

**Agenda**  
**City of Springboro Planning Commission Meeting**  
**Wednesday, January 19, 2022, 6:00 p.m.**

- I. Call to Order
- II. Appointment of Officers for 2022
  - A. Chair
  - B. Vice Chair
- III. Approval of Minutes
  - A. December 8, 2021 Planning Commission Meeting
- IV. Agenda Items
  - A. Preliminary Review, Final Development Plan, Advanced Drive PUD, Planned Unit Development, commercial building #4
  - B. Recommendation for Adoption, Springboro Master Plan
- V. Guest Comments
- VI. Planning Commission and Staff Comments
- VII. Adjournment

**Background Information, Staff Comments & Recommendations**  
**City of Springboro Planning Commission Meeting**  
**Wednesday, January 19, 2022, 6:00 p.m.**  
**Council Chambers, City Building, 320 West Central Avