRACTOR SHALL SEED OR MULCH ANY DISTURBED AREAS WITHIN 30 DAYS OF FINAL GRADING.

SEEDING CONTRACTOR IS RESPONSIBLE FOR CREATING PROPER CONDITIONS TO ALLOW FOR OPTIMIZATION OF GRASS SEED GERMINATION AND CONTINUED GROWTH. SEEDING CONTRACTOR WILL BE REQUIRED TO DO ROCK PICK-UP AND VERIFY ADEQUATE FALLOUT OF TOPSOIL, SEED, FERTILIZER, MULCH & TACKIFIER AND/OR STRAW AS REQUIRED. SEEDING CONTRACTOR IS TO NOTIFY GENERAL CONTRACTOR ANY CONDITIONS NOT CONDUCTIVE TO ENSURING ESTABLISHED GRASS. UPON COMPLETION AND AFTER INITIAL WATERING THE OWNER WILL BE RESPONSIBLE FOR WATERING AS REQUIRED.

PARKING LOT LIGHTING

THE ELECTRICIAN SHALL COMPLY WITH ALL REQUIREMENTS OF THE SPRINGBORO ZONING CODE INCLUDING CHAPTER 1273 "EXTERIOR LIGHTING"

- PARKING LOTS AND ASSOCIATED CIRCULATION AREAS MINIMUM ILLUMINATION OF 0.4 FOOT-CANDELES AND MAXIMUM ILLUMINATION OF 6.0 FOOT-CANDELES.
- LIGHT FIXTURES SHALL NOT BE MOUNTED IN EXCESS OF THE MAXIMUM HEIGHT LIMITATION OF THE DISTRICT IN WHICH THEY ARE LOCATED. FOR LIGHTING IN RESIDENTIAL DISTRICTS AND FOR USES ADJACENT TO A RESIDENTIAL DISTRICT OR USE, LIGHT FIXTURES SHALL NOT BE MOUNTED IN EXCESS OF 25 FEET ABOVE GRADE. FIXTURE HEIGHT SHALL BE MEASURED FROM THE GRADE OF THE ILLUMINATED SURFACE TO THE TOP OF THE FIXTURE.
- ALL FIXTURES SHALL BE FULLY SHIELDED.
- NO DIRECT LIGHT SOURCE SHALL BE VISIBLE AT THE PROPERTY LINE (ADJACENT TO RESIDENTIAL) AT GROUND LEVEL.
- MAXIMUM ILLUMINATION AT THE PROPERTY LINE SHALL NOT EXCEED 0.5 FOOT-CANDELES.

OFF-STREET PARKING CALCULATION

EXISTING BUILDING AREA = 37,180 SQ. FT.
NEW PROPOSED ADDITION AREA = 11,400 SQ. FT.
NEW TOTAL BUILDING AREA = 48,580 SQ. FT.
NEW TOTAL INDUSTRIAL AREA = 43,280 SQ. FT.
NEW TOTAL OFFICE AREA = 5,000 SQ. FT.

OFF-STREET PARKING STALLS REQUIRED
OFFICE AREA = 5,000 SQ. FT. (1 STALL / 300 SQ. FT.) = 16.67 STALLS = 17 STALLS
INDUSTRIAL AREA = 43,280 SQ. FT. (1 STALL / 750 SQ. FT.) = 57.71 STALLS = 58 STALLS
TOTAL OFF-STREET PARKING REQUIRED = 17 STALLS (OFFICE) + 58 STALLS (INDUSTRIAL) = 75 STALLS
TOTAL OFF-STREET PARKING PROVIDED = 88 STALLS (INCLUDES 4 ADA (1 VAN ACCESSIBLE))

Revisions		
#	Date	Description

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BUILDING ADDITION FOR THE TOOLING ZONE
WARREN COUNTY
285 S PIONEER BLVD
SPRINGBORO, OHIO 45066



Sidney, Ohio
400 Canal Street
Sidney, OH 45365-0726
Phone: (937) 498-2381
Fax: (937) 498-2243

Dayton, Ohio
2201 Embury Park Road
Dayton, OH 45414-5544
Phone: (937) 274-1173
Fax: (937) 277-1379

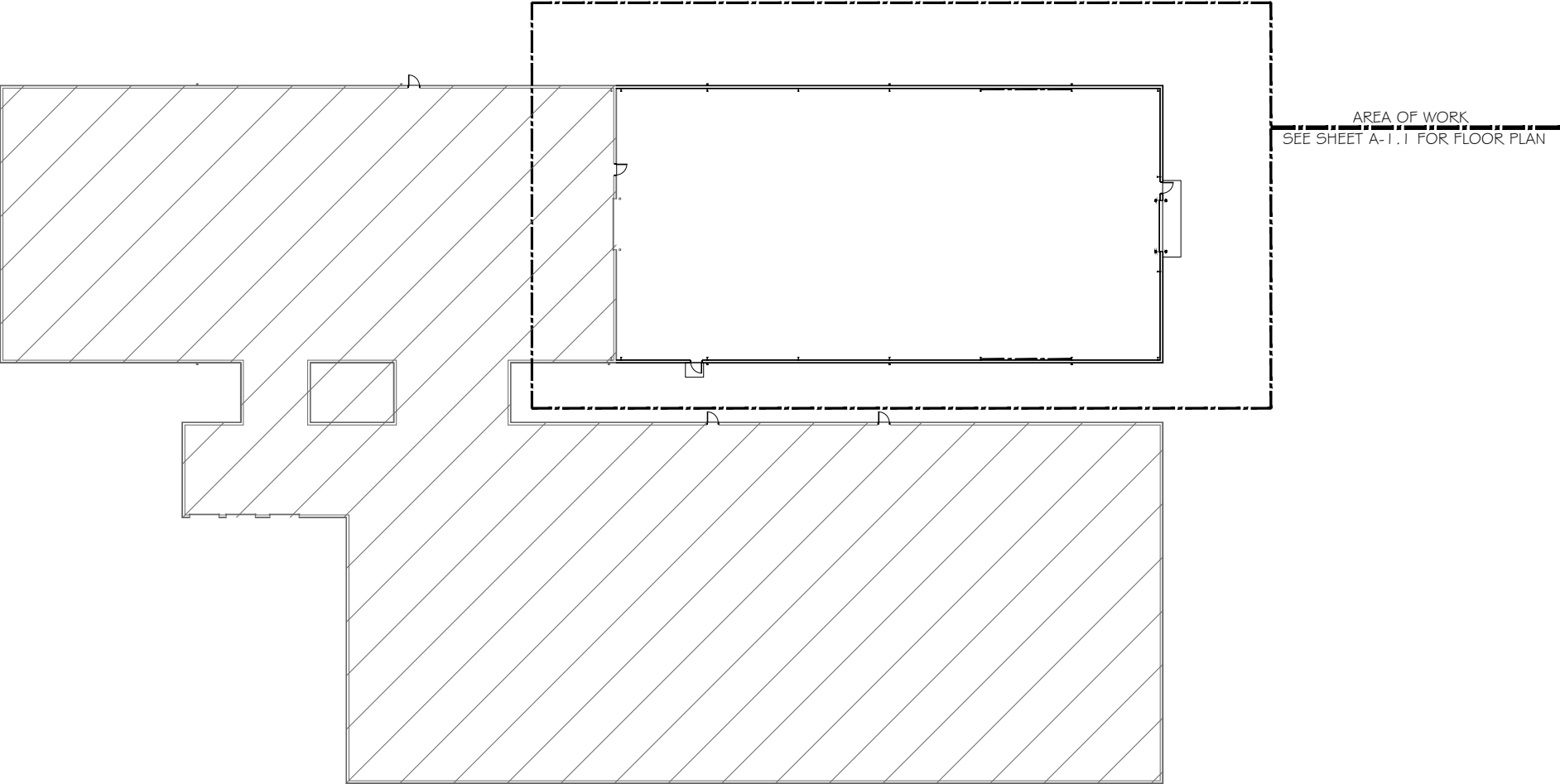
Columbus, Ohio
3595 Johnny Appleseed Ct.
Columbus, OH 43231
Phone: (614) 876-8496
Fax: (614) 876-8155

Indianapolis, Indiana
7157 W. 200 N.
Greenfield, IN 46140
Phone: (317) 477-3615
Fax: (317) 477-3630

Job Number	21-16259
Date	3/25/21
Drawn By	BJR
Checked By	DJW

SITE PLAN

C-1.1



OVERALL FLOOR PLAN SCALE : 1/16" = 1'-0"



Revisions

#	Date	Description

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PRELIMINARY

BUILDING ADDITION
FOR

THE TOOLING ZONE

WARREN COUNTY
285 S. PIONEER BLVD.
SPRINGBORO, OHIO 45066

FERGUSON
CONSTRUCTION CO.
www.ferguson-construction.com

Sidney, Ohio
400 Canal Street
Sidney, OH 45365-0726
Phone: (937) 498-2381
Fax: (937) 498-2243

Dayton, Ohio
2201 Embury Park Road
Dayton, OH 45414-5544
Phone: (937) 274-1173
Fax: (937) 277-1379

Columbus, Ohio
3595 Johnny Appleseed Ct
Columbus, OH 43231-4985
Phone: (614) 876-8496
Fax: (614) 876-6155

Indianapolis, Indiana
7157 W. 200 N.
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Phone: (317) 477-3615
Fax: (317) 477-3630

Job Number21-16259

Date3/26/21

Drawn ByJTS

Checked ByJWM

OVERALL FLOOR
PLAN

A-1.0

DOOR AND FRAME SCHEDULE																
DOORS									FRAMES			HARDWARE			COMMENTS	
MARK	NO. OF DOORS	TK	W	HT	ELEV	MATL	GL	LVR	ELEV	MATL	DTL	THRESHOLD	LOCK	CLSR		WEATHER STRIPPING
1A	1	1 3/4"	3'-0"	7'-0"	A	HM	-	-	A	HM	1	YES	SEE DOOR NOTE # 1	YES	YES	SEE DOOR NOTE # 1
1B	1	-	14'-0"	14'-0"	B	STL	-	-	-	STL	2	SEE FRAME DETAIL 3	BY DIE MANU.	ELEC	YES	SEE DOOR NOTE # 1
1C	1	1 3/4"	3'-0"	7'-0"	A	HM	-	-	A	HM	1	YES	SEE DOOR NOTE # 1	YES	YES	SEE DOOR NOTE # 1

DOOR NOTES

- AT THE BOTTOM OF EXTERIOR H.M. DOORS PROVIDE A CONCEALED DOUBLE SEALING SWEEP MADE OF SYNTHETIC MATERIAL, MODEL NO. F5074 AS MANUFACTURED BY FAS-SEAL (OR EQUAL). WEATHERSTRIP TO BE RECESSED IN THE BOTTOM CHANNEL OF THE DOOR.
- ALL EXTERIOR HOLLOW METAL DOORS INSTALLED IN PRE-ENGINEERED BUILDING WALLS ARE TO BE FURNISHED BY THE PRE-ENGINEERED BUILDING MANUFACTURER.
- ALL EXTERIOR HOLLOW METAL DOORS SHALL INCLUDE MORTISE LOCKS AND LEVER HANDLES, CYLINDER LOCK AND KEYING BY HARDWARE SUPPLIER.

HARDWARE NOTES

HARDWARE FOR ALL EGRESS DOORS SHALL MAINTAIN THE DOORS READILY OPENABLE FROM THE SIDE FROM WHICH EGRESS IS TO BE MADE WITHOUT THE USE OF A KEY OR SPECIAL KNOWLEDGE OR EFFORT. DRAW BOLTS, HOOKS, AND OTHER SIMILAR DEVICES SHALL BE PROHIBITED ON ALL EGRESS DOORS. THE USE OF MANUALLY OPERATED FLUSH BOLTS OR SURFACE BOLTS IS PROHIBITED. REFER TO SECTION 1003.0 GENERAL MEANS OF EGRESS OF THE OHIO BUILDING CODE.

ALL DOORS TO BE FURNISHED W/ APPROVED HANDICAP ACCESSIBLE HARDWARE W/ LEVER TYPE HANDLES AND HANDICAPPED TYPE CLOSERS.

ALL LOCKS TO BE ON A MASTER-KEYED SYSTEM APPROVED BY THE OWNER.

THE MAXIMUM FORCE REQUIRED FOR PUSHING AND/OR PULLING OPEN AN INTERIOR DOOR EQUIPPED WITH AN AUTOMATIC DOOR CLOSER SHALL BE 5 POUNDS PER ICC A 117.1 SECTION 404.2.8.

ROOM FINISH SCHEDULE												
NO.	ROOM NAME	FLOOR	BASE	WALLS				WAINSCOT		CEILING		COMMENTS
				NORTH	EAST	SOUTH	WEST		HT.		HT.	
	MANUFACTURING	EXP. CONC.	-	EXP. INSUL.	EXP. INSUL.	EXP. INSUL.	EXIST.	LP	8'-0"	EXP. INSUL.	VARIES	

ROOM FINISH LEGEND

ALL INTERIOR FINISHES MUST COMPLY WITH O.B.C. CHAPTER 8.

EXIST - EXISTING ROOM FINISH MATERIAL TO REMAIN. REWORK AND/OR PATCH EXISTING WALLS, FLOORS, ETC., AS REQUIRED.

EXP. CONC. - EXPOSED CONCRETE

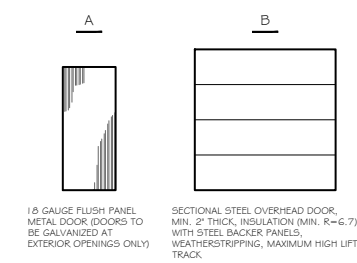
EXP. INSUL. - EXPOSED INSULATION AND STRUCTURAL

LP - 28 GA. METAL LINER PANEL

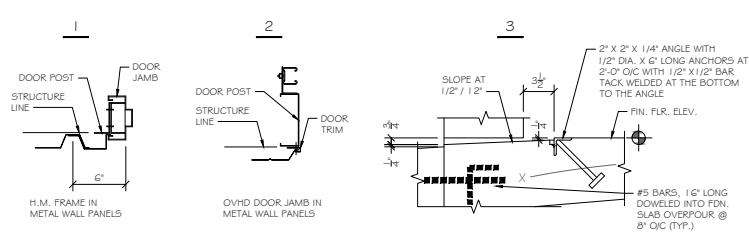
PAINTING NOTES

- ONE COAT PRIMER AND ONE COAT ENAMEL ON ALL OVERHEAD DOOR JAMBS, HEADS AND SILL ANGLES, BUMPER POSTS AND OTHER EXPOSED MISCELLANEOUS METALS.
- ONE COAT PRIMER AND ONE COAT INDUSTRIAL GRADE ACRYLIC LATEX ON ALL HOLLOW METAL DOORS AND FRAMES.
- ALL EXPOSED STRUCTURAL STEEL TO RECEIVE ONE REGULAR PRIME COAT SHOP APPLIED AND ONE COAT SEMI-GLOSS SHERWIN-WILLIAMS DRY FALL SPRAY LATEX (OR EQUAL) IN COLOR TO MATCH EXISTING.
- EXPOSED SERVICE PIPING, DUCTS AND CONDUIT TO BE UNPAINTED. COLOR CODING OF MECHANICAL, ELECTRICAL AND SPRINKLER SYSTEM NOT INCLUDED.
- NEW EXTERIOR METAL WALL PANELS TO BE FIELD PAINTED WITH ONE COAT BONDING PRIMER AND ONE COAT LATEX TO MATCH EXISTING.
- ALL FINISH COATS OF PAINT TO PROVIDE ADEQUATE COVERAGE.

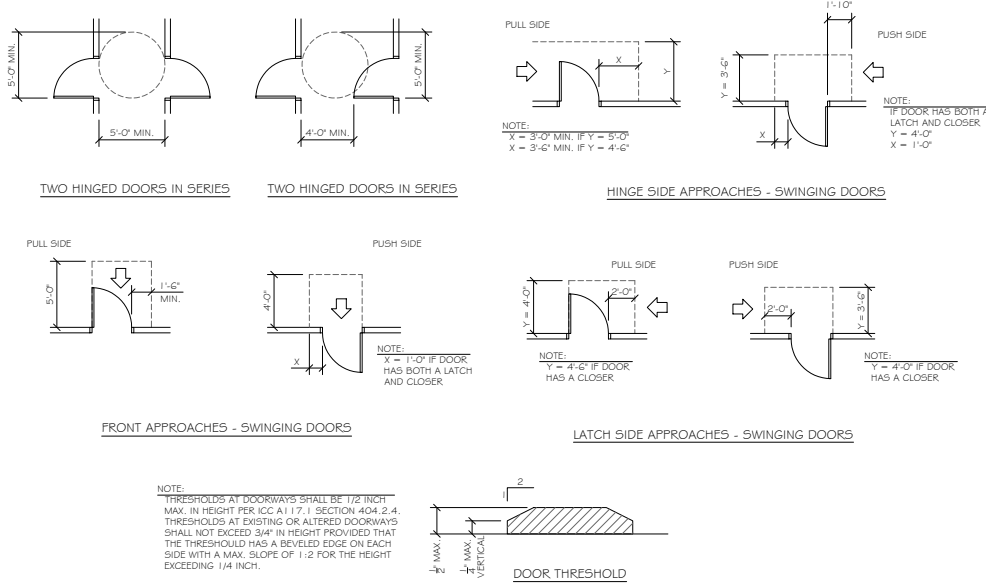
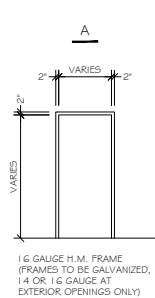
DOOR ELEVATIONS



FRAME DETAILS



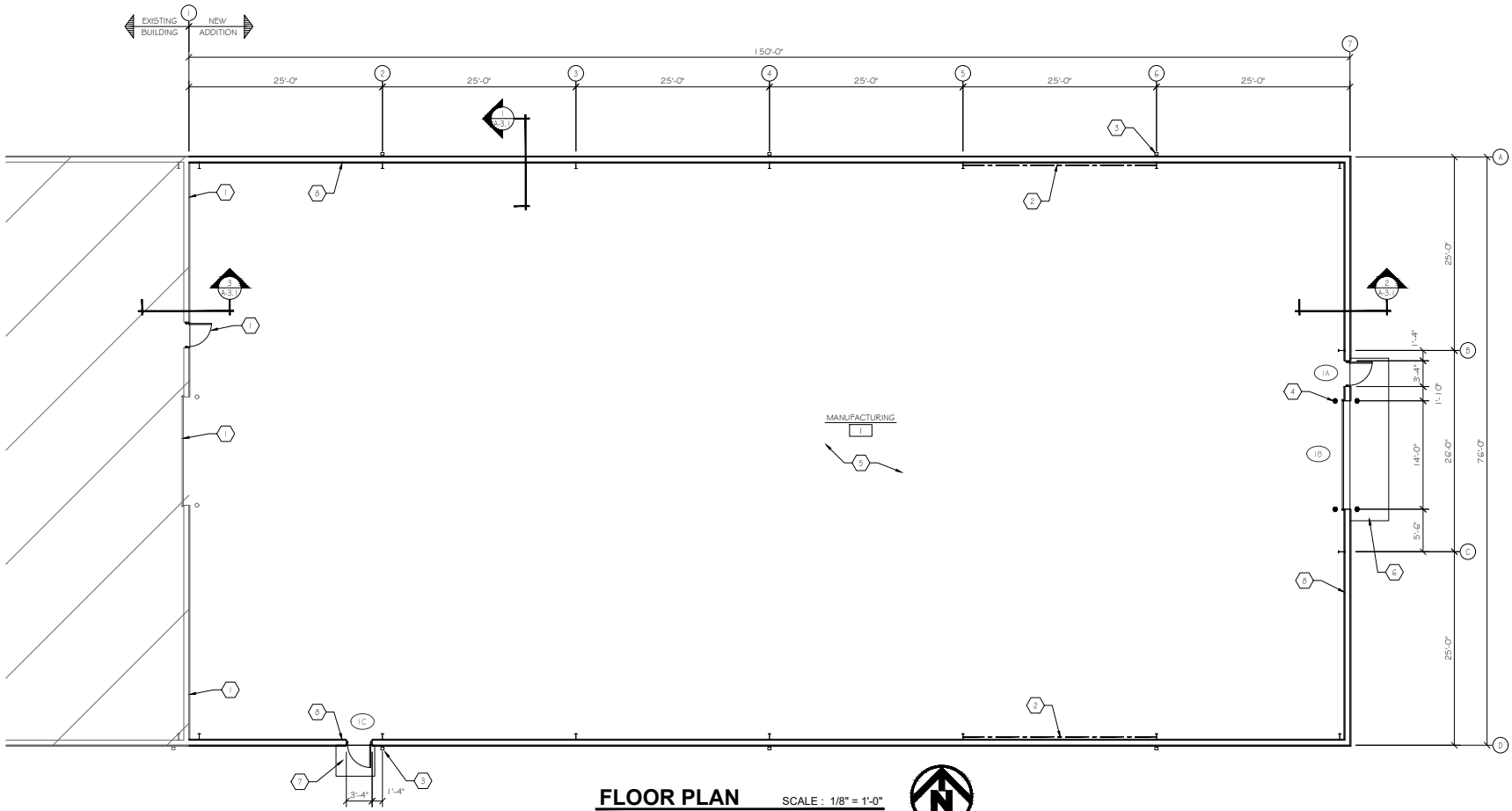
FRAME ELEVATIONS



ADA DETAILS SCALE : 3/16" = 1'-0"

CONSTRUCTION NOTES

- EXISTING CONSTRUCTION TO REMAIN.
- X-ROD WALL PLANE BRACING.
- DOWNSPOUT TO BE CONNECTED TO STORM LINE.
- 6" DIAMETER X 7'-0" LONG CONCRETE FILLED STEEL GUARD POST, 4'-0" A.F.F. SET IN MAX. 1/25 CUBIC YARD OF CONC. (TYP.)
- 6" THICK POURED CONCRETE FLOOR SLAB REINFORCED WITH ONE LAYER 42# W.W.F. ON 4" CUSHION OF GRANULAR MATERIAL.
- 6" THICK POURED CONCRETE APRON REINFORCED WITH ONE LAYER 42# W.W.F. ON COMPACTED OR UNDISTURBED EARTH.
- 4" THICK POURED CONCRETE APRON REINFORCED WITH ONE LAYER 21# W.W.F. ON COMPACTED OR UNDISTURBED EARTH.
- MUCOR EXPOSED FASTENER 28 GA. METAL LINER PANELS TO 8'-0" A.F.F.



FLOOR PLAN SCALE : 1/8" = 1'-0"



Revisions

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Job Number 21-16259

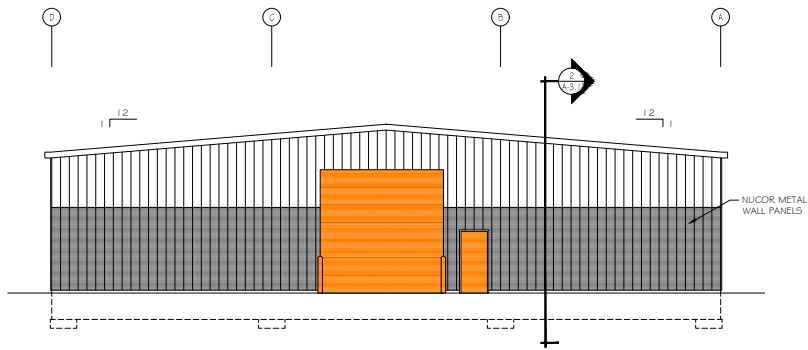
Date 3/26/21

Drawn By JTS

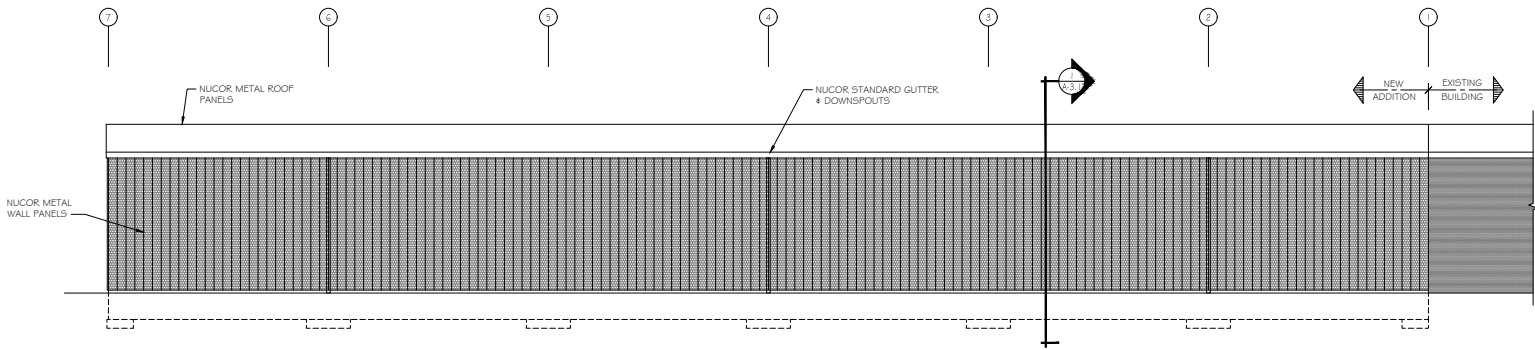
Checked By JWM

ENLARGED
FLOOR PLAN,
SCHEDULES

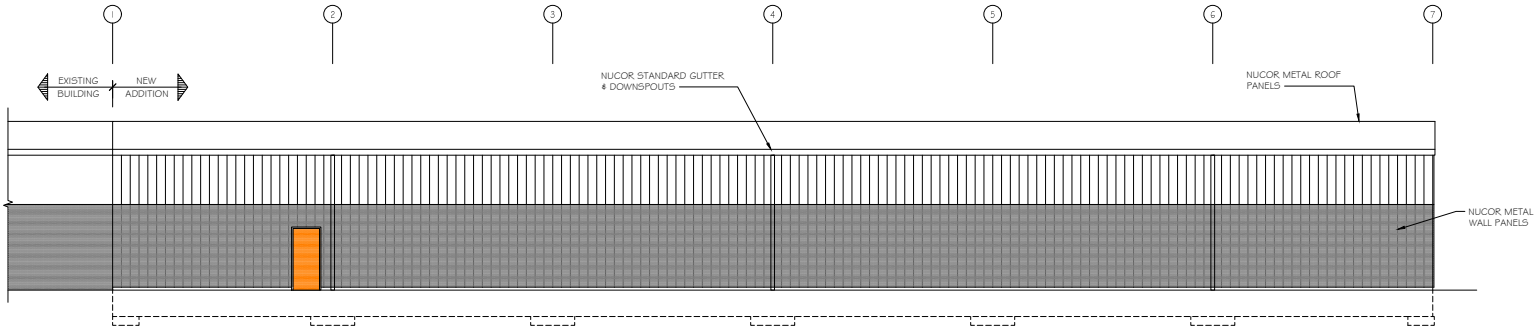
A-1.1



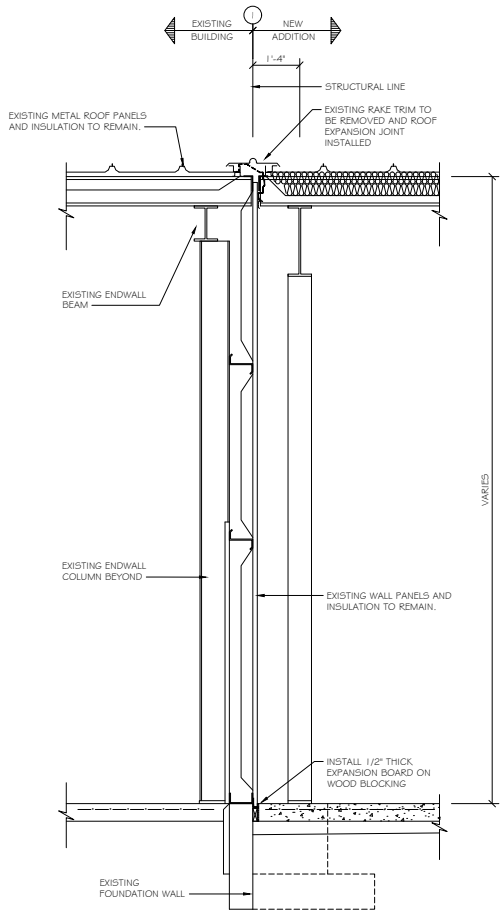
EAST ELEVATION SCALE : 1/8" = 1'-0"



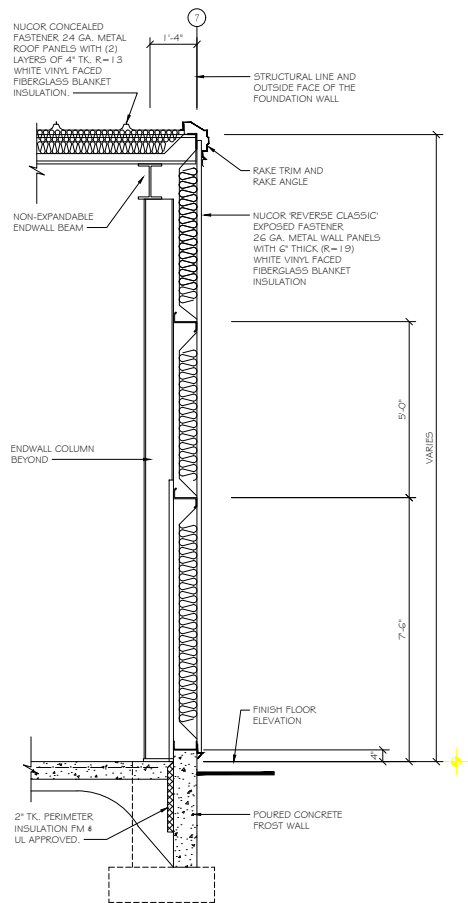
NORTH ELEVATION SCALE : 1/8" = 1'-0"



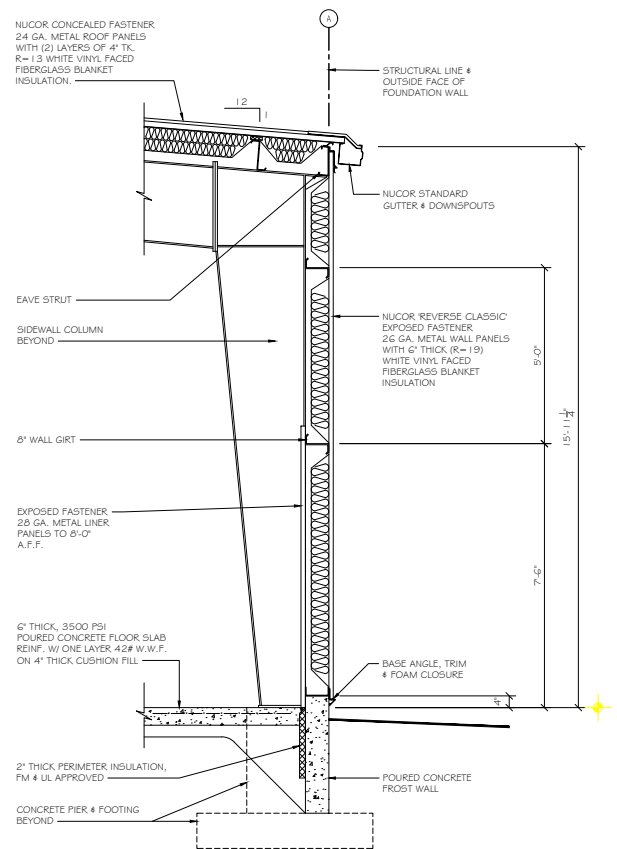
SOUTH ELEVATION SCALE : 1/8" = 1'-0"



3 WALL SECTION SCALE : 1/2" = 1'-0"



2 WALL SECTION SCALE : 1/2" = 1'-0"



1 WALL SECTION SCALE : 1/2" = 1'-0"

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BUILDING ADDITION
FOR
THE TOOLING ZONE
WARREN COUNTY
285 S. PIONEER BLVD.
SPRINGBORO, OHIO 45066



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Sidney, OH 45365-0726
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Job Number	21-16259
Date	3/26/21
Drawn By	JTS
Checked By	JWM

BUILDING
ELEVATIONS,
WALL SECTION

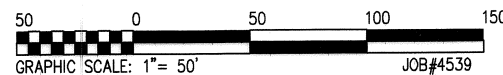
A-3.1

VOLUME 141 PLAT NO. 80
WARREN COUNTY ENGINEER'S
RECORD OF LAND SURVEYS

PRELIMINARY ACCESS APPROVAL:
 ___ GRANTED X NOT APPLICABLE
Neil F. Tunison
 NEIL F. TUNISON P.E., P.S. (BF)
 WARREN COUNTY ENGINEER



BEARINGS BASED ON EAST RIGHT OF WAY OF PIONEER
BOULEVARD (N 03°51'45"E) AS RECORDED ON SURVEY
83-34 OF THE SURVEY RECORDS OF WARREN COUNTY
ENGINEER'S OFFICE.



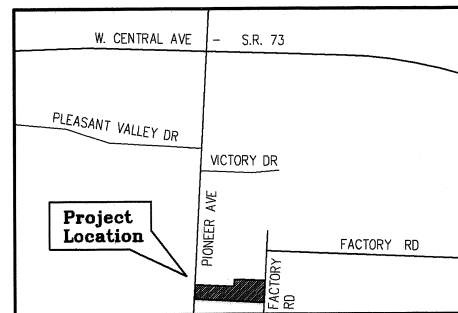
MONUMENT LEGEND

- ⊗ Indicates 5/8" iron pin found (unless otherwise noted)
- Indicates 5/8" iron pin set
- ⊗ Indicates Iron Pipe found
- ⊗ Indicates MAG nail found (unless otherwise noted)
- △ Indicates MAG nail set
- Indicates Railroad Spike found
- × Indicates Scribe set

REFERENCES

DEED: 2.066 ACRES - WHO DEY INVESTMENTS, LLC / DEED BOOK 5241, PAGE 642
2.8230 ACRES - WHO DEY INVESTMENTS, LLC / 2014-000199

PLAT: BECK HEIGHTS – PLAT BOOK 3, PAGE 407
PIONEER BLVD – PLAT BOOK 22, PAGE 23
PIONEER BLVD EXTENSION – PLAT BOOK 25, PAGE 100



Vicinity Map
No Scale

Plat of Survey

Who Dey Property

Section 19, Town 2, Range 5 M.Rs
City of Springboro & Franklin Township
Clearcreek Township, Warren County, Ohio

CONTAINING 4.8892 Acres

Prepared by:
McDougall - Marsh
Land Surveyors
8529 Byers Road, Miamisburg, Ohio 45342
Tel: 937-847-2660 - Fax 937-847-2670
www.mcdougallmarsh.com

2.865 ACRES (BY DEED)
JOHN M. MOCKABEE
O.R. 1101, PAGE 954

S.R. 69-41
S.R. 81-56
S.R. 93-48
S.R. 83-33

$$\begin{array}{r} 2.8230 \text{ ACRES (2.6164 ACRES IN CITY)} \\ + \text{TRACT II} \\ \hline 3.1668 \text{ ACRES} = \text{TRACT III (2.9602 ACRES IN CITY)} \end{array}$$

2.8230 ACRES (BY DEED)
WHO DEY INVESTMENTS LLC
2014-000199
(SUR. 121-06)
(SUR. 90-64)
(SUR. 83-33)
(SUR. 83-34)

I certify that this plat of survey was prepared in accordance with Ohio Administrative Code Chapter 4733-37 standards for boundary surveys. All monumentation is or to be set as shown.

McDougall - Marsh Land Surveyors

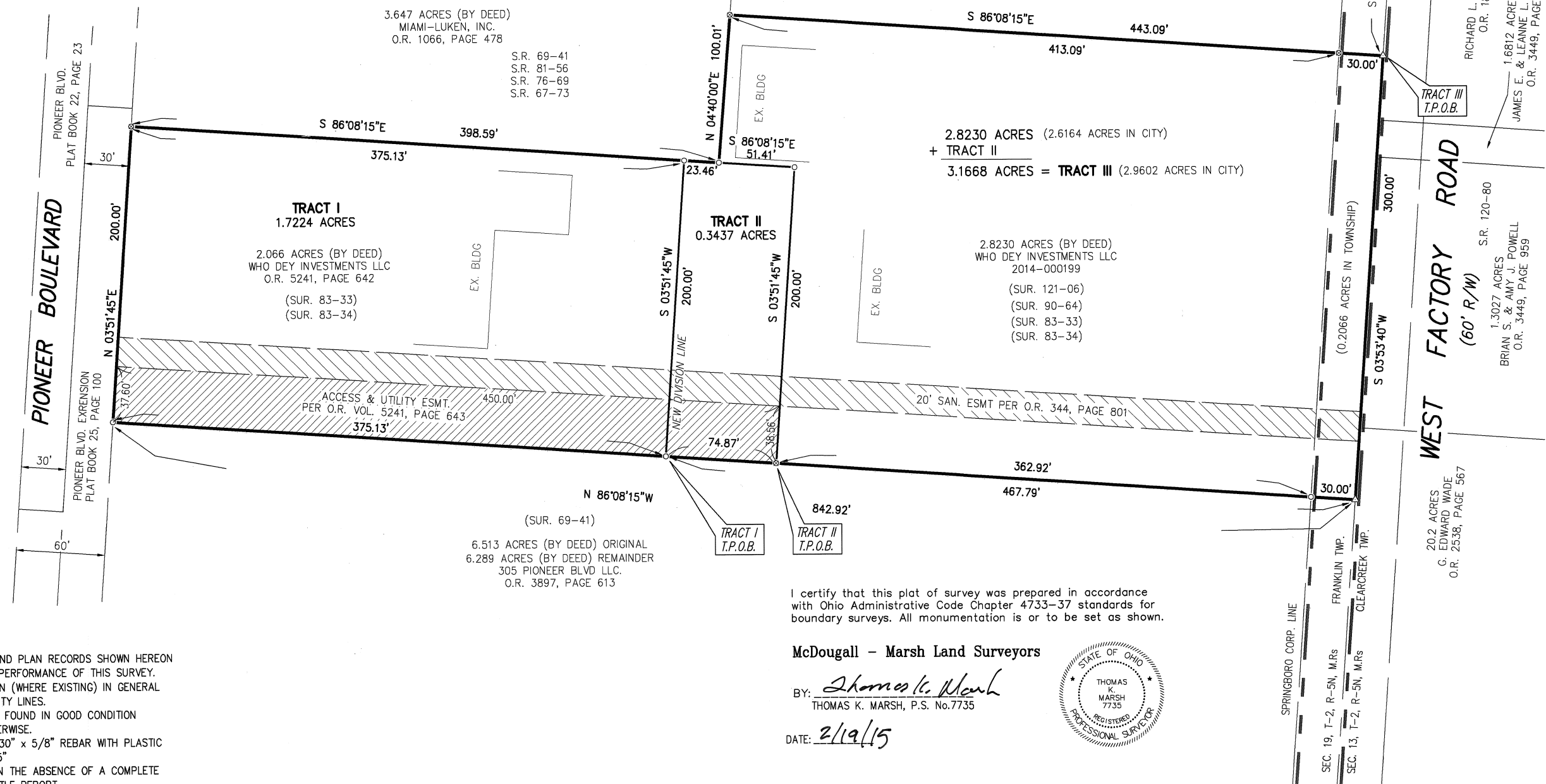
BY: Thomas K. Marsh
THOMAS K. MARSH, P.S. No. 7735

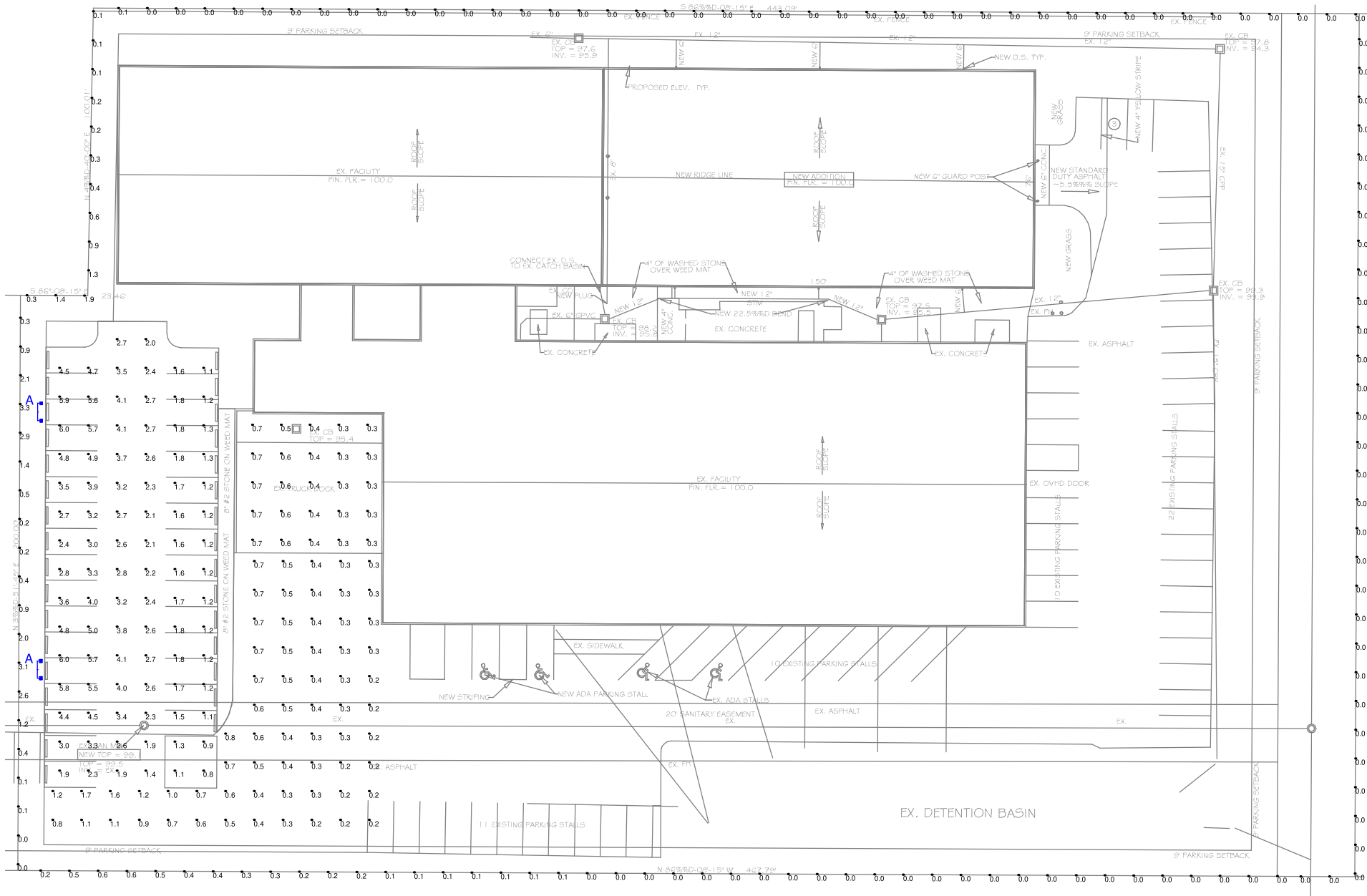
DATE: 2/19/15



SURVEY NOTES:

1. ALL DEED, SURVEY AND PLAN RECORDS SHOWN HEREON WERE USED IN THE PERFORMANCE OF THIS SURVEY.
2. LINES OF OCCUPATION (WHERE EXISTING) IN GENERAL AGREE WITH PROPERTY LINES.
3. ALL MONUMENTATION FOUND IN GOOD CONDITION UNLESS NOTED OTHERWISE.
4. IRON PINS SET ARE 30" x 5/8" REBAR WITH PLASTIC CAP STAMPED "7735"
5. SURVEY PREPARED IN THE ABSENCE OF A COMPLETE AND UP TO DATE TITLE REPORT.





Scale: 1 inch= 20 Ft.

Calculation Summary							
Project: Base Bid							
Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
Existing Paved Surfaces	Illuminance	Fc	0.52	1.7	0.2	2.60	8.50
Existing Truck Dock	Illuminance	Fc	0.46	0.7	0.3	1.53	2.33
Property Boundary	Illuminance	Fc	0.23	3.3	0.0	N.A.	N.A.
Proposed Parking Area	Illuminance	Fc	2.80	6.0	0.8	3.50	7.50

CALCULATION NOTES:
A. BASED ON THE INFORMATION PROVIDED. ALL DIMENSIONS AND LUMINAIRE LOCATIONS SHOWN REPRESENT RECOMMENDED POSITIONS. THE ENGINEER AND/OR ARCHITECT MUST DETERMINE THE APPLICABILITY OF THE LAYOUT TO EXISTING OR FUTURE FIELD CONDITIONS.
B. THIS LIGHTING PLAN REPRESENTS ILLUMINATION LEVELS CALCULATED FROM LABORATORY DATA TAKEN UNDER CONTROLLED CONDITIONS IN ACCORDANCE WITH THE ILLUMINATING ENGINEERING SOCIETY (IES) APPROVED METHODS. ACTUAL PERFORMANCE OF ANY MANUFACTURER'S LUMINAIRES MAY VARY DUE TO CHANGES IN ELECTRICAL VOLTAGE, TOLERANCE IN LAMPS/LED'S AND OTHER VARIABLE FIELD CONDITIONS.
C. CALCULATIONS SHOWN ARE MAINTAINED ILLUMINANCE UNLESS NOTED OTHERWISE.
D. CALCULATIONS ARE TAKEN AT GRADE UNLESS NOTED OTHERWISE.

Luminaire Schedule							
Project: Base Bid							
Symbol	Label	Qty	LLF	Lum. Lumens	Series	Tilt	Mounting Height
	A	2	0.900	21156	E-APR19A-T340B	50 DEG.	25' AFG

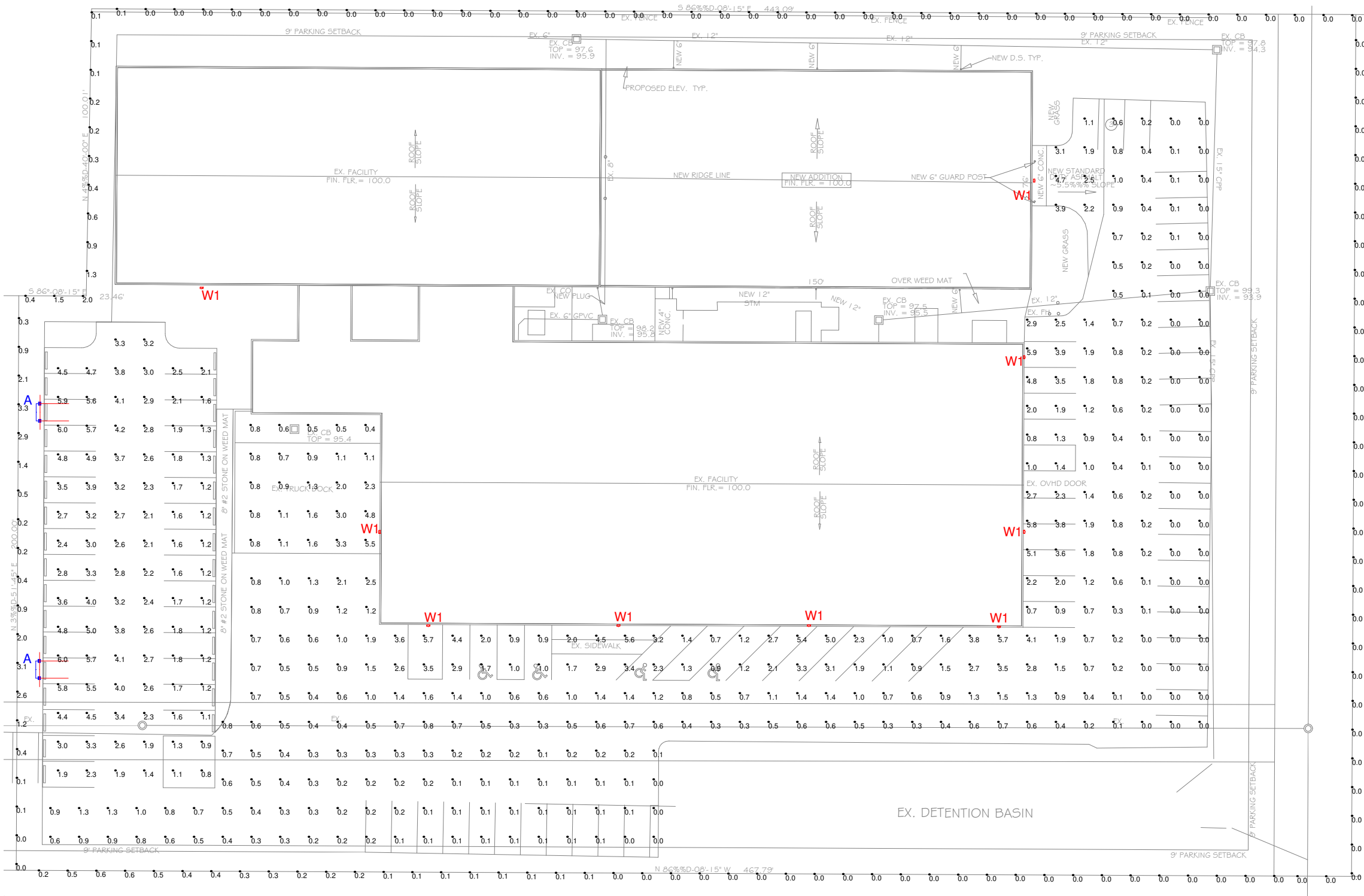
SPRINGBORO, OH CODIFIED ORDINANCES CHAPTER 1273, Exterior Lighting
E. Outdoor lighting shall be designed to achieve uniform illumination levels. The ratio of the average light level of the surface being lit to the lowest light level of the surface being lit, measured in foot-candles, shall not exceed a ratio of 4-to-1. In no case shall the maximum to minimum light levels exceed a ratio of 10-to-1 between any 2 points on a lot.
N. The following illumination levels shall act as minimum standards for all exterior lighting. Maximum lighting will be governed by the 4-to-1 ratio of average to minimum illumination of the surface being lit standards identified in subsection (a)(3)E. above; in no case shall the maximum lighting level exceed the maximum identified in the table below:

Parking lots and associated circulation areas
0.4 Minimum
6.0 Maximum



9753 CRESENT PARK DR
WEST CHESTER, OH 45069
513-761-6360



LIGHTING PROPOSAL: BRLC21-010				
THE TOOLING ZONE				
285 S PIONEER BLVD SPRINGBORO, OHIO 45066				
By: C.B.	Date:3/9/2021	Scale: AS NOTED	REV:	SHEET 1 OF 2



Scale: 1 inch= 20 Ft.

Calculation Summary							
Project: Base Bid							
Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
Existing Paved Surfaces	Illuminance	Fc	0.97	5.9	0.0	N.A.	N.A.
Existing Truck Dock	Illuminance	Fc	1.53	5.5	0.4	3.83	13.75
Property Boundary	Illuminance	Fc	0.24	3.3	0.0	N.A.	N.A.
Proposed Parking Area	Illuminance	Fc	2.87	6.0	0.8	3.59	7.50

CALCULATION NOTES:
A. BASED ON THE INFORMATION PROVIDED, ALL DIMENSIONS AND LUMINAIRE LOCATIONS SHOWN REPRESENT RECOMMENDED POSITIONS. THE ENGINEER AND/OR ARCHITECT MUST DETERMINE THE APPLICABILITY OF THE LAYOUT TO EXISTING OR FUTURE FIELD CONDITIONS.
B. THIS LIGHTING PLAN REPRESENTS ILLUMINATION LEVELS CALCULATED FROM LABORATORY DATA TAKEN UNDER CONTROLLED CONDITIONS IN ACCORDANCE WITH THE ILLUMINATING ENGINEERING SOCIETY (IES) APPROVED METHODS. ACTUAL PERFORMANCE OF ANY MANUFACTURER'S LUMINAIRES MAY VARY DUE TO CHANGES IN ELECTRICAL VOLTAGE, TOLERANCE IN LAMPS/LED'S AND OTHER VARIABLE FIELD CONDITIONS.
C. CALCULATIONS SHOWN ARE MAINTAINED ILLUMINANCE UNLESS NOTED OTHERWISE.
D. CALCULATIONS ARE TAKEN AT GRADE UNLESS NOTED OTHERWISE.

Luminaire Schedule								
Project: Base Bid								
Symbol	Label	Qty	LLF	Lum. Lumens	Manufacturer	Series	Tilt	Mounting Height
	A	2	0.900	21156	RZ	E-APR19A-T340B	50 DEG.	25' AFG
	W1	9	0.900	5316	SIGNIFY GARDCO	101L-32L-530-NW-G1-4	0 DEG.	12' AFG

SPRINGBORO, OH CODIFIED ORDINANCES CHAPTER 1273, Exterior Lighting
E. Outdoor lighting shall be designed to achieve uniform illumination levels. The ratio of the average light level of the surface being lit to the lowest light level of the surface being lit, measured in foot-candles, shall not exceed a ratio of 4-to-1. In no case shall the maximum to minimum light levels exceed a ratio of 10-to-1 between any 2 points on a lot.
N. The following illumination levels shall act as minimum standards for all exterior lighting. Maximum lighting will be governed by the 4-to-1 ratio of average to minimum illumination of the surface being lit standards identified in subsection (a)(3)E. above; in no case shall the maximum lighting level exceed the maximum identified in the table below:

Parking lots and associated circulation areas
0.4 Minimum
6.0 Maximum



9753 CRESENT PARK DR
WEST CHESTER, OH 45069
513-761-6360

LIGHTING PROPOSAL: BRLC21-010

THE TOOLING ZONE

285 S PIONEER BLVD
SPRINGBORO, OHIO 45066

By: C.B.

Date:3/9/2021

Scale:
AS NOTED

REV: 1

SHEET
1 OF 1











2021-Mar-05

Page 1/1

MPALPRO-20-5K-T3 (480V).ies

Best Lighting Products
MPALPRO-20-5K-T3 (480V)



Luminaire Watts 154 W

Ballast/Driver Factor	1.00
-----------------------	------

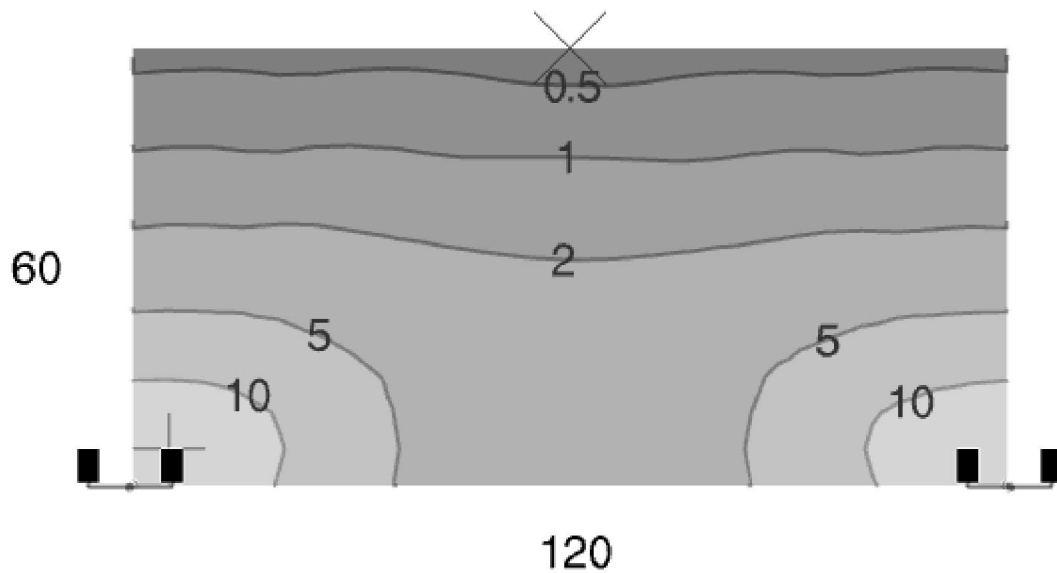
Light Loss Factor	1.00
-------------------	------

Total Proration Factor	1.00
------------------------	------

Luminaire Lumens	20114	lms
------------------	-------	-----

 120 ft

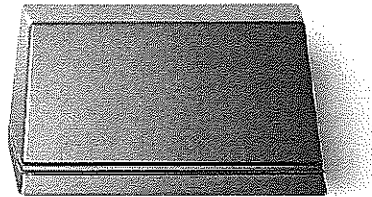
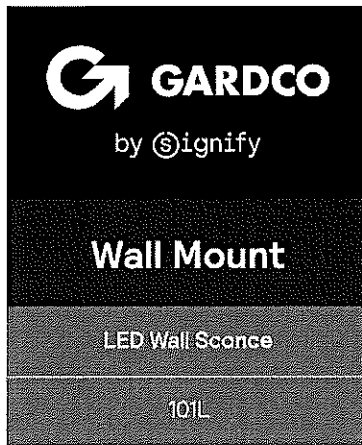
25 ft



⚠ Min: 0.31 fc ✕
Max/Avg: 4.1

Avg: 3.5 fc
Avg/Min: 11

Max: 15 fc +
! Max/Min: 46



Project:	The Tooling Zone		
Location:	Springboro		
Cat.No:	101L-32L-530-NW0G104		
Type:	Wall Pack		
Lamps:	1	Qty:	9
Notes:			

Gardco 101 LED wall sconces feature a low-profile design that provides wide flexibility in high performance exterior wall illumination. Full cutoff performance, usable illumination patterns, and powerful wattages combine into a compact and architecturally pleasing design. 101L sconces are available in Type 2, 3, and 4 distributions, and provide output of up to 9500 lumens. Energy saving control options increase energy savings and offer California Title 24 compliance. Emergency Battery Backup option available for path of egress.

Ordering guide

example: 101L-32L-700-NW-G1-3-120-IMRI2-BZ

							Options		
Prefix	Number of LEDs	Drive Current	LED Color - Generation	Distribution	Emergency	Voltage	Controls	Electrical	Finish
101L	32L	530	NW-G1	4		UNV	N/A	F1	bz
101L 101L LED Wall Sconce	16L 16 LEDs (1 module)	530 530mA	CW-G1 Cool White 5700K, 70 CRI Generation 1	2 Type 2 3 Type 3 4 Type 4	EBPC Emergency Battery Pack Cold Weather ^{3,4,6,12} Leave blank to omit an emergency option	UNV 120-277V	DD 0-10V Dimming Driver ^{5,6} DCC Dual Circuit Control ^{6,7,8} DynaDimmer: Automatic Profile Dimming CS50 Safety 50% Dimming (7 hours) ^{7,9,10} CM50 Median 50% Dimming (8 hours) ^{7,9,10} CE50 Economy 50% Dimming (9 hours) ^{7,9,10} DA50 All Night 50% Dimming ^{7,9,10} Photoelectric/Receptacle systems (Twist Lock Receptacle) PCB Photocontrol Button ^{5,10,11,12} TLRD5 Twist Lock Receptacle 5-Pin ¹³ TLRD7 Twist Lock Receptacle 7-Pin ¹³ TLRPC Twist Lock Receptacle w/ Photocell ^{11,12,16} Infrared Motion Response systems IMRI2 Integral with #2 lens ^{9,12,14} IMRI3 Integral with #4 lens ^{9,12,14} Network system (SiteWise) SW SW Integral module ^{4,17} Wireless system LLC2 Integral module with #2 lens ^{5,7,9,15} LLC3 Integral module with #3 lens ^{5,7,9,15}	Fusing F1 Single (120, 277, 347VAC) ¹² F2 Double (208, 240, 480VAC) ¹³ F3 Canadian Double Pull (208, 240, 480VAC) ¹²	BK Black WH White BZ Bronze DGY Dark Gray MGY Medium Gray Customer specified RAL Specify optional color or RAL (ex: OC-LGP or OC-RAL7024) CC Custom color (Must supply color chip for required factory quote)
		650 650mA ¹				HVU 347-480V			
		700 700mA				120 120V			
		1000 1000mA				208 208V			
	32L 32 LEDs (2 module)	530 530mA	NW-G1 Neutral White 4000K, 70 CRI Generation 1			240 240V			
		650 650mA ¹				277 277V			
		700 700mA				347 347V			
		1000 1000mA ¹				480 480V			
			WW-G1 Warm White 3000K, 70 CRI Generation 1						

- 650mA only available with Emergency Battery Pack Cold Rated (EBPC) option
- 32L rated for 30°C at 1000mA
- Available for use with 16L and 32L in 530mA or 650mA only. Rated for -20°C to 35°C.
- Available in 120 or 277V only.
- Not available with Dual Circuit Control (DCC) option.
- EBPC is not available with DCC.
- Not available with Dimming Driver (DD) option.

- Available in 32L with 530mA. Consult technical support center for use with photocell and CS/CM/CE/DA.
- Available in 120-277V (UNV) only.
- Not available with LLC, TLR and DCC.
- Not available with 480V.
- Must specify input voltage.
- TLRD5/7 option not available with LLC, PCB, DCC. Works with 3 or 5 pin NEMA photocell dimming. Dimming will not be connected to TLR if ordering with DD, CS/CM/CE/DA and IMRI.
- Not available with DD, DCC or LLC.

- LLC2/3 Not available with TLR, PCB, IMRI, CS/CM/CE/DA. Ships with WS accessory attached to wireless module. Not for use with LLC accessory.
- Not available with PCB, TLRD5/7, DCC, LLC.
- SW option is not available with any other control options with the exception of IMRI2, IMRI3 motion response options.



101L Sconce LED

Wall Mount

Luminaire Accessories (order separately)

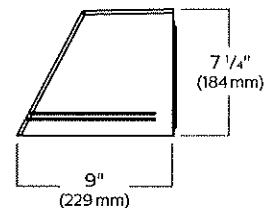
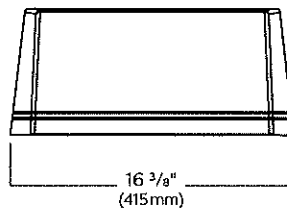
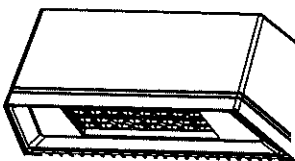
Mounting accessories
Wall Mount
WS Wall Mounted Box for Surface Conduit

System accessories
Wireless system remote mount module
LLCR2-(F) #2 lens - specify finish in place of (F)
LLCR3-(F) #3 lens - specify finish in place of (F)
Central Remote Motion Response (used connected to SiteWise main panel)
MS2-A-FVR-3
MS2-A-FVR-7

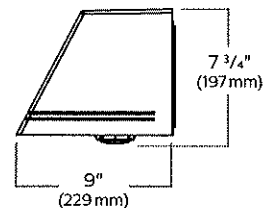
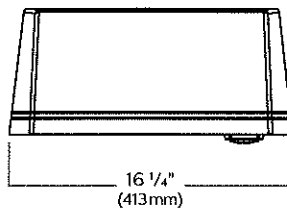
Wireless system remote controller accessory

Wireless system offers a remote radio/sensor module that allows to connected to a Limelight system (sold by other). Remote module can be mounted to wall or pole with j-box supplied. May be specified by choosing one of two different lenses to accommodate a variety of mounting heights/sensor detection ranges. Must specify option DD on luminaires that are planned to be used with remote mount controllers. See page 4 for Wireless system details.

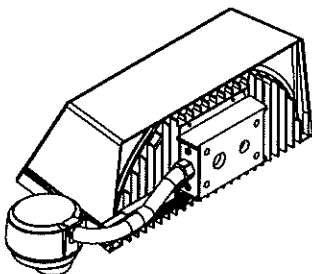
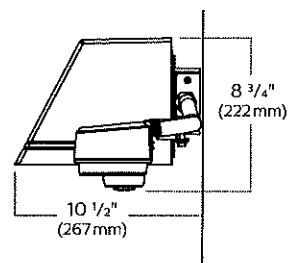
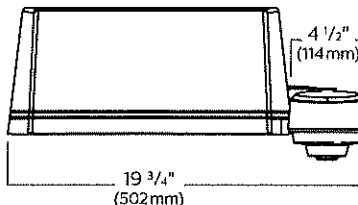
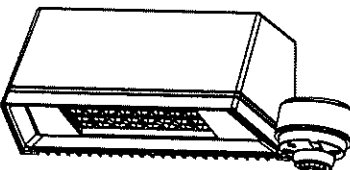
Dimensions



Motion Response



Wireless Controls



Luminaire Weights	
LED Wall Sconce 101L	Weight
Luminaire	13.5 lbs
Luminaire - EBPC (EM battery pack)	17.0 lbs
Luminaire - Integrated system controls	16.3 lbs

101L Sconce LED

Wall Mount

LED Wattage and Lumen Values

Ordering Code	LED Qty	LED Current (mA)	Color Temp.	Average System Watts ¹	Type 2			Type 3			Type 4		
					Lumen Output ^{1,2}	BUG Rating	Efficacy (LPW)	Lumen Output ^{1,2}	BUG Rating	Efficacy (LPW)	Lumen Output ^{1,2}	BUG Rating	Efficacy (LPW)
101L-16L-530-NW-G1	16	530	4000K	28	2944	B1-U0-G0	106	2687	B1-U0-G1	97	2747	B1-U0-G1	99
101L-16L-700-NW-G1	16	700	4000K	37	3789	B1-U0-G1	103	3458	B1-U0-G1	94	3535	B1-U0-G1	96
101L-16L-1000-NW-G1	16	1000	4000K	55	5050	B1-U0-G1	92	4609	B1-U0-G1	84	4712	B1-U0-G1	86
101L-16L-1200-NW-G1	16	1200	4000K	65	5744	B2-U0-G1	89	5242	B1-U0-G2	81	5359	B1-U0-G2	83
101L-32L-530-NW-G1	32	530	4000K	52	5698	B2-U0-G1	110	5200	B1-U0-G2	100	5316	B1-U0-G2	102
101L-32L-700-NW-G1	32	700	4000K	70	7242	B2-U0-G1	103	6609	B1-U0-G2	94	6757	B1-U0-G2	96
101L-32L-1000-NW-G1	32	1000	4000K	107	9797	B2-U0-G1	92	8941	B2-U0-G2	84	9140	B2-U0-G2	86

LED Wattage and Lumen Values (Emergency Mode)³

Ordering Code	LED Qty	LED Current (mA)	Color Temp.	Ave. System Watts (charging mode)	Type 2	Type 3	Type 4
101L-16L-NW-EBPC	16	N/A	4000K	14	1345	1228	1255
101L-32L-NW-EBPC	32	N/A	4000K	14	1754	1600	1636

1. Wattage and lumen output may vary by +/- 8% due to LED manufacturer forward volt specification and ambient temperature. Wattage shown is average for 120V through 277V input. Actual wattage may vary by an additional +/- 10% due to actual input voltage.
2. Lumen values based on photometric tests performed in compliance with IESNA LM-79.
3. For emergency EBPC option, published values are based on initial lumens.

Luminaire options

DD: 0-10V dimming driver with leads supplied through back of luminaire (for secondary dimming controls by others).

Dynadimmer Automatic Profile Dimming: Automatic dimming profiles (CS50/CM50/CE50) offer safety, median, or economy settings, for shorter or longer duration. Dimming profiles provide flexibility towards energy savings goals while optimizing light levels during specific dark hours. 50% dimming is standard. DASO offers 50% instantaneous dimming all night (during all dark hours). Other dimming settings are also available if different light levels are required (contact Technical Support for details).

Profile	Dimming		
	Schedule	Duration	Level
Economy	9 PM - 6 AM	9 hours	50%
Median	10 PM - 6 AM	8 hours	50%
Safety	11 PM - 6 AM	7 hours	50%
Reactive 50	all night	dynamic	50%

TLRD5: Twist Lock Receptacle with 5 pins enabling dimming, can be used with a twistlock photoelectric cell or a shorting cap. Can also be used with third party control system.

TLRD7: Twist Lock Receptacle with 7 pins enabling dimming and additional functionality, can be used with twistlock photoelectric cell or a shorting cap. Can also be used with third party control system.

TLRDPC: Receptacle with twistlock photoelectric cell (must specify voltage). Receptacle located on top of luminaire housing.

IMRI2, IMRI3: Infrared Motion Response Integral (IMRI). IMRI module is mounted integral to the luminaire door and is available with two different sensor lens types to accommodate various mounting heights and occupancy detection ranges (see charts for approximate detection patterns). Motion response used in combination of Dynadimmer and SiteWise are not programmable and used to override controllers schedule when motion is detected. When used not combined with any controller, IMRI is set/operates in the following fashion: The motion sensor is set to a constant 50%. When motion is detected by the PIR sensor, the luminaire returns to 100% light output. Dimming on low is factory set to 50% with 5 minute default in "full power" prior to dimming back to low. When no motion is detected for 5 minutes, the motion response system reduces the wattage by 50% of the normal constant wattage reducing the light level. IMRI can also be specified with automatic profile dimming for the added benefit of a combined dimming profile with sensor detection, where the PIR sensor will override the dimming profile when occupancy is detected. Passive infrared (PIR) motion sensor, WattStopper FSP-211, equipped with lens choice specified. Available from 120V to 277V input only. Motion sensor off state power is 0.0 watts. The FSP-211 can also be reprogrammed with WattStopper's FSIR-100 remote programming tool accessory.

DCC: Dual Circuit Control permits separate switching of 32L models only, where a quantity of (2) 16 LED modules are controlled independently by use of two sets of leads, one for each module.

SW

SiteWise option is a fully integrated controller that connects to SiteWise system in order to offer a complete area lighting management system. The communication signal is based on patented central dimming technology. SiteWise delivers it deliver optimal energy savings using your site's existing cabling. No additional wiring required, installation and commissioning are simple. An intuitive, mobile app makes it easy for authorized users to set schedules to meet site specific lighting needs, local regulations, and energy codes.

Wireless system: 101L luminaires are available with optional wireless controllers ready to be connected to a Limelight system (sold by other). The system allows you to Wirelessly manage the entire site, independent lighting groups or individual luminaires while on-site or remotely.

Based on a high density mesh network with an easy to use web-based portal, you can conveniently access, monitor and manage your lighting network remotely. Wireless System can be combined with site and area, pedestrian, and parking garage luminaires as well, for a completely connected outdoor.

101L Sconce LED

Wall Mount

Luminaire options (continued)

F1: Fusing Single (for 120, 277 or 347VAC)

F2: Fusing Double (for 208, 240 or 480VAC)

F3: Fusing Canadian Double Pull
(for 208, 240 or 480VAC)

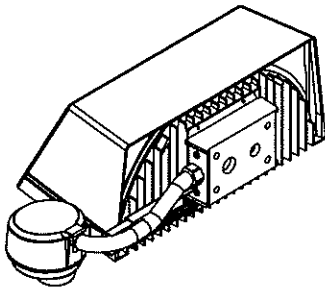
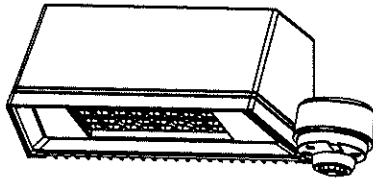
EBPC: Emergency battery pack is cold weather rated down to -20C (-4F) and integral to the luminaire, allowing for a consistent look between emergency and non-emergency sconces. A separate surface mount accessory box is not required. Dual light engines (32L) are wired in parallel, both operating in emergency mode to meet various redundancy lamp

requirements. Also available with single light engine (16L). Secondary driver with relay immediately detects AC power loss and powers luminaire for a minimum of 90 minutes from the time power is lost.

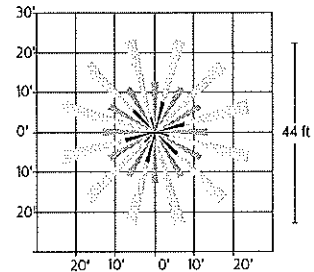
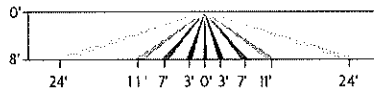
Infrared Motion Response and Wireless system sensor coverage patterns

LLC2/3 Luminaire mounted controller

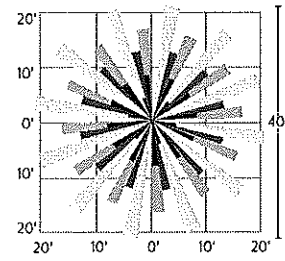
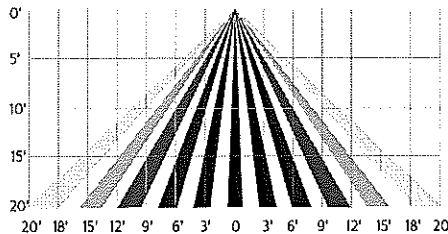
Controller attached to luminaire and includes radio, photocell and motion sensor with #2 or #3 lens for 8-20' mounting heights.



IMRI2/LLC2/LLCR2

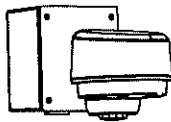


IMRI3/LLC3/LLCR3

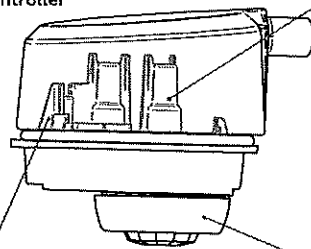


Remote Mount Wireless Controller

Used to extend the communication on site, to extend motion response and add other luminaires that are not pole mounted. Consult factory for more information.



Controller



Wireless Radio

- 1.8 Watts max (no load draw)
- Operating voltage 120-277 VAC RMS
- Communicates using the ZigBee protocol
- Carries out dimming commands from Gateway
- Reports ambient light readings to 1500 Ft-Cd
- Transmission Systems Operating within the band 2400-2483.5Mhz
- ROHS Compliant

Photocell

- Ambient light photocell on every wireless radio that averages the light levels of up to 5 controllers for an accurate reading and optimal light harvesting activity.
- Reports ambient light readings to 1500 Fc.

Motion Response

- Detects motion through passive infrared sensing technology with three different lens configurations
- Motion sensor coverage can be adjusted from a narrow to a wide detection range, which helps reduce false triggers to further increase energy savings.
- Sensing profiles can be updated to adapt to activity levels in the environment, such as occupancy level, wind, and mounting height

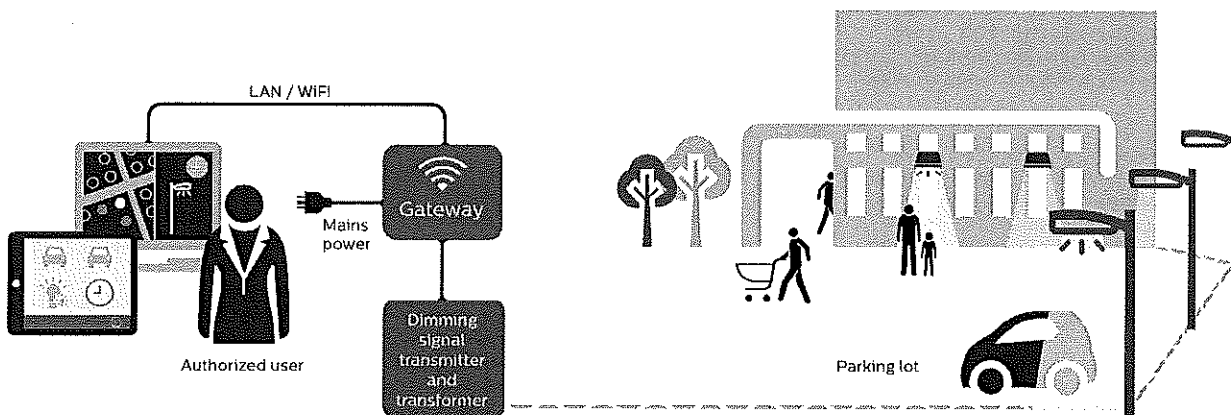
101L Sconce LED

Wall Mount

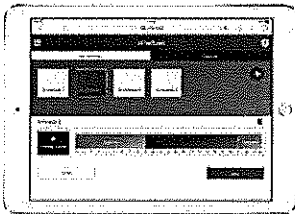
SiteWise system

SiteWise is a complete area lighting management system including a luminaire integrated controller, dimming signal transmitter cabinet, and locally accessible user interface. Installation and commissioning are simple. The cabinet communicates with the luminaires using a patented central dimming technology. The control signal is embedded on the existing electrical line – no new cabling is required. An intuitive, locally accessible interface makes it easy for authorized users to set schedules in order to meet site specific lighting needs, local regulations, and energy codes.

SiteWise system diagram



SiteWise system interface



SiteWise has an intuitive user interface that makes it easy to plan, edit, and implement lighting schedules for your site. Authorized users can access the interface via a local app.

To ensure that only authorized users can access your lighting, SiteWise offers two user types, each with different permissions. An advanced user, or administrator, can set and edit schedules using the ten pre-set scenes, assign those schedules to calendar days, and check system status.

For everyday use, a basic user can manually override a schedule that is currently running but cannot create or edit schedules.

SiteWise system specifications

The SiteWise system includes both luminaires and controls. The controls used for SiteWise are circuit load dependent. Required for a complete installation are the following SiteWise components: user interface, control kit, dimming signal transmitter cabinet, and dimming signal receiver located in the luminaire (SW option). Optional luminaire-integrated or external motion sensors may also be specified as required. Within the electrical closet, the control kit and dimming signal transmitter cabinet are installed into the electrical system between the existing breaker panel and the site luminaires. New LED luminaires containing the dimming signal receiver are installed on the site. Once completed, use of the interface allows for scheduling and override capabilities. Wireless access point and tablet should be supplied by others. Complete information on the control system can be found on the SiteWise website at philips.com/sitewise

101L Sconce LED

Wall Mount

Specifications

Housing

Main body cast housing and back plate made of a low copper die cast Aluminum alloy for a high resistance to corrosion, 0.100" (2.5mm) minimum thickness. Hinged door allows access to driver and LED compartment.

Mounting

Mounting is completed through integral back plate that features a separate recessed feature for hook and lock quick mount plate that secures with two set screws from bottom of luminaire. Mounting plate is located in the center of the luminaire width and 3.5" above the luminaire bottom (lens down position). Luminaire ships fully assembled, ready to install.

Light Engine

Composed of 4 main components: Heat Sink / LED Module / Optical System / Driver. Electrical components are RoHS compliant. IP66 sealed light engines. LEDs tested by ISO 17025-2005 accredited lab in accordance with IESNA LM-80 guidelines extrapolations in accordance with IESNA TM-21. Metal core board ensures greater heat transfer and longer lifespan.

Heat Sink

Integral door/heat sink design made of low copper die cast Aluminum alloy for a high resistance to corrosion.

LED Module

Composed of high performance white LEDs. Color temperature as per ANSI/NEMA bin Neutral White, 4000K nominal (+/- 275K), CRI 70 Min. Available in other color temperatures including Cool White, 5700K and Warm White, 3000K.

SiteWise Network System

SiteWise system includes a controller fully integrated in the luminaire that enables the luminaires to communicate with a dimming signal transmitter cabinet located on site using patented central dimming technology. A locally accessible mobile app allows users to access the system and set functionalities such as ON/OFF, dimming levels and scheduling. SiteWise is available with motion response options in order to bring the light back to 100% when motion is detected. Additional functionalities are available such as communication with indoor lighting and connection to BMS systems.

Hardware

All exposed screws shall be stainless and/or corrosion resistant and captive.

Optical System

The advanced LED optical systems provide IES Types 2, 3, 4. Composed of high performance UV stabilized optical grade polymer refractor lenses to achieve desired distribution optimized to get maximum spacing, target lumens and a superior lighting uniformity. System is rated IP66. Performance shall be tested per LM-63, LM-79 and TM-15 (IESNA) certifying its photometric performance. Dark sky compliant with 0% uplight and U0 per IESNA TM-15.

Driver

High power factor of 90% min. Electronic driver, operating range 50/60 Hz. Auto adjusting universal voltage input from 120 to 277 VAC or 347 to 480 VAC rated for both application line to line or line to neutral, Class I, THD of 20% max. The current supplying the LEDs will be reduced by the driver if the driver experiences internal overheating as a protection to the LEDs and the electrical components. Output is protected from short circuits, voltage overload and current overload. Automatic recovery after correction. Standard built in driver surge protection of 2.5kV (min).

Surge Protection

Each luminaire is provided as standard with surge protector (designed SP1) tested in accordance with ANSI/IEEE C62.45 per ANSI/IEEE C62.41.2 Scenario I Category C High Exposure 10kV/5kA waveforms for Line Ground, Line Neutral and Neutral Ground, and in accordance with U.S. DOE (Department of Energy) MSSLC (Municipal Solid State Street Lighting Consortium) Model Specification for LED Roadway Luminaires Appendix D Electrical Immunity High Test Level 10kV / 5kA.

Wiring (supplied by others)

Splices must be made in the junction box.

Finish

Five standard colors offered in textured black, white, bronze, dark gray and medium gray. Color in accordance with the AAMA 2604 standard. Application of polyester powder coat paint 2.5 mils minimum. The thermosetting resins provides a discoloration resistant finish in accordance with the ASTM D2244 standard, as well as luster retention in keeping with the ASTM D523 standard and humidity proof in accordance with the ASTM D2247 standard. RAL and custom color matching available.

LED Products Manufacturing Standard

The electronic components sensitive to electrostatic discharge (ESD) such as light emitting diodes (LEDs) are assembled in compliance with EC61340-5-1 and ANSI/ESD S20.20 standards so as to eliminate ESD events that could decrease the useful life of the product.

LED Useful Life

Luminaire Useful Life accounts for LED lumen maintenance. Refer to IES files for energy consumption and delivered lumens for each option. Based on ISTMT in situ thermal testing in accordance with UL1598 and UL8750, LED LM-80/TM-21, expected to reach 100,000 + hours with >L70 lumen maintenance @ 25°C.

Certifications and Compliance

cULus Listed for Canada and USA suitable for wet locations when mounted downward facing. cULus Listed for Canada and USA suitable for damp locations when inverted upward facing when mounted in covered ceiling application. Emergency Battery Pack option is tested and listed to UL924 and CSA C22.2 No. 141-10 DesignLights Consortium qualified on models as listed on DLC QPL. Luminaire is rated for operation in ambient temperature of -40°C (-40°F) up to +40°C (+104°F).

Limited Warranty

5-year limited warranty. See [philips.com](https://www.philips.com)/warranties for details and restrictions. Visit our eCatalog or contact your local sales representative for more information.

101L Sconce LED

Wall Mount

LED Performance

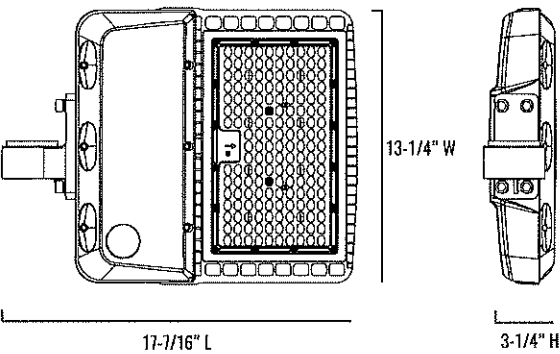
Predicted lumen depreciation data ¹				
Ambient Temperature (°C)	Driver mA	Calculated L ₇₀ hours ^{2,3}	L ₇₀ per TM-21 ^{2,3}	Lumen Maintenance % @ 60,000 hours
25°C	up to 1200 mA	>100,000	>60,000	88%

1. Predicted performance derived from LED manufacturer's data and engineering design estimates, based on IESNA LM-80 methodology. Actual experience may vary due to field application conditions.
2. L₇₀ is the predicted time when LED performance depreciates to 70% of initial lumen output.
3. Calculated per IESNA TM21-II. Published L₉₀ hours limited to 6 times actual LED test hours.

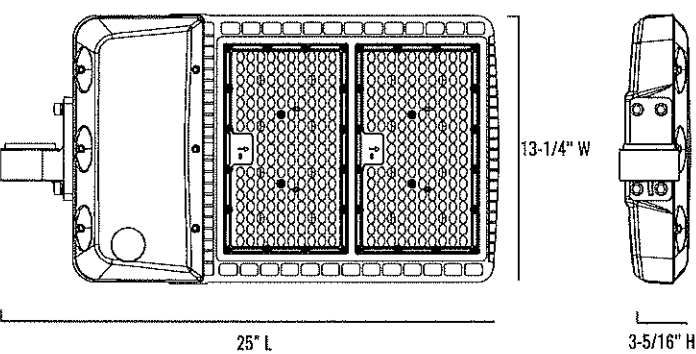


E-APR SERIES

E-APR13A, E-APR19A (SMALL HOUSING: 100W AND 150W)



E-APR32A (LARGE HOUSING: 240W)



SERIES OVERVIEW

DIMENSIONS	PRODUCT WEIGHT	MOUNTING HEIGHT	SPACING
E-APR13A (100W): 17-7/16" L x 13-1/4" W x 3-1/4" H	10.43 lbs.	15 - 25 feet	4 to 5 times the mounting height
E-APR19A (150W): 17-7/16" L x 13-1/4" W x 3-1/4" H	11.1 lbs.	15 - 25 feet	4 to 5 times the mounting height
E-APR32A (240W): 25" L x 13-1/4" W x 3-5/16" H	16.46 lbs.	20 - 35 feet	4 to 5 times the mounting height

FIXTURE SPECIFICATIONS

HOUSING	Medium bronze, durable, die-cast aluminum
LENS ASSEMBLY	UV stabilized polycarbonate
MOUNTING	Mounting sold separately. For use with adjustable direct arm mount (E-DI-FIXEDMNT) or 2-inch adjustable slip fitter mount (E-SF-ADI-FTR)

WARRANTY & CERTIFICATIONS

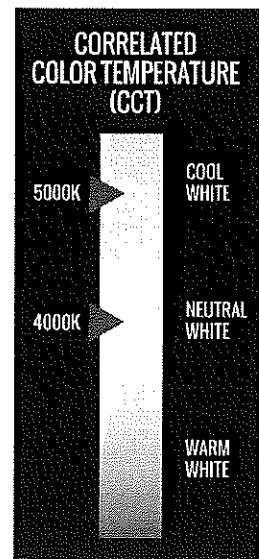
WARRANTY	5-Year Limited
eULus LISTED	Wet locations
DLC	Premium

E-APR SERIES



ELECTRICAL PERFORMANCE

OPERATING TEMPERATURE RANGE	LIFESPAN L70 AT 25°C (77°F)	POWER FACTOR	TOTAL HARMONIC DISTORTION	DIMMABLE
-40°F to 104°F (-40°C to 40°C)	Estimated >100,000 Hours	> 0.9	< 20%	to 10%
INPUT VOLTAGE Current Draw (Amps)	120V	208V	240V	277V
E-APR13	0.85A	0.48	0.42	0.36
E-APR19	1.26	0.73	0.63	0.54
E-APR32	1.99	1.16	1.01	0.87



OUTPUT SPECIFICATIONS

SKU	DISTRIBUTION TYPE	REPLACES	LIGHT OUTPUT	COLOR TEMP	POWER CONSUMPTION	COLOR ACCURACY	BUG RATINGS
E-APR32A-T550B	Type 5	750W PSMH	32,000 Lumens	5000K	240W	≥ 70	B5-U3-G4
E-APR32A-T350B	Type 3	750W PSMH	32,000 Lumens	5000K	240W	≥ 70	B4-U3-G4
E-APR32A-T540B	Type 5	750W PSMH	32,000 Lumens	4000K	240W	≥ 70	B5-U3-G4
E-APR32A-T340B	Type 3	750W PSMH	32,000 Lumens	4000K	240W	≥ 70	B4-U3-G4
E-APR19A-T550B	Type 5	320W PSMH	19,000 Lumens	5000K	150W	≥ 70	B4-U2-G2
E-APR19A-T350B	Type 3	320W PSMH	19,000 Lumens	5000K	150W	≥ 70	B3-U3-G3
E-APR19A-T540B	Type 5	320W PSMH	19,000 Lumens	4000K	150W	≥ 70	B4-U2-G2
E-APR19A-T340B	Type 3	320W PSMH	19,000 Lumens	4000K	150W	≥ 70	B3-U3-G3
E-APR13A-T550B	Type 5	250W PSMH	13,000 Lumens	5000K	100W	≥ 70	B4-U2-G3
E-APR13A-T350B	Type 3	250W PSMH	13,000 Lumens	5000K	100W	≥ 70	B3-U3-G3
E-APR13A-T540B	Type 5	250W PSMH	13,000 Lumens	4000K	100W	≥ 70	B4-U2-G3
E-APR13A-T340B	Type 3	250W PSMH	13,000 Lumens	4000K	100W	≥ 70	B3-U3-G3

E-APR SERIES



ACCESSORIES (SOLD SEPARATELY)



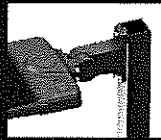
2-inch Adjustable Slip Fitter Mount

SKU: E-SF-ADJ-FTR

USE: For use with 2-3/8" O.D. (Outer Diameter) pipe or tenon.

MAXIMUM TILT: 45-degrees.

*Not intended for use as an up light.



Adjustable Direct Arm Mount

SKU: E-DI-FIXEDMNT

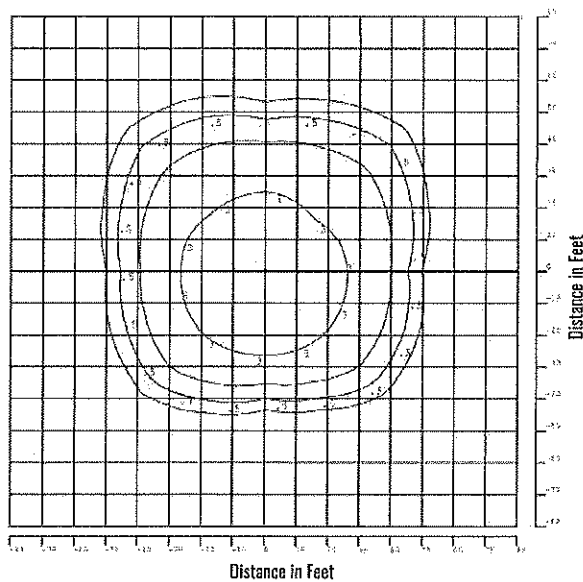
USE: For direct mounting to square poles with vertical adjustability from 0-45 degrees.

*Not intended for use as an up light.

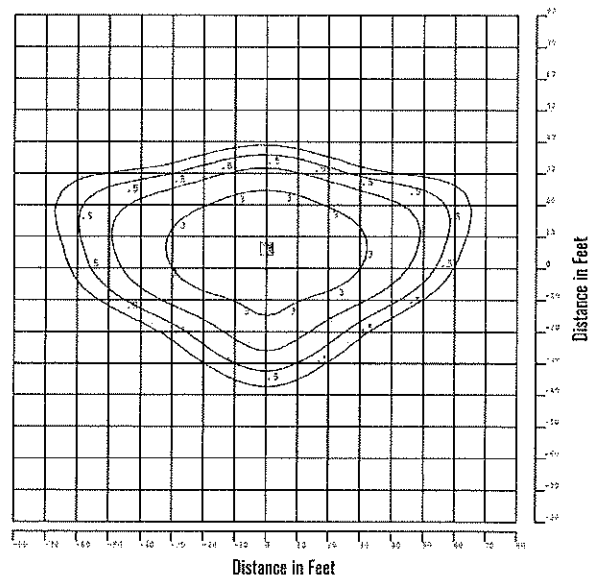


ISO PLOT CHART

Type 5 Distribution (symmetric distribution), used for the inner medians of parking lots. Type 5 optics product a symmetrical square distribution pattern that distributes light equally on all sides of the fixture. Type 5 fixtures are universal for most area lighting applications.

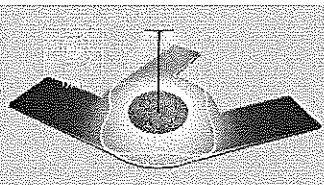


Type 3 Distribution (forward throw distribution), used on the perimeter of parking lots or in applications where you want to throw the light out in front of the fixture. Often Type 3 Distribution is used with the 2" Adjustable Slip Fitter because you can tilt the fixture to get the light to go further away from the pole if desired.



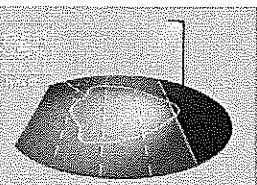
E-APR19A-T550B

- 20' mounting height
- IES Classification Type 5



E-APR19A-T350B

- 20' mounting height
- IES Classification Type 3



Note: All published photometric testing performed to IES LM-79-08 standards. Fixture photometry was completed on a single representative fixture. Foot candle levels are noted inside graph.

E-PS Series

Square Steel Pole

E-PS4 (4 INCH)

SKU	HEIGHT X WIDTH X WALL THICKNESS	BOLT CIRCLE RANGE	ANCHOR BOLT SIZE	STRUCTURAL WEIGHT OF POLE
E-PS4E15G(*)DB	15' x 4" x 0.120"	8" - 9"	3/4" - 10 x 17" + 3"	108
E-PS4E17G(*)DB	17' x 4" x 0.120"			120
E-PS4E20G(*)DB	20' x 4" x 0.120"			140
E-PS4E20S(*)DB	20' x 4" x 0.180"			200
E-PS4E22S(*)DB	22' x 4" x 0.180"			220
E-PS4E25S(*)DB	25' x 4" x 0.180"			245

E-PS5 (5 INCH)

SKU	HEIGHT X WIDTH X WALL THICKNESS	BOLT CIRCLE RANGE	ANCHOR BOLT SIZE	STRUCTURAL WEIGHT OF POLE
E-PS5E17G(*)DB	17' x 5" x 0.120"	10" - 12"	3/4" - 10 x 17" + 3"	160
E-PS5E20G(*)DB	20' x 5" x 0.120"			185
E-PS5E22S(*)DB	22' x 5" x 0.180"			305
E-PS5E25S(*)DB	25' x 5" x 0.180"			330
E-PS5E27S(*)DB	27' x 5" x 0.180"			350
E-PS5E30S(*)DB	30' x 5" x 0.180"			380

E-PS6 (6 INCH)

SKU	HEIGHT X WIDTH X WALL THICKNESS	BOLT CIRCLE RANGE	ANCHOR BOLT SIZE	STRUCTURAL WEIGHT OF POLE
E-PS6E27S(*)DB	27' x 6" x 0.180"	11" - 13"	1" - 8 x 36" + 4"	508
E-PS6E30S(*)DB	30' x 6" x 0.180"			520
E-PS6E35S(*)DB	35' x 6" x 0.180"			540

*Select Drill Configuration Note: Tenon measures (2.38" O.D. x 4.0" L)



Recommended Use

- Parking lots
- Roadways
- General area

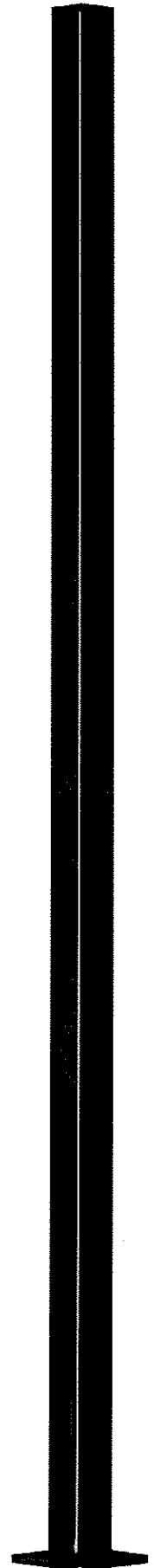
Certifications



Construction & Materials

- Mechanically cleaned and coated with an electrostatically applied, baked-on powder top coat
- Painted in dark bronze
- Partially galvanized anchor bolts and hardware included.
- Anchor bolts are made from carbon steel bar conforming to AASHTO and ASTM Grade-55 requirements
- Base is supplied with slotted anchor bolt openings for range of bolt circles to be utilized
- Base plate telescopes the pole shaft and is circumferentially welded top and bottom

- A standard full base cover is included with each pole and made from ABS plastic; secured together with two plastic hand push rivets
- Rectangular shaped handhole is furnished and includes a steel cover, attachment bar with machine screw
- Handhole is located 18-inches above the pole base
- Ground lug is provided near the handhole and includes a hex-head bolt and nut
- Removable pole cap is included with each pole configured for fixture side mounting or no drill
- Poles with tenon top configuration do not include a pole cap



E-PS Series

EPA INFORMATION - EPS4 (4 INCH)

HEIGHT X WIDTH X WALL THICKNESS	BOLT CIRCLE RANGE	STRUCTURAL WEIGHT OF POLE	ANCHOR BOLT SIZE	80 MPH		90 MPH		100 MPH	
				MAX EPA	MAX FIXTURE WEIGHT	MAX EPA	MAX FIXTURE WEIGHT	MAX EPA	MAX FIXTURE WEIGHT
15' x 4" x 0.120"	8" - 9"	108	3/4"	14.1	353	10.6	265	8.1	203
17' x 4" x 0.120"		120		11.4	285	8.4	210	6.2	155
20' x 4" x 0.120"		140		8.1	203	5.6	140	3.7	93
20' x 4" x 0.180"		200		14.4	360	10.8	270	8.2	205
22' x 4" x 0.180"		220		12.2	305	9.1	228	6.8	170
25' x 4" x 0.180"		245		9.6	240	6.7	168	4.8	120

EPA INFORMATION - EPS5 (5 INCH)

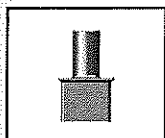
HEIGHT X WIDTH X WALL THICKNESS	BOLT CIRCLE RANGE	STRUCTURAL WEIGHT OF POLE	ANCHOR BOLT SIZE	80 MPH		90 MPH		100 MPH	
				MAX EPA	MAX FIXTURE WEIGHT	MAX EPA	MAX FIXTURE WEIGHT	MAX EPA	MAX FIXTURE WEIGHT
17' x 5" x 0.120"	10" - 12"	160	3/4"	19.5	488	14.7	368	11.1	278
20' x 5" x 0.120"		185		14.0	373	10.9	273	8.0	200
22' x 5" x 0.180"		305		19.5	488	14.6	365	11.0	275
25' x 5" x 0.180"		330		15.6	390	11.3	283	8.1	203
27' x 5" x 0.180"		350		13.1	328	9.2	230	6.3	158
30' x 5" x 0.180"		380		9.9	248	6.4	160	3.6	90

EPA INFORMATION - EPS6 (6 INCH)

HEIGHT X WIDTH X WALL THICKNESS	BOLT CIRCLE RANGE	STRUCTURAL WEIGHT OF POLE	ANCHOR BOLT SIZE	80 MPH		90 MPH		100 MPH	
				MAX EPA	MAX FIXTURE WEIGHT	MAX EPA	MAX FIXTURE WEIGHT	MAX EPA	MAX FIXTURE WEIGHT
27' x 6" x 0.180"	11" - 13"	508	1"	21.6	540	15.8	395	11.5	288
30' x 6" x 0.180"		520		17.6	440	12.2	305	8.3	208
35' x 6" x 0.180"		540		11.9	298	7.8	183	4.0	100

For mounting other manufacturers' fixtures, please reference EPA and maximum fixture weight information above.

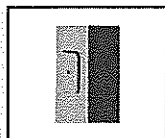
Included



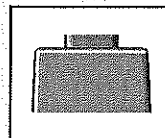
Welded Tenon Top
(Configuration T)



Pole Cap
(Configuration 0, 1, 2, 3, 5, 6)



Handhole



Full Base Cover
(Standard)

The Tooling Zone

To: The City of Springboro
From: Steve Iiams
CC: Brent Given

New addition and parking lot

To whom it may concern,

I Steve Iiams owner of The Tooling Zone, give permission to Ferguson Construction to submit plans for a permit with the City of Springboro on my behalf.

Sincerely,



Steve Iiams

President & CEO

Who Dey Investments LLC

APPLICATION—REZONING

CITY OF SPRINGBORO PLANNING COMMISSION

The undersigned requests the approval identified above. Site Plan Review approvals subject to expiration provided for in Section 1284.18 of the Planning and Zoning Code. For all approvals under this application, it is understood that it shall only authorize the approval described in this application, subject to any conditions or safeguards required by the Planning Commission, and/or City Council.

☐ Owner
☐ Agent
☐ Lessee
☒ Signed Purchase Contract

APPLICANT NAME: Easton Farm Partners

Address 155 West Central Avenue Springboro OH 45066

Telephone No. (937) 560-2535

Fax No. ()

Email Address larry@dillincorp.com mpione@borror.com

PROPERTY OWNER NAME (IF OTHER): Ted and Rebecca Hall Living Trust

Address: 605 North Main Street Springboro OH 45066

Telephone No. ()

Property Address or General Location: 605 North Main Street Springboro OH 45066

Parcel Number(s): 0414227055 **Existing Zoning District:** R-1

Proposed Zoning District: PUD-MU

Proposed Use: Mixed Use Development per provided plans and supporting documents.

The applicant or representative who is authorized to speak on behalf of the request must also be present at all meetings.


(Signature of Applicant and/or Agent)

3/26/2021
(Date)

Larry B. Dillin
Printed Name



February 2021

EASTON FARM
DESIGN GUIDELINES
SPRINGBORO, OHIO

Dillin

 BORROR

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PROJECT INTRODUCTION

Easton Farm looks to develop a new in-town neighborhood on the historic 'Easton Farm' site in the City of Springboro Ohio, located on the west side of State Route 741, between Anna Drive and Gardner Park to the north, and North Park and Tamarack Trail on the south.

The Easton Farm concept takes inspiration from the townscapes of small, historic Ohio villages, while incorporating updated ideas for streetscapes, parks, open spaces and connective pathways as a platform for a variety of market-rate residences and a mixed-use commercial district facing Route 741.

The residences within these new neighborhoods are scaled to the street, with most featuring garages placed along mid-block service ways that are landscaped,

thereby eliminating multiple front driveways and allowing for a series of un-interrupted sidewalks, bike paths, greenways, interspersed with community amenities that encourage pedestrian interconnection and walkability both within The Easton Farm site, and through to the adjacent city parks and neighborhoods.

These interconnected green spaces and pathways are designed to connect to smaller internal neighborhood parks and open spaces that are judiciously placed throughout The Easton Farm, and will feature play areas, shade structures, seating areas, dog parks and significant passive, and well-landscaped green spaces. Tree-lined streets will connect these residential neighborhoods to the mixed-use district close to S. R. 741 and to its retail, restaurant, office, central park, and service offerings.



Easton Farm
Illustrative Master Plan

MIXED USE NEIGHBORHOOD CENTER

The purpose of this Article is to allow for flexibility that will allow for the overall Easton Farm development to become a walkable, integrated mixed-use, in-town neighborhood with the following emphases:

- Building design criteria that allows for unified and creative interpretations of traditional and potentially more contemporary design vernaculars, motifs and building materials, provided the final design creates a visually complementary, integrated urban village environment;
- Residential and mixed-use neighborhoods, activity areas, and open space(s) crafted around a well-defined transportation/circulation network where pedestrian activity is strongly emphasized and judiciously integrated with vehicular streetscapes, service and parking areas;
- Identifying and locating market-driven retail, café, restaurant, personal care and/or business service offerings where they can best generate high pedestrian activity, along ground floor locations, below potential office space, hospitality, or residential units on upper floors;
- A site plan that can defines and accommodate options where commercial/retail, office, institutional and public spaces can optimally

be incorporated into the overall neighborhood fabric.

- Residential areas that are either built above, adjacent or within close/walkable proximity of new commercial uses, places of work or dedicated public open spaces and/or amenities;
- An emphasis on developing neighborhoods where new and existing residents and visitors can live, work and play, and interconnected with a multiuse path and linear park.

1. **Permitted Uses**

The The following uses are permitted within the mixed-use neighborhood adjacent to S.R. 741:

- (a) Specialty retail including apparel, fashion accessories, home goods, sporting goods, hobbies, gifts, specialty packaged/prepared foods, large(r) footprint retail/anchors (potentially up to 35k s.f., temporary or 'pop-up' retail/vendors, kiosk-based retail/ food vendors, service businesses, personal service businesses, financial services, grocery and or specialty foods market (not to exceed 15k s.f.) personal care services (salons and spas), professional retail services (for example, florists/

printing/copying, dry cleaner, shoe repair, real estate/escrow, or similar).

- (b) Restaurants, cafés, bistros, nightclubs and bars
- (c) Offices and professional services, medical, dental, pet care, financial/ investment service offices, real estate, escrow/ title, educational, training facilities, schools and specialized academic tutoring operations.
- (d) Multi-family, multi-level residential, with potential for portions of it to be above retail, office or other first floor uses.
- (e) Senior Lifestyle, multi-family, multi-level residential including; 55+ Active Adult, Independent and/or Assisted Living, and Memory Care Facilities. A portion of any such development(s) may be located over ground level commercial and/ or covered parking depending on the specific site condition and location.
- (f) Entertainment and recreational venues
- (g) Theater, cinemas or live performance venues
- (h) Conference and meeting facilities.
- (i) Parking, including street and surface lots/garages
- (j) Public and Institutional uses
- (k) Accessory structure uses
- (l) Open Spaces and Event spaces
- (m) Other, as approved by the Master Developer and the City consistent with goals set forth in this text.
- (n) Mixed Use - Ground Floor Uses: The ground floor of buildings in the mixed-use Retail/ Commercial neighborhood shall be restricted to retail business, restaurants/bars, cafés, professional services, and other uses that tend to encourage street level pedestrian-oriented activity. The Master Developer and City will work to encourage storefront designs that create a vibrant,

pedestrian oriented, neighborhood environment.

- (o) Mixed-Use - Upper Level Floor Uses: Offices, medical, and/or residential uses in the Multiple Use Retail/ Commercial Area buildings should be encouraged on any or all upper floors of the building where feasible.
- (p) Public and Institutional Uses: The Property may include a mix of community and city buildings, meeting halls, libraries, post offices, schools, public recreational facilities, museums, performing arts centers or other uses permitted by the Master Developer and the City, and may be located within or adjacent to open spaces and parks.

Where possible, civic uses should be sited and designed with key architectural features that help define its character, and/or to be at the end of street vista(s) or centered on an open space or plaza. Such uses may utilize shared parking, however any required off street parking shall be located in rear yards. Parking needs will be assessed by the Master Developer and City as part of any submission.

2. Lot Requirements

- (a) Lot requirements are defined in the following table based upon the usage classification indicated. The definitions for each classification appear below the table. If there is a question as to the appropriate usage classification for a user, the City will determine the most appropriate classification at its discretion.

USAGE							
	MINIMUM LOT WIDTH OR AREA	MINIMUM OPEN SPACE	FRONT YARD SETBACK	SUM OF SIDE YARD SETBACK ⁵	REAR YARD SETBACK	MAXIMUM OCCUPIED HEIGHT	MINIMUM OCCUPIED HEIGHT
Freestanding, Single Use Retail/Commercial Services ¹	50'	25% within the development	0'	0'	0'	2 stories	1 story
Multiple Use/Retail/Commercial Services ²	75'		0'	0'	0'	3 stories	1 story
Office ³	75'		0'	0'	0'	3 stories	1 story
Multi Family Residential Areas ⁴	5,000 sf		<10'	0'	15'	4 stories	-
Single Family Attached	8-30 du/ac		<10'	0'	0'	*2 stories	1 story
Multi-Family Attached	18-65 du/ac		<10'	0'	0'	4 stories	1 story
Accessory Structures	500 sf	-	-	-	-	2 stories	-

* except where roof deck occupancy is permitted

- Freestanding, Single Use Retail/Commercial Services (In 'pad' or 'lot' locations.)** These uses include: non-specialty retail, medium/large format 'anchors', commercial businesses, personal care services, pet care or veterinarian services, financial services, restaurants/ bars, cafés or specialty foods, educational services, automotive services, sales and/or repair, child daycare centers and other uses located on individual lots.
- Multiple Use Retail/Commercial Services.** Primary uses include specialty retail, apparel and/or accessories, home goods, gifts, medium or larger format general merchandise retailers including department stores, restaurants/bars, specialty foods, entertainment facilities, public services, retail service businesses, business and personal services, personal care services pet care or veterinarian services, small professional offices and/ or financial services, and educational services, to be built along a streetscape orientation that fosters a lively pedestrian atmosphere.
- Office.** Primary uses include professional offices and services, medical offices and/or laboratories, essential services including walk-in medical and/or pet care, and educational uses. Secondary uses may include live/work residential per the discretion of the City.
- Multi-Family Residential Area.** Area includes single-family attached dwellings, such as town houses or duplex units, multi-family attached dwellings, apartments and condominiums, assisted and/or independent living residences, senior housing, and live/work hybrid residential units. Lot widths, Maximum Occupied Height and Minimum Occupied Height are set forth separately for Single Family Attached and Multi-Family Attached units.
- Structure Setbacks.** To foster and create a series of traditionally inspired, urban-scaled, pedestrian oriented, walkable streetscape environments, within an integrated mixed-use, urban village district, no more than 50% of any Structure shall be set further back than the setback or build-to line location that fronts on a pedestrian walk area. Where setbacks do occur, they should be to create visual interest, break down scale and massing, and should not form a large break in the street wall effect, except to create a pocket for entries, arrivals, small public or semi-public gathering spaces, or to work around a service or operational requirement.

3. Site Development

In keeping with the design intent to create a series of integrated mixed-use neighborhoods, this section for public roads may vary from Springboro subdivision standards and shall be submitted for review and approval to the Master Developer and the City.

(a) **Street Design and Materials**

Beyond the likely predominant use of either conventional asphalt or concrete use of differing specialty paving materials is strongly encouraged. Such materials as brick, cobblestone, pavers, colored or infused concrete, stamped and colored asphalt pavement and/or other pavement products, to demarcate special pedestrian walk and/or activity zones, dual pedestrian/vehicular crossing areas, feature street surfaces, crosswalks, pathways, unique 'arrival' zones, plazas and entries. Where viable, such areas may be set to 'zero curb' elevations to facilitate ease of pedestrian use, and as a visual cue for traffic calming in these specific areas.

Such zones or feature streets may have vehicular and pedestrian areas segregated by use of either fixed or removable bollards that can be removed for programmed or promotional events. For specific site development areas within public roadways, pavement and substrate design must be based on calculations that anticipate future traffic conditions and must be submitted to and approved by the Master Developer and the City. Creation of roadways featuring landscaped islands, boulevards, arrival courts and roundabouts are also encouraged where feasible.



A pedestrian-friendly street using flush curbs, bollards, and richly textured materials to demarcate various spaces.



Street crossing emphasized with specialty paving and bollards.



A finely detailed arrival court serves as an interface between vehicular and pedestrian space.

(b) Street Right-of-Way (ROW)

The Easton Farm Conceptual Development Plan contains a series of right-of-way ("ROW") widths. All initial development will front on a first-phase series of streets that will become the primary access points into and through the development, from both S.R. 741 and Tamarack Trail. These roadways will vary in design depending on service level for that particular street or access way, service drive or entry.

There are five right-of-way widths, from a multi-lane, divided boulevard, to internal streets that accommodate a center turn lane, parallel parking on either side, to a one-way street, with both angle-in and parallel parking.

The key dimensions include travel lanes of twelve feet (12'), twenty-four foot (24') (face of curb to face of curb) two-lane roadways, and eighteen foot (18') one-way road lanes, parallel parking spaces at eight feet (8') wide (face of curb to edge of pavement), and a sidewalk/landscape area on each side of no less than twelve feet (12'), featuring a sidewalk of no less than six feet (6') on each side. The remaining areas on each side to be landscape buffer, up to either a building, setback line, landscape area, or adjacent building or property line.

Additional dimensional width is added for the following uses:

- For an additional parallel parking lane, add 8' feet width x 23' feet per space.
- For each additional travel lane and/or center turn lane, add twelve feet (12') for through lanes and ten feet (10') for turn lanes.
- To add angled-in parking on any one side, add an additional twenty feet (20') for angled parking and eighteen feet (18') for one-way drive aisles.

- The primary landscaped median areas in the center of two way boulevard entrances roadways shall be no less than ten feet (10') face of curb to face of curb. Current plan shows a portion of each entry median at Easton Way and S.R. 741 as concrete with a width that varies between three feet (3') and seven feet (7').

These dimensions ensure that streets developed within the mixed-use urban village are developed to help shorten/ make more secure pedestrian crossings and inhibit drivers from traveling at high speeds within and through the development/ neighborhoods.

For specific site development enhancements that do not fall within these basic ROW guidelines, such as roundabouts, arrival courts, and streetscape features (such as street furniture, water features, sculpture, accessory structure, hardscape/ landscape features) and/or in combination with on-street parking, deviation from these ROW requirements may be permitted, with the approval of the City. This will be assessed in context with a specific development's design intent, in context with any existing adjacent development, especially if any such deviation adds to the pedestrian experience of the Property.

(c) Utility Easements along Streets

All public street utility easements shall fit within planned street ROW's or in dedicated service lanes where possible, and are to be located under pavement, landscape and sidewalk areas as necessary. Deviation from this requirement will be considered by the Master Developer and City, and only for specific site conditions that prove a variation is necessary.

(d) **Street Radii Corner Radii & Clear Zones**

Corner curb radii shall be between eight feet (8') and twenty-five feet (25') as determined by the intended use (residential areas up to fifteen feet (15'), and mixed use areas up to twenty-five feet (25')). These tighter turning radii are intended to shorten pedestrian crossings and inhibit drivers from turning corners at high speeds. To allow emergency vehicles (e.g. fire ladder trucks) to turn corners, a twenty-five foot (25') radius Clear Zone may need to be established free of all vertical obstructions including but not limited to street lighting poles, decorative bollards, wayfinding signage, sign poles, fire hydrants, utility boxes, or dense vegetation landscaping or street trees. Curve and radius data and intersection details for public roads shall be submitted for review and approval to the Master Developer and the City. Where viable, the curve should also be used to create 'bump-out' landscape and pedestrian areas that cap rows of either parallel or angle-in parking spaces.

(e) **Utility and Equipment Locations**

All utility, telecommunication and landscape irrigation distribution lines shall be located underground preferably within street right-of-ways, with all visible, in-ground utility access boxes placed as to not interfere with building pads, sidewalks, pathways and other pedestrian zones, light poles, wayfinding, directional or other street signage, parking areas, landscaping other than trees. This would also include submitting integrated plan(s) prior to installation, from both the master developer and any other subsequent developer entity that needs to place such devices as to not be in conflict with any existing or prior proposed utility items.



Small corner curb radii reinforce pedestrian-friendly areas by shortening crossing distances and slowing cars.



Small turning radius reduces crosswalk distances, while larger turning radius is accommodated with bollards and a flush curb.



Utility cabinets should be placed discreetly (to the side or rear of building) and screened with plant materials.

This is to avoid redundant placement of light poles and fixtures or to avoid the potential to visually clutter or impede corners, sight lines, pedestrian ways or streetscapes. All utility providers and/or governmental agencies requiring similar placements for transformers, traffic control equipment, telecommunication pedestals, electrical cabinets, metering, exposed pipes and/or valves, and any other mechanical equipment or devices requiring above-ground placement in areas visible to the public and residents that are considered common areas, will also be required to submit and integrate with the master developer and any other adjacent private development entities. Any such fixtures and devices should be visually screened from and/or within public pedestrian and vehicular zones, preferably within landscape beds, landscaped parking 'islands' and/or landscape buffer areas.

- (f) **Utility and Equipment Screening**
All exposed structures requiring open-air locations, particularly rooftop mechanical/HVAC equipment, utility and/or other functional hardware, whether located atop, attached, or separated from the building it services, shall be adequately screened from public view. If atop a structure, these features shall be screened behind parapets, decorative roof elements, or screen enclosure walls for a visual distance of no less than one hundred fifty feet (150'), or beyond any public facing elevation that can be viewed from any public view point, or from the highest public vista possible looking down onto such a roof structure. Screen devices shall be fabricated to match and/or enhance the overall architectural massing, with materials and colors harmonious with the overall building(s) or block where



Dumpster are completely screened on all sides. Wall and gate materials are consistent with the building architecture.



Screening of tenant equipment, such as trash receptacles, storage bins, and grease containers with an architectural fence.



Loading dock and compactor screened with a building wall and landscaping.

applicable.

All such devices are approved at the discretion of the Master Developer and the City. Where possible, such elements as generators, electrical and telecommunication cabinets should be set into lower grade areas, or within the building walls either open air or within a separated utility room or service area. Such features shall also be adequately screened by either screen walls or dense landscaping if they are in open area settings.

(g) **Service/Loading Area Screening**

All building and/or tenant back-of-house functions, such as trash containers, grease traps, recycling bins, compactors, or any other functional need, located outside of any interior lease area, shall be grouped in an approved common area/service zone(s), within individual blocks located within that specific development block and/or complex, and placed or screened as is practical from public view, preferably in rear, or side yard or interior service rooms.

Where exposed to public view, such service zones shall be adequately screened with architecturally complementary access doors and screen walls, and/or berms or dense landscaping.

Recessed and/or raised service docks that face any public streetscape or parking area, or visible at the rear of any building, shall be screened from any public right-of-way with screen walls and/or screen doors, of a height and scale that will adequately hide any service vehicles that may use such zones.

Screen wall, doors and/or roof enclosure

elements are to be constructed of materials, colors, trim and/ or patterns to mimic the primary architectural character of that particular building.

Exceptions are fully screened service zones, located between or surrounded by (or reasonably expected to be surrounded in the future by) other buildings and can provide service bay access that is not directly visible to public view. This would also include a roof or trellis element that screens the service bay from being seen from higher floors overlooking such zones.

Service entrances located at the rear of buildings shall be screened, unless such areas are comprised only of only service access doors. On street, curbside loading areas need not be screened from view, but should be made dual-purpose to encourage other uses after hours, with clear signage as to the hours such potential uses are allowed.

Hours of loading/servicing shall be coordinated with the Master Developer and/or any subsequent 'association' to limit service hours to non- operational, or lesser traffic impact times of the day.

(h) **Loading Space Requirements**

Within Easton Farm retail and commercial areas, loading may be accomplished at specific curbside locations for smaller, in-line retail and service tenants, as practical and necessary depending on distance from a screened delivery bay. Curbside loading spaces should be made dual-purpose where practical and functionally viable for daytime, public use.

- (i) **Loading in Required Yards**
Loading spaces in other than high density mixed use retail/commercial areas, shall not occupy any part of a required yard setback, except locations at rear yards, per the discretion of the City.
- (j) **Screen Wall Construction and Materials**
All service bay screen walls will mimic building design, materials, colors of walls and fences, if visible from any public right-of-way, and shall be uniform and compatible with the base-building architectural style, color and building materials of the principal building and its surroundings.
- (k) **Fencing.**
Any or all fencing is to be painted or powder-coated metal railing, set into ground or bolted/incorporated into a wall or between pier supports. Chain link fences are not permitted along any visible public right-of-way, and may be placed only in service areas screened from public view.
- (l) **Open Space Area Requirements**
Open space shall be provided at ratios of 25% in Residential areas and 5% in mixed-use areas as per the "Usage" Table in this chapter.

4. Building Design

- (a) **Architectural Character**
The following architectural design guidelines are meant to achieve the result of establishing consistent, high-quality design, materials and construction that works with the site and streetscape guidelines to establish and reinforce the mixed-use district 'neighborhood' design and planning goals.
- (b) **Orientation to Street**
Buildings shall be architecturally oriented to frame any immediately adjacent street(s) to create a 'street wall' effect, with the main/primary entrance(s) located, facing the street, public right-of-way or courtyard, and for larger-format users over seven thousand square feet (7,000 sf), facing a relevant-sized surface parking area, unless deviation is deemed appropriate, per the discretion of the City. In cases where there is an anti-access easement or access is not permitted from a particular road or public right-of-way, the building shall not be required to face the subject road or right-of-way. Buildings located on an intersection shall have the main entrance oriented toward the major street and any facade facing other streets shall



Buildings should be oriented with fronts, including storefronts and entrances, toward the street.



Windows should be expressed with a vertical orientation.

have facades that ‘turn the corner’ with similar/complementary design details, window openings and architectural character.

(c) **Orientation of Windows and Doors**

All windows and doors, openings or frame fenestrations located within an opening shall be ‘vertical’ in orientation, meaning a primary design character of most building openings reflects a more top to bottom aesthetic, mimicking traditional storefronts and window motifs. window features including transoms, divided lights, and sidelights are encouraged, along with storefronts, doors and framing components, which follow vertical enhancing design elements. Sliding commercial-grade glass doors, folding glass windows and butt-jointed glazing may also be allowed, but only if window and/or door orientations are also vertical in nature. Any sliding doors must pocket out of view, and all folding windows should not encroach into an exterior walk or public area beyond a tenant storefront closure line, unless that line is part of the tenant lease area, as in an exterior patio or display space. Uninterrupted linear ‘strip-style’ windows are not permitted, since they do not reinforce the design vocabulary. The City may consider specific exceptions, primarily related to functional use, provided that the overall public facing building design is in keeping with the goals of the mixed-use district. In more modern or contemporary interpretation(s), design elements such as overall building massing, window orientation and rooflines should take inspiration from motifs.



Glass should be translucent and provide visibility in and out.



Acceptable building materials include brick, stone, decorative block, precast masonry and EIFS.



Building materials should be applied in a variety of stylish and creative applications.

(d) **Glass Materials**

Clear or colored glass, translucent, sandblasted and/or decorative pattern glass that can emit light through is encouraged. Retail and/ or commercial storefronts should be predominantly clear glass at any street level opening. Where a clear window opening is desired, but for functional reasons is not viable, an internally backlit shallow display box-out is desired. Translucent, patterned or colored glass panel will be allowed in all other applications. Other options, including back painted, fritted, sandblasted, color or non-illuminated translucent glass may be acceptable where structural elements or back-of-house functions or unoccupied space would be visible behind. Black, opaque, spandrel and reflective glass materials are discouraged except in façade areas where its placement is a key part of a final design aesthetic.

(e) **Building Materials**

Building materials shall be commercial grade, and applied in variety of stylish, creative and clever applications across the expanse of all visible public-facing elevations. Acceptable materials include: pre-cast masonry, brick, decorative block, cementitious siding, wood (both new and reclaimed in specific applications), natural stone, cast or cultured stone, exposed metal structure and/or architectural details and/ or surfaces where viable, stucco, EIFS or materials of equal appearance consistent with the acceptable architectural character outlined in these Architectural Guidelines Plywood, unadorned corrugated siding, vinyl siding and plastic panels are prohibited from use in publicly visible locations.



Partial sloped roof on the left and a parapet on the right with cornice detailing.



This roofline does not effectively screen rooftop equipment. A parapet wall should be extended up to fully hide the equipment.



Varying roof materials with changes to building massing, including shingles of various colors, and standing seam metal.

(f) **Rooflines**

Flat roofs are permitted, however, parapets or the underside of roof eaves should showcase some form of cornice or similar design element, masonry and/or brick trim and/or patterning, decorative blocks/brackets, moldings or some form of architectural emphasis along the roofline to tie that element to the building massing below. Where an adjacent building's façade, which adjoins it, has a similar roofline height, a variation of the parapet or roofline is required

NOTE: All rooflines must be able to fully screen any and all HVAC/utility or functional apparatus from public view from a distance equivalent to the parallel height of that roofline as viewed downhill from any distance into the project. If that proves difficult, a subsequent parapet screen may be installed on roof to provide additional screening.

(g) **Visible Roofing Materials**

Slate style shingles, fiberglass shingles and either copper or painted/ powder coated standing seam metal are



Large format retail using massing elements and detail to visually reduce the scale of the front façade.



Building heights should vary in coordination with changes in building massing.



Architectural features such as towers may extend above the typical roofline.



Changes in building massing should include variations to rooflines, setbacks, materials, and include features such as "pop-outs".

permitted materials, for sloped roofs per the discretion of the City. Where possible, patterning of shingles colors for more vertical roof surfaces is highly encouraged.

- (h) **Building Heights**
Heights shall be consistent with those of other similar uses.
- (i) **Larger format retailers, entertainment venues or other commercial uses, where taller interior spaces is the normative**
Heights of the front facades should use massing elements and materials to blend in with any adjacent structure(s) as to not overwhelm the overall visual appearance.

Subject to the restrictions on Maximum Occupied Height and Minimum Occupied Height as set forth in this chapter, individual structures should feature a variety of heights, utilizing elements such as roofline variation, decorative towers or portal elements, steeples, pediments, clerestories, dormers or domes, consistent with the overall architectural character and these Architectural Guidelines. The height of enclosed unoccupied building elements may not exceed a maximum height up to sixty (60') from main street level grade.

Purely decorative unoccupied elements such as mansard and/or peaked roofs, towers, flagpoles, spires and steeples may be located on top of both occupied and unoccupied enclosed space, to a total building height of no more than eighty-five feet (85'), excluding decorative metal finials or spires.

- (j) **Building Massing**
Individual buildings and any attached structures should strive for complementary building massing, reminiscent of traditional townscape/



Building massing used to break up large monolithic appearance on a building elevation.



Changes in color coordinated with building massing, but not so dissimilar as to clash.

streetscape patterns. “Monoplanar” or monolithic appearance and surfaces, with little variation in setbacks, wall fenestration, or rooflines are not permitted.

Building massing should take into account adjacent buildings, property uses, sightlines, identity, and relationship to its site to accomplish building a contextual variety in its overall massing. As noted above, elements such as rooflines, setbacks, decorative architectural elements, visual openings and wall fenestration

elements such as 'pop-outs', window openings, trellises, pergolas, verandas, entrances, piers, pilasters, decorative panels, or grilles, and eaves or cornice lines are highly encouraged, and visual articulates a structure's overall massing and creates pedestrian scale to its overall context.

5. Colors

(a) Color Variation

The use of sharply contrasting colors on significant swaths of any building(s) facades is discouraged, but may be used for accenting or enhancing an architectural elements. If used either as an accent or as some form of tenant identity, they should be limited to specific architectural elements, details or specific and limited surface areas. Their use in tenant design areas however is encouraged, but subject to tenant design guidelines established by the City for their particular building or development.

Color schemes should be kept to no more than one or two field or trim colors on any given individual facade, unless deviation from this is deemed appropriate by the Master Developer and the City. All exterior colors and finished material surface colors are subject to City approval.

(b) Color Consistency

Color scheme(s) should help to visually tie all parts of building massing(s) together. Monolithic or singular color schemes and appearances are discouraged, but monochromatic color palettes featuring shade variations are acceptable. Typically, the color that is used in the storefront area may be repeated in the upper story windows or to accent a roofline element/cornice

area. Colors used at the front façade should be utilized on the sides and rear of buildings.

6. Landscaping-(Also covered in Landscape Section)**Plant Material Selection**

Plant materials shall be chosen which are indigenous, moderately fast growing and require low maintenance. All planted surfaces, whether in ground or in raised landscape beds shall be fully irrigated and maintained in good working order. The landscape design shall incorporate the total development site, be in context with adjacent users, and consist of a palette of plants with year round appeal which might include annuals, perennials, shrubs and trees.

(a) Surface Parking Area Screening

Parking areas and driveways shall be screened from adjacent public and private streets and open spaces to a height of 30 inches above the finished parking surface. Taller shrubs and low-profile ornamental trees are permitted in areas where screening of outboard utility or service areas is required.

(b) Surface Parking Area Landscape Area Requirements

A minimum of eight percent (8%) of any one total interior surface parking area shall be landscaped with planted islands or landscape island beds for every linear run of no more than 16 parking spaces. Exceptions to this rule are if the entire run is no more than 15 spaces, or if a landscaped cross-surface walk is aligned within or part of a larger landscape island or row, or if an irregularly shaped island creates a naturally larger landscape bed area where the edges may be more low plantings, and the interior may be more lawn in season. A minimum of two (2) trees, of no less than 2" caliper with lawn shall be planted on all interior islands within any row

of surface parking area. The use of all mulch islands shall be prohibited.

(c) **Exemptions**

Parking area landscape requirements shall be waived if the surface parking area is not, nor will ever conceivably be, visible from a public right-of-way, or is considered a temporary use as determined by the Master Developer.

(d) **Planting Island Requirements**

Planted islands shall be at least 9' in width (face-of-curb-to face-of-curb) and the length of the adjacent parking space with a pervious surface area adequate for proper root aeration and expansion. Creative alternatives are subject to review and approval of the Master Developer.

(e) **Screening Materials**

Where plants are used as screening it shall be opaque year round. Plants shall be of a height and density so as to provide the full desired effect within three (3) growing seasons.

(f) **Street Trees**

All streets shall be planted with regularly spaced shade trees, with a minimum 2" caliper. Trees shall be placed and selected in consultation with the Master Developer.

(g) **Street Furniture**

'Street furniture/furnishings' are key features of public spaces that can greatly facilitate in creating vibrantly-used pedestrian spaces along all streetscapes, open spaces and connecting outdoor areas. These features include such basic items as benches, trash receptacles, bollards, drinking fountains, wayfinding and street signs.

Other elements include game areas and surfaces, moveable tables, chairs, outdoor lounge seating groupings,



Parking screened with a continuous evergreen hedge.



Street trees in a tree lawn between the curb and sidewalk.



A variety of furnishings activate the street and provide necessary "creature comforts".

picnic benches and tables, market umbrellas, outdoor fireplaces, water features, children's play areas, sculpture and other durable, exterior public art.

Street furniture shall be chosen and placed in consultation with the Master Developer and the City, and shall be located within agreed zones to maintain a clear pedestrian path.

Street furniture shall be made of materials consistent with extended public use. These materials include painted or powder-coated metal with a matte finish; wood in either a stained, painted or treated finish; outdoor grade fabrics and canvas, and in certain instances, high-impact, composite or resin-based materials that mimic natural finishes that are weather and fade resistant.

All street furniture and public features shall always be maintained in good working order and appearance by their respective owner(s), and repaired or replaced as necessary.

Street furniture shall be consistent in material, color and style along both sides of any street street in public areas fronting onto other developments. Within any one development parcel, a developer may utilize differing selections, provided they are part of an overall design scheme, placed appropriately and upholds the pedestrian-friendly spirit of The Easton Farm. All street furniture elements, either new or replacement that vary in color, material, number, placement or size from prior approved plan(s) are subject to Master Developer and approval.



Games can be provided to activate the street, and should be coordinated with seating.



Pedestrian-scale lighting. Cut-off style fixtures reduce glare and light pollution.



Taller fixtures for use in vehicular applications, but still with pedestrian detailing consistent with the setting.

- (h) **Registered Professional Required**
Each applicant shall utilize a registered landscape architect who shall work with the Master Developer on all landscape design, landscape plans and/or alterations.
- (i) **Maintenance**
The Master Developer may supplement these Architectural Guidelines by adopting, through rules and regulations, more specific Landscaping Standards that will apply to this Property.

7. Outdoor Lighting

- (a) **Outdoor Lighting.**
All outdoor lighting of buildings, surface parking areas, service zones, or any non-illuminated, surface mounted project signage or tenant identity signage, shall be directed so as to prevent glare on adjacent properties and streets and to shield the lighting from residences, to the maximum extent feasible.

All fixtures shall be LED or similar product, and their placement, along with all support posts/poles and brackets, shall be consistent in fabrication quality, and selected to enhance the overall design character of the urban village theme.

Street Lighting.

Consistent and commercial grade fixtures for street and pedestrian lighting shall be provided on all streets, sidewalks and pedestrian pathways, including public gathering/event spaces, both public and private within the Property. Street and pedestrian poles shall be located at all intersections and along sidewalks at intervals consistent with the urban village design character.

Note: All lighting fixtures, light levels, placement and scale needs to be coordinated with any existing adjacent

or abutting streetscape, developed property, or proposed project to avoid redundant fixtures and 'over-lit' conditions along these interfaces.

Street lighting shall be scaled to the pedestrian, with pedestrian street support posts no less than 12' high to underside of actual fixture, which shall be the recommended minimum height, including 30" minimum bases and tapered support posts of no less than 6" diameter at the base.

Pedestrian light poles are to be featured within all community gathering space areas, and shall feature exterior outlets in the base for seasonal and functional needs. Street light fixtures shall be painted or powder coated metal, with a matte finish, consistent in color and style on both sides of any street and chosen in consultation with the Master Developer and the City. An alternative for public spaces, gathering areas, and pedestrian pathways are low-profile LED, internally lit bollards, placed at intervals to fully illuminate all pedestrian surfaces, or concealed, undermount lighting that surface illuminates or 'washes' pedestrian walk or gathering areas.

A street/pedestrian lighting plan, showcasing a fixture schedule, light levels, and placement within the Property, and showing any adjacent existing fixture(s) and placement, shall be submitted to the Master Developer and the City for approval.

(b) **Decorative Accent/Architectural Lighting**

All decorative accent/architectural lighting, meant to illuminate a building feature or surface(s), as well as any surface mounted, non-illuminated signage that is a permanent feature of the overall building or site design, shall also be submitted to the Master Developer and the City for review and approval.

8. Outdoor Storage

(a) **Screening.**

Outside storage of merchandise, raw material, finished products and/ or equipment must be visually screened from any publicly visible right-of-way. All tenant service areas, including service docks, back-of-house areas, trash containers or compactors, recycling bins, grease traps and general service locations must be fully screened with non-lockable closure gates on one side for access and removal.

Large format uses requiring large service bays shall screen all areas with walls and/ or screen doors that fully hide all service vehicles, both lengthwise and in height, and are to be consistent materials/colors that complement their base building architectural character. Exception: if the service bay is fully screened from any public view, then these requirements may be waived, but only upon review and approval of the Master Developer and the City.

Initial site planning should work to negate or minimize views from any public areas or such large bays wherever possible. Masonry, brick, stone, decorative block, composite materials, pressure treated wood and/or painted or powder-coated metal are all

acceptable materials to screen attached or free-standing service areas and bays.

Storage facilities shall feature walls that reflect the surrounding architectural character, tall-growth landscaping, and enclosure doors featuring decorative fencing and opaque metal screen backing.

For uses that require more than occasional access, and are outboard or freestanding from the main building area, three sides shall be screened, but screened areas must not be visible from the public right-of-way, and shall feature unlocked, decorative and opaque doors to one side for clear access. Chain-link fencing with inserted slats, or plastic-coated walls and/ or support wood posts, are not permitted in any publicly viewable area.

(b) **Storage/Screened Service Area Design.**

Any required outdoor storage building(s) or structures shall be constructed in the same style/materials and colors as their surrounding buildings or primary project character. The overall concept is that they should not look 'temporary'.

(c) **Visibility from Public Right-of-Way.**

Where possible, locate any outdoor storage structure away from the public right-of-way or view behind the main building(s).

(d) **Accessory Structures.**

Accessory Structures may contain auto and accessory vehicle parking, developer/landlord/tenant or resident storage, building or amenity functions/uses, general maintenance storage, or trash/recycling bin enclosures. Prefabricated shed structures are not permitted.

9. Vehicular and Pedestrian Circulation.

(a) **Safety.**

The safe and functional movement of both vehicles and pedestrians, on and off-site, shall be provided as the primary consideration in designing transportation networks within the Property, and all site plans shall be crafted to minimize such conflicts wherever possible.

(b) **Pedestrian Circulation.**

Pedestrian circulation is highly encouraged, and can be accomplished via sidewalks, crosswalks, cross-surface lot walkways, community gathering spaces, plazas, terraces, seating and amenity zones, hard-surface paths, bike paths, and permeable pathway surfaces. These varying pathways should be crafted to connect between all buildings, and within and outboard of specific development parcels. Early site planning should make this a core design consideration within and along the edges of the Property.

(c) **Sidewalks.**

Where appropriate, sidewalks shall be provided on each side of every public and or semi-private street or drive,



Provide sidewalks on both sides of each street and demarcate crosswalks. Use of curb-extensions shortens crossing distances.



Sidewalk on a low-volume residential frontage with street trees in a tree-lawn.



Sidewalks along a high volume street with parking spaces should allow for street trees and/or landscape beds at intervals for optimal pedestrian access.



Sidewalks in an urban setting. The minimum width should be Unrestricted with furnishings.

and shall be intentionally developed to interconnect different land use areas and open spaces. Sidewalks should be no less than 6' wide.

(d) **Sidewalks and Buildings.**

Pedestrian connections, including crosswalks, sidewalks and pathways shall be provided along any surrounding streets/driveways/drive isles, along the front, side or back of any building or group of structures, on all specified and dedicated streetscapes, both public and private.

(e) **Sidewalk Width.**

Sidewalks along the primary or front facades of buildings in the height density Multiple Use Retail/Commercial areas shall be an average of no less than twelve feet (12') back of curb to building face, incorporating a dedicated landscape/tree well area at the curb line of no less than six ft. x five ft. (6'x5'), and at intervals of every other parallel space (46' center to center) and approximately every fourth head-in space (42' center to center). Sidewalks at sides and rear of buildings may be of lesser width, but

in no case less than six feet (6') wide.

Sidewalk and pathway widths within other areas are to be determined by Master Developer prior to City review for each application as submitted, but in no case are to be less than five feet (5') wide.

(f) **Open Space/Gathering Spaces**

Usable and functional open spaces and programmable event spaces are required as part of the multiple use areas, and adjacent neighborhoods. This definition may also include/allow for landscaped traffic islands, pedestrian plazas, arrival courts, landscaped sidewalk/pathway areas, game areas, public amenity features such as water features, fireplaces and seating areas, general green space, playgrounds, tot lots, pocket parks and other recreational uses. Open spaces between buildings that create landscaped courtyards or walkways to connect rear parking areas, or to adjacent neighborhoods, or other nearby anchors are strongly encouraged, and should be 'programmed' to encourage pedestrian activity, seating areas and amenities.



Parking along surrounding arterial roadways shall require setbacks to allow for landscape buffers.



Buildings should be sited to allow for screening when parking is located to the side or rear of the structure where viable.

10. Off-Street Parking Requirements.

(a) **Size of parking spaces and aisles**

Parking spaces and drive aisles shall be sized in a manner consistent with the following chart, unless specifically allowed otherwise by the City.

PARKING DIMENSIONS					
ANGLE (DEGREES)	0	30	45	60	90
Stall Width	8'	9'	9'	9'	9'
Stall Length	23'	18'	18'	18'	18'
Stall Length Perpendicular to Aisle	8'	17'	19'	20'	18'
Aisle Width, One Way	12'	12'	12'	18'	24'
Aisle Width, Two Way	20'	20'	20'	22'	24'
Total Width, One Stall, One-Way Aisle	20'	29'	31'	38'	42'
Total Width, Two Stalls, One-Way Aisle	28'	46'	50'	58'	60'
Total Width, One Stall, Two-Way Aisle	28'	37'	39'	42'	42'
Total Width, Two Stalls, Two-Way Aisle	36'	54'	58'	62'	60'

(b) The number of parking spaces, which are required in each usage classification, are as follows:

- Easton Farm generally (Including all uses allowed except as set forth below) - one (1) for each 250 square feet of lease able floor space, except 1.25 spaces per dwelling unit (4/1,000).
- Freestanding Restaurants/Bars – Eight (8) spaces per each 1000 square feet of gross leasable floor area.
- Large Format Retail – A minimum of three (3) spaces for each 1000 square feet of gross leasable floor area (3/1,000) is permitted, up to a maximum of no more than four and a half (4.5) spaces per 1000 square feet of gross leasable floor area. The Master Developer and City will assist any development entity, and any/all end user retail tenant(s) to facilitate the potential for cross-parking usage with other complementary services/attractions for all large surface parking areas. This effort should be encouraged in early site planning phases.
- Office - a minimum of one (1) space for every 250 square feet of gross leasable floor space per BOMA standard lease calculations. However for medical office uses, the ratio of parking spaces shall be one (1) space for every 200 square feet of gross leasable floor area on single use lots.

- Multi-Family Residential - One and one-quarter (1.25) spaces per dwelling unit, except single family attached dwellings: Two (2) spaces per dwelling unit.
- Public and institutional - Shall not utilize shared parking, and all off-street parking shall be located in rear yard areas. The "Parking Area" for Institutional Uses shall be a minimum of one (1) space for every 250 square feet of internally occupied floor space. However, the ratio of parking spaces actually constructed can be reduced to three (3) spaces for every 1,000 square feet of occupied floor space, provided the balance of the "Parking Area" is set aside as green space or permeable pavers (lawn or landscaping) in designs approved by the City.

(c) **Shared Parking.**

Where permissible, shared-parking ratios may be allowed for certain developments/uses. Shared parking arrangements are meant to encourage a more "round the clock" and effective usage of any parking facility, therefore generating smaller overall surface parking area(s), while avoiding the creation of expansive, underutilized lots.

The City may consider the acceptance of a formula that can demonstrate certain combinations of mixed-uses and adjacent developments that will yield increased overall parking area utilization, especially throughout any given twenty-four hour period. Such formulas shall be consistent with the recommendations and current standards recognized by the *Urban Land Institute* and/or the guidelines from the *Congress for New Urbanism*.

(d) **Off-Street Surface Parking Lot Placement.**

Any off-street, surface parking areas within Easton Farm shall be set back a no less than fifteen feet (15') or no more

than seventy-five feet (75') from any existing, surrounding arterial, including along the property line adjacent to State Route 741 and along the length of Easton Way. The City shall have discretion to make this requirement applicable elsewhere on any subsequent prominent frontages, such as along key pedestrian connections, within significant vistas and within important public spaces. Surface parking lots may be built up to adjacent property lines on all other street frontages within the Property and as established in subsequent approved site plan areas that anticipate connecting adjacent development at a future date. Placement of surface parking areas should be related to both the building type served and the adjacent buildings, connecting streetscapes and uses, which may require the lot to be screened by building or landscaping from surrounding streets and/or pedestrian pathways.

(e) **Structured Parking Lot Placement.**

Any parking structure(s) shall be set back a minimum of 60 feet from the property lines of all adjacent streets to reserve room for Liner Buildings between parking structures and the lot frontage, unless the parking structure is sited within the multi-family residential area in the southeast corner of the site separate from the property line by two retention ponds including fountains/aerators of the parcels adjacent to the southern property line. The buildings surrounding the parking structure shall be no less than two stories in height.

Any parking structure(s) shall be set back a minimum of 60 feet from the property lines of all adjacent streets to reserve room for Liner Buildings between parking structures and the lot

frontage, unless the parking structure is sited within the multi-family residential area in the southeast corner of the site separate from the property line by two retention ponds including fountains/aerators of the parcels adjacent to the southern property line. The buildings surrounding the parking structure shall be no less than two stories in height.

11. Drainage and Water Detention

- (a) **Detention as an Amenity.**
Storm retention/detention ponds are proposed within the southeast and northwest corners of the site, between the proposed neighborhood and existing properties between and along the southern boundary and existing properties on that side and along State Route 741 and adjacent to the existing farmstead bordering a new internal neighborhood.
- (b) **Impact on Surrounding Properties**
The water features shall be secured from accidental access through the use of in by natural elements such as landscaping, rocks, boulders, and a low decorative rail or ornamental fence, or as otherwise approved by the Master Developer and the City.

12. Signage/Environmental Graphics/Tenant Signs.

- (a) **Project Signage/Brand.**
The Master Developer will prepare and submit to the City an overall project identity graphic/font/logo as part of an overall 'way finding' signage program. This will be physically used on entry walls, street signs, directories, and in subtle other applications where a logo or font script will help to establish an overall brand for the Easton Farm development.

This will establish the parameters for execution of all project signage, including size, application, color, use, materials and fabrication, along with potential uses and styling for all marketing and promotional potentials.

The project name will also be a 'tag line,' to be placed at the end of individual projects within the development to help both promote the Easton Farm and to establish a sense of place. For example, a multi family development hypothetically name 'The Lofts' will be promoted at "The Lofts at Easton Farm." This would apply to all larger scale developments that require their own address.

The Master Developer retains full control of the use and promotion of the overall Easton Farm project branding, logo, identity and any entity that uses such aspects in a co-branded promotion or application.

- (b) **Individual Project and/or Tenant Sign Criteria.**

At present, the Master Developer believes that, given the fluid nature of the various components and end users, it would be difficult and limiting to the creative potential to establish exacting signage guidelines this early in the process.

Instead, as an individual development begins its design process, and the building uses/tenants are established, along with the resulting architectural character, massing and materials, the Master Developer will require and work with the developer(s) to craft their own site-specific signage criteria, and to assist potential tenants with tailoring creative, stylish and well-executed signage and graphics that add another dimension of quality to the overall development

This allows each individual project to establish their unique brand within the Easton Farm development, with the signage being a 'signature.'

The Master Developer will require all individual developments to submit a project/tenant signage criteria for review, comment and required adjustment within a timeline to be established early in the development process. These individual signage/tenant criteria(s) will cover all aspects including, but not limited to, the placement, scale, application, materials, colors, fabrication, and illumination of all project identity, tenant signage, branding and way finding elements. This process allows for the Development to accommodate ongoing shifts in style, application and fabrication technology.

In all instances, the Master Developer and City retain full approval rights for all signage and graphics, as well as any proposed alterations or adjustments to any individual project/tenant signage criteria or resulting signage.

- (c) **Comprehensive Sign Guidelines.** When the Development is at a point where end users have been identified and sufficient project development has occurred that Comprehensive Sign Guidelines may be promulgated, Master Developer shall prepare and submit to the City for its approval proposed Comprehensive sign Guidelines. The City shall consider such guidelines in the form of rules and regulations supplemental to this text.

03

RESIDENTIAL

The purpose of this Section is to allow for flexibility when the result will be residential development that is compatible with nearby walkable town/village center/urban village.

1. Permitted Uses

- (a) single-family detached dwellings
- (b) single-family attached dwellings (town homes)
- (c) multi-family dwellings
- (d) apartments
- (e) condominiums
- (f) loft-style residential

- (g) assisted or independent living residences
- (h) senior housing
- (i) accessory structure

2. Orientation.

Residential area building entrances shall be oriented toward the street, primary right of way, open areas or courtyard unless deviation is appropriate, per the discretion of the Master Developer. Buildings located on an intersection shall have consistent architectural style and detailing on both street-fronting elevations.



Residential building entrances should be oriented toward the street, primary right-of-way...



...open areas or courtyard.

3. Signage

All residential project signage may be placed, either bracketing the entry into, or in the middle of the right of way, into any new entrance. It may also be placed on the structure, above or to the side of a dedicated entrance. No one development can have more than two such entrances. Signs/identity may also be placed in landscaped islands, on each side of an entry, along existing boulevards per the discretion of the Master Developer and the City.

SINGLE FAMILY LOTS								
	LOT WIDTH (MIN.)	LOT DEPTH (MIN.)	GARAGE LOADING	FRONT YARD SETBACK (MIN.)	SIDE YARD SETBACK (MIN. EA. SIDE)	REAR YARD SETBACK (MIN.)	MAXIMUM OCCUPIED HEIGHT	MINIMUM OCCUPIED HEIGHT
Village Center Lots	31'	110'	Service way	6'	3'	5' ²	2 stories	1 story
Neighborhood Lane Lots	50'	125'	Service way	15'	5'	5' ²	2 stories	1 story
Neighborhood Edge Lots	50'	125'	Front	25'	5'	20'	2 stories	1 story
Northern Transition Lots	110'	125'	Front	25'	5'	20'	2 stories	1 story

1. **Setback** to wall of the primary building massing; 5' encroachment by front porch permitted.
2. **Setback** from service way right-of-way or easement line to face of garage.

5. Site Development.

(a) Maximum Block Size.

The maximum length of any block shall not exceed 600 feet without an intersecting street, service drive, connecting pedestrian greenway or adjoining open space.

(b) Service ways.

Service ways shall be incorporated in the residential neighborhoods to provide access to parking and service areas behind rows of town house-style units. Service way locations and dimensions are not fixed but shall be designed to accommodate the service way's purpose, preferably no less than 20' from building edge to building edge across any one service way way to facilitate proper backing out of any one garage space.

Any required additional curb cuts shall be added only with the permission of the City. Service ways may also be incorporated into parking lots as drive aisles and fire lanes.

(c) Lot Coverage.

Multi-family dwellings may not cover more than eighty percent (80%) of an overall parcel area. This does not include any outboard freestanding garage parking or covered spaces/areas.

(d) Building Fronts and Backs.

The front or side of every building must face the street, right of way or courtyard. Rear facing buildings, overhead doors and service entries are prohibited on street façades.

- (e) Streets having single family homes with garage accessed off the frontage street shall be constructed with rolled curb and gutter.

6. Residential Building Materials.

The following materials shall be permitted materials for use in residential buildings:

- (a) Brick in standard sizes.
- (b) Stone: natural, cast or cultured
- (c) Siding shall be wood, simulated wood, or vinyl (minimum thickness - .046"), or fiber-cement siding.
- (d) Stucco, EIFS in standard finishes.
- (e) Columns, cornices, and similar elements should be natural or simulated natural materials such as wood, steel or stone.

7. Residential Roof Materials:

- (a) Natural or simulated slate or tile.
- (b) Wood shakes.
- (c) Dimensional shingles (30 year).
- (d) Standing seam panels (20 yr. Paint warranty, minimum).
- (e) Copper
- (f) Single-ply membrane roofs allowed when hidden from street level view.

8. Parking Requirements.

The intent of these parking ratios is to encourage a balance between developing a desirable, pedestrian oriented development and necessary car storage for both short and long-term use. The goal is to construct neither more nor less parking than is needed, and where viable, to overlap the timeline of the various uses to achieve a shared time frame aspect.

- (a) Minimum Residential Space Requirements.
The minimum parking spaces in residential areas are as follows: 1.25 space per dwelling unit at multi-family



Service ways shall be used to provide parking and service access to townhomes and smaller lots.

residential, and 2 spaces per dwelling for single-family dwellings.

(b) On-Street & Structured Parking.

On-street and/or structured parking is permitted and encouraged for all site plans for any residential neighborhood within Easton Farm. Parking need not be contiguous with the building(s) or the use it serves.

(c) Shared parking solutions.

Shared parking solutions are encouraged by the Master Developer and the City. Each development applicant shall provide a parking analysis justifying any proposed shared parking solution, per guidelines developed by the Urban Land Institute.

9. Access to Off-Street Parking.

(a) Access from Service ways. No Parking Permitted.

Service ways shall be the primary source of access to all off-street or individual unit garage parking. (Parking along service ways, if permissible per the individual development site plan, may be head-in, diagonal or parallel and may be made of a permeable block surface material.)

(b) Connection of Service ways to Adjacent

Properties.

Service ways may be incorporated into parking lots as standard drive aisles. Access to all properties adjacent to the service way shall be maintained. Access along such service ways between differing ownership parcels or parking areas is also encouraged.

- (c) Corner lots.
Corner lots that have both rear and side access shall access parking through the rear.
- (d) Garage door(s).
Garage doors shall not exceed 16 feet in width.

10. Parking Lot Landscaping Requirements.

- (a) Landscape Island/Beds.
Parking lot landscape island/beds of at least eight feet (8') in width shall be provided between parking aisles of either head-in or diagonal parking. The objective is to create a continuous shade canopy. A diversity of tree species throughout Easton Farm is encouraged. To minimize water consumption, the use of low-water vegetative ground cover other than lawn is encouraged, unless the lawn is part of a public open space or a foreground landscape feature. Plant material and trees should be of native species that are climate and drought tolerant.

11. General Ancillary Restrictions.

- (a) The following utility and functional needs shall be properly screened and concealed from public view, from any direction, whether facing a public street, walkway, or side street, by either a planting screen, wall device or incorporated in the base building architecture:
 - Temporary window and/or Wall Air Conditioners/HVAC units, except that for

model homes such units

- All Utility Metering devices
- Air Conditioning Compressors;
- Irrigation sheds
- Pool maintenance facilities and pumps.
 - (b) The following shall be located in private open spaces, courtyards or to the side or rear private outdoor space of any attached dwelling unit:
 - Permanent fixed barbecues
 - Antennas if concealed
 - To the maximum extent permitted by law, satellite dish antennas greater than 18" in diameter.

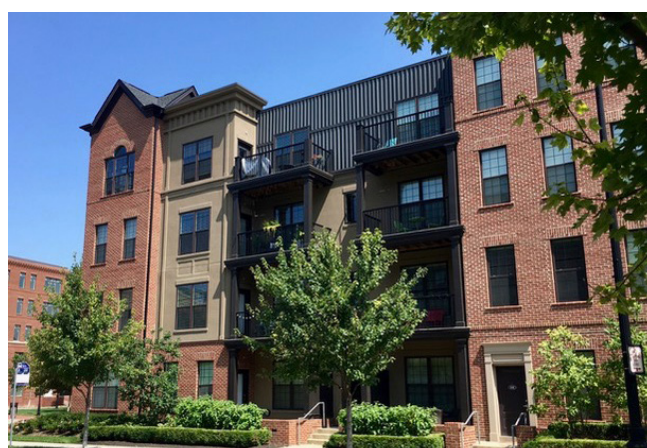
For any residential units over commercial space, to the maximum extent permitted by law, a satellite dish needs to be concealed either on roof surfaces or setbacks, well away from any public streetscape vistas, public parking areas and/or walkways.

- c) The following decorative and/or other elements are prohibited:

- Plastic shutters, bris soleil shutters
- Clotheslines
- Clothes Drying Yards
- Reflective and/or bronze-tint glass; Plastic or PVC roof files; Backlit awnings
- Awnings (awnings shall also be prohibited on any front and side elevation), except that for model homes, awnings shall be permitted until the home is sold for use as a dwelling.
- Fences made of chain link, barbed wire, or plain wire mesh, or rough- textured/timber or 'fortress style' wood fences.
- Unpainted and/or untreated wood fencing.

12. Residential Typologies

(a) Multifamily



(b) Townhomes



(c) Village Center Lots



(d) Neighborhood Lane Lots



(e) Neighborhood Edge Lots



(f) Northern Transition Lots



(g) Amenity Areas



Service way example



Service way example

LANDSCAPE AND FEATURE PARK GUIDELINES

1.) Introduction

The Easton Farm's design character will be enhanced by a series of crafted landscaped parks and open spaces that link and define the various neighborhoods.

This component is a key signature of what will make this a landmark.

Note; In the spirit of making the development a one-of-a-kind community, if any development partner would prefer to creatively deviate from these guidelines, or add landscape or amenity features not covered in the following sections, the Master Developer reserves the right to review and allow for such changes and enhancements that exceed these requirements.

There are three key components to the landscape;

- A 'Central Green' that forms the 'front door' of The Easton Farm in the mixed-use commercial district.
- A 'Linear Park' that is the open-space 'spine' of the project that links both the various neighborhoods within The Easton Farm, and connects through the site to Springboro's existing North Park

on the south side of the site to Gardner Park on the north.

- A series of street trees, neighborhood parks, open spaces and pathways that interconnects the various neighborhoods, giving them a 'pedestrian-centric' feel.

2.) Central Green

The Central Green is the signature open space at the center The Easton Farm's commercial district.

It is at the confluence of the three main public streets in the development and is framed by the larger structures within the mixed-use neighborhood.

The Central Green is made up of two asymmetrical, angular park spaces that are bifurcated by Noel Drive in the center.

The larger of these two open spaces at is at the corner of Easton Farm Boulevard and Anna Drive, and is framed by the larger structures within the community. It is made up of the following components. The second one to the northwest is more triangular and modest in scale and is perceived as more of

a passive space. It is expected that this park is fully irrigated and up to the sidewalks so that it can be easily maintained.

Features and Details

- A series of curving and linear concrete sidewalks, between 5' and 10' feet in width.
- Park entrances that utilize key amenity pieces, with a wider hardscape surface in a variety of potential materials such as colored and pattern-stamped concrete, pavers, pea gravel or composite deck where viable.
- Several hundred feet of low walls that function as potential landscape retaining with a partial 'seat wall' at key points facing onto the sidewalk side. Wall surfaces are to be stone veneer or patterned block with a precast cap on top of no less than 12" wide (with skateboard stops on certain sections).
- Clusters of flowering landscape beds, hedge rows at key intersections, primarily along the edge of the park and at the entries.
- Amenities such as freestanding, open-air pavilion(s), a signature fountain/water feature, a 'giant checkerboard' with moveable chess and checker pieces, fixed art and/ or sculpture, fixed and/ or moveable bollards at key entries and along the zero-curb of Noel Drive, benches, trash receptacles, bike racks and moveable tables and chairs along section of a low seat wall, and a series of flagpoles leading up to the point of the triangular side of the park.
- Subtle lighting that includes pedestrian light poles, illuminated bollards, and up-lighting in trees in landscape beds along the street edges and at key entries into the park and under-edge lighting along some of the linear walls where viable. Also, all landscape beds should contain outlets to allow for season lighting,

maintenance use and event functions.

The Central Green is bifurcated by Noel Drive, which will be a zero-curb surface area defined by pavers and/or colored and stamped concrete with a vehicular travel zone framed by removable bollards that can be removed and relocated to cap the ends of the street, and allow for seasonal events within a combined park scape.

Trees and landscape beds will be clustered to define entries into the park and set in rows to define the edges of the curving and linear walk zones. These are a mix of trees well-suited to the SW Ohio climate and are primarily 2" caliper, interspersed with larger 4" caliper at intersecting areas or to define a park entry, and to enhance the variety of the growth and character of the Central Green as time goes on.

Trees stands should be tightly spaced to give the park an enclosed feeling along the edges and entries.

Amenities such as the pavilions, fountain/water feature, benches and chairs, checkerboard, flagpoles, trash receptacles, bike racks and pedestrian light poles are to be placed to the edges of the park space, and the curved and linear walks and at the entries off the corners facing Easton Farm Boulevard and Anna Drive, with the center of the 'Green' defined more as a passive open lawn space that can contain a series of programmed community events as necessary.

3.) Linear Park

The Linear Park is the spine of The Easton Farm development.

It is a predominantly passive, open space feature, which is a minimum of thirty-five (35') wide depending on location, that connects and interconnects The Easton Farm

neighborhoods with Springboro's North Park and Gardner Park that brackets the development.

The Linear Park was designed to be an almost uninterrupted walking or biking experience, with minimal vehicular crossings and connects to other open spaces and parks within The Easton Farm community.

The park is bordered on one side by predominantly single family home sites whose front doors and/or porches are designed to face onto the park, and Crockett Crossing which will feature defined parallel parking spaces along the sidewalk edge that adds a sense of enclosure from the street traffic.

Features and Details

There is a primary main pedestrian and bike trail through the linear park, that winds and meanders essentially through the central portion of the green space, with its sweeping curves allowing for significant portions of lawn or landscaping to allow for play and gathering spaces. At key intersections, the trail will connect to corners or crossings and intersect with the sidewalk that runs along the edge of Crockett Crossing. The trail is designed to be eight feet (8') in width and can accommodate both pedestrian and bike traffic. It will be scored concrete within the residential community, including sections that cross from The Easton Farm community property and into North and Gardner Parks.

The sidewalk bordering and connecting front doors of the row of home sites along the inside section of the Linear Park will be connected to the main trail at key points and intersections where appropriate.

Trees placement is to be patterned to allow for growth clusters that will provide shade at main points along its run. The trees are mostly 2" caliper and interspersed with 4" caliper where appropriate to allow for a more casual park feel as time goes on.

Amenities are widely interspersed along the spine of the park and mostly feature benches at key intersections or crossings to provide rest areas. Also included are pedestrian scale wayfinding signage at key points, and bike racks spaced at every 1200' linear feet, and placed to provide their highest use and best use and an open-air bike repair stand, placed off the main trail closest to either Gardner or North Park.

At key corners or intersections, planter beds of hedges, low plantings and flower beds will frame these areas. These spaces will feature wider hardscape surfaces to allow for benches, lighting, wayfinding and other amenities as appropriate and these should be placed off to the side of the main path of travel along the trail to not impede pedestrian or bike travel, and the taller, non-seating features optimally placed within landscape beds to provide protection and avoid damage to the bases. These landscape beds areas are to be irrigated to maintain their growth and maintenance through the growing seasons.

Lighting is provided by a series of pedestrian light poles, spaced to provide illumination in more open areas along the park, and illuminated bollards where the trail and walks need illumination, but to avoid glare or over lighting in areas where street lighting is more a dominant feature. They should always be placed within landscape beds and/ or in lawn and green spaces but close enough to the edge of the

trail or path to provide proper illumination. All amenities should all be coordinated with tree and landscape placement to enhance all areas and avoid misplacement or impede sightlines. Low planting and landscape should also be coordinated and placed to provide enclosure and screening of utility and junction boxes as necessary whose placement may be necessary along some portions of the Linear Park route.

4.) Street Trees, Landscape and Hardscape Features.

Street trees within the development are a key feature of the character of the neighborhoods, and will over time create a canopy of shade along the sidewalks and fronting the structures and homes.

In the mixed-use neighborhood, and along the main pedestrian streetscapes along Easton Farm Boulevard, Anna Drive, Crockett Crossing and Noel Drive, street trees of 2" to 4" caliper should be placed no less than every 20'-30' or depending on where it best aligns with building pilasters and/or feature paving areas at key intersections or mid-block crossings. All trees should be in either irrigated planter beds of low landscaping of no less than 6'x6' or 6'x12' along Easton Farm Boulevard and Anna Drive, set within the opening of a 6'x6' metal, decorative tree grate at sidewalk level, or set within a grass landscape strip bordering streets or at bump-out intersections.

Along the primary streets in the commercial zones, planter beds will feature a 6" curb to avoid winter salt intrusion and erosion. All planter beds and street tree placement

should be coordinated to not conflict with pedestrian light poles or building entrances that face onto that walk.

Planter beds along the main commercial streets, passages and building entries where pedestrian activity is expected to be highest will include up-lighting for the trees and electrical outlets for season and maintenance use. They also should be wired to allow for low-profile speaker systems to be set within and screened by landscaping, but only within the commercial district itself.

Street corners where the curb lines are extended outward should include special paving zones of patterned, stamped and/or colored concrete, pavers or other hardscape surfaces for high pedestrian use. These areas will often be wider than the main walk and will feature such amenities as seating, benches, trash receptacles, landscape set in moveable pots and planters, wayfinding signage and potential space for outdoor dining tied to an adjacent food service tenant.

Landscape edges and beds will feature defined additional 2" (min) caliper trees, shrubs, hedge rows and flowering beds, and may contain pedestrian street light poles and/ or illuminated bollards.

All freestanding pedestrian light poles within the mixed-use district along the primary commercial streets shall include electrical outlets in the base.

All pedestrian street crossings within the mixed-use commercial district and at key corner and mid-block crossings connecting to the Linear Park and residential neighborhood parks will be made of stamped and colored asphalt

that compliments the sidewalk patterns and colors. The walk will vary in width depending on the location and expected use, but should be no less than 6' wide. A light color border strip of no less than one foot should be used to define the central pattern within each crossing.

Pedestrian sidewalks along the main commercial addresses along Easton Farm Boulevard, Anna Drive, Noel Drive and Crockett Crossing will be no less than 12' and optimally 15'. The first two feet closest to any building will require a colored and/or stamped concrete 'transfer' zone and should be aligned to cleanly accommodate building conditions, and set at elevation with the finished sidewalk. This is to allow for a visually clean, defined sidewalk path just beyond any potential building pilaster bump-outs and inset storefronts or windows, and to the landscape beds and curbs on the other side.

Landscaping in and within parking lots and will require two, 2" (min) caliper trees within each end cap island, and the spacing of one, 2" caliper tree every 25' along drives cross-site streets within these areas. All head-in or angle-in parking rows against a landscape edge or facing a linear building façade should contain a bump-out landscape bed every 16 spaces or less, and centered along that row if more than 16 spaces, but a number less than 32 spaces. All parallel spaces should have a landscape bump out every 6 spaces.

Street trees within residential neighborhoods are conditional to the home types. All street trees that front any residential lot with service way access and no front driveways should be placed at intervals of one at each individual lot line lot of 35' wide or less, and groups of 2, spaced at 20 feet apart, of any lot more than 45' wide, with the two

trees centered on the dividing lot line. If the lot is more 'pie-shaped' where the front is dimension is wider due to a convex curve, and additional tree will be required for any lot wider than sixty feet.

For lots with street-accessed driveways and 40' wide or smaller, two trees bracketing the drive way shall be placed no less than 10' away from the apron, unless the apron is set close to a neighboring one and less than 20' between each.

For lots with a street-accessed driveway and wider than 50' there should be 2 trees along the street edge, spaced no less than 25' apart, and no less than 10' from the apron of the driveway, and an additional street tree will be required for lots wider than 85'.

All street trees will need to be coordinated with lighting pole placement, and where in conflict, a street tree should be moved to no less than 15' away from such lighting, provided it's not in conflict with the spacing of other trees along that street edge, and smaller than the dimensions required for proper growth.

5.) Neighborhood Parks

There are two defined neighborhood parks within the single-family residential neighborhoods, and are expected to be a mix of passive and active play areas.

These should contain rows or clusters of trees, or set along the edge of walkways and/ or to frame along a fronting street, or set within irrigated planting beds, with trees provided at a ratio of one for every 1000' sq. ft. of open space within each park itself.

Each park will potentially feature one or more of the following amenities and should be placed for highest and best use and to not impede pedestrian flow.

- Children's play areas with fixed jungle gyms and other play devices, and set on soft impact play surfaces.
- Benches, and other fixed seating
- Game and/play courts.
- Passive green lawns and open spaces
- Pedestrian light poles and/or illuminated bollards
- Wayfinding signage
- Fixed pavilions, shelters, trellises and/or gazebos
- Community mail box pavilion(s)



(a) Landscape Amenities



(b) Streetscapes



DEFINITIONS

The following words when used in this Declaration or any supplement hereto (unless the context shall prohibit) shall have the following meaning:

1. "ACCESSORY STRUCTURE" A building, shed, covering, or vertical design element which is subordinate and/ or supportive to a main or primary building or series of buildings. Such structures are often freestanding but may be attached if the use requires, and are often purely functional and/ or decorative, and in architectural harmony with its primary building(s). Accessory structures are predominantly one story and may be habitable and/ or climate-controlled as use permits.
2. "ARCHITECTURAL GUIDELINES" shall mean the written architectural standards set forth in this Declaration for development of the Property.
3. "ASSOCIATION" shall mean the owners association formed for the Property as provided in Article Six below.
4. "CITY" shall mean the City of Springboro, Ohio
5. "CONCEPTUAL DEVELOPMENT PLAN" shall mean the Conceptual Development Plan for the Property and the improvements to be constructed thereon.
6. "DECLARATION" shall mean this Declaration of Site Development and Design Standards and shall include without limitation all restrictions, covenants, conditions and agreements referred to herein.
7. "DEVELOPER" see Master Developer.
8. "DEVELOPMENT PLANS" shall mean the plans and specifications for the use of any Lot as set forth in this Declaration.
9. "FENESTRATION" shall mean openings in a building elevation, such as windows and doors.
10. "HOME OCCUPATION" shall mean a home-based business restricted to the owner or tenant of any residential dwelling unit and up to two employees, and shall not include noxious or disruptive functions, including the disruption of parking for neighboring residents. Home occupation uses are not limited to accessory structures.
11. "IMPERVIOUS SURFACE" shall mean any material that substantially reduces or prevents the infiltration of storm water into previously undeveloped land. Impervious

surfaces shall also mean any surface that has been compacted or covered with a layer of material so that it is highly resistant to infiltration by water. Impervious surface shall include compacted sand, lime rock and clay as well as conventional surfaces such as gravel driveways, parking areas, surfaced streets, roofs, sidewalks, structures and other similar surfaces.

12. "LINER BUILDING" refers to a retail or business structure built to camouflage a more utilitarian façade/use such as a substation, or parking structure. Liner Buildings are generally narrow in depth and placed along the street and consistent with approved uses in Mixed-Use areas.
13. "LOT" shall mean and refer to any parcel or lots designated hereafter on a plat or replat, or subdivision thereof, recorded against the Property, or portion thereof, improved or unimproved, on which a structure may be located.
14. "MASTER DEVELOPER" shall mean Easton Farm Partners, LLC, an Ohio limited liability company, its successors and permitted assigns as provided herein.
15. "MAXIMUM OCCUPIED HEIGHT" shall mean the height limit on certain Structures, as set forth herein, measured from median front exterior grade level to the specified height above the highest occupied floor.
16. "MAXIMUM UNOCCUPIED HEIGHT" shall mean the height limit on certain Structures, as set forth herein, measured from front exterior grade level to the highest unoccupied level of an enclosed Structure.
17. "MINIMUM OCCUPIED HEIGHT" shall mean the minimum height of certain Structures, as set forth herein, based either upon a minimum number of occupied stories above grade level or a minimum height measured from the front exterior grade level.
18. "MIXED USE" shall mean an area or structure that contains a mixture of commercial, office and/or residential uses.
19. "MULTI-FAMILY DWELLING" shall mean a structure designed to contain more than two dwelling units in which the units can share primary exterior entrances, but where each dwelling unit has separate housekeeping and cooking facilities. Dwelling units within a multi-family dwelling may be attached through common walls or common floors.
20. "OPEN SPACE" shall mean any hardscaped, landscaped, grass or appropriate vegetation area but shall not include any interior landscaping or landscaped areas within parking areas.
21. "OWNER" shall mean and refer to the record owner, whether one or more persons or entities, of the fee simple title to any Lot which is a part of the Property, including the Developer or Master Developer, but shall not mean or refer to any mortgagee or subsequent holder of a mortgage, unless and until such mortgagee or holder has acquired title pursuant to foreclosure or any proceedings in lieu of foreclosure.
22. "PARKING STRUCTURE" A multi-level, ventilated or open-aired structure meant to park vehicles in a vertical manner to achieve a higher yield on such a land use. Such structures may be freestanding or integrated into another building, or at the base of a multi-use structure. Parking structures may have multiple entries/ exits and connected between levels by ramps, stairs and/or elevators.
23. "PERSONAL CARE SERVICES" means an establishment or place of business primarily engaged in the provision of services of a personal nature related to the care, hygiene, or appearance of the human body or the maintenance of items worn or carried by persons. Such services are usually but not always recurrent in nature. Examples

of personal service uses include, but are not limited to, beauty and barber shops, shoe repair shops, health spas, therapeutic massage, tailor shops, and the like.

24. “PUBLIC AND INSTITUTIONAL USES” shall mean buildings and uses such as libraries, schools, government buildings, churches or other uses approved by the City.

25. “SERVICE WAY” A separate drive either behind or to the side of a building or row of buildings that is subordinate in nature to the primary streets surrounding or connected to it. Its primary function is to allow for access to garages, driveways, and to allow for rear or side service access to a building(s), home or row of homes. They are often primarily vehicular in nature but can be made into a mini-street effect with setbacks and landscaping.

26. “SET BACKS” shall mean designated areas adjacent to lot lines on which the construction of buildings is not permitted, and shall also be defined as “build to lines.”

27. “SINGLE-FAMILY, ATTACHED DWELLING” shall mean a structure containing more than one single-family dwelling unit in which the units are physically attached only by common walls. Each unit has its own housekeeping facilities, cooking facilities and separate exterior entrance way.

28. “STORMWATER MANAGEMENT” shall mean the impoundment of stormwater in ponds, swales, or other collection devices, and the holding of such for a period of time and release at the prescribed rate determined by the City.

29. “STRUCTURE” shall mean any improvement on the Property that is capable of occupancy or use including, but not limited to, any building, garage, fence, wall, sign or any

other temporary or permanent improvement; and any excavation, fill, ditch, dam or other thing or device that changes the grade of any land by more than six inches or alters the natural flow of waters from, upon or across any part of the Property.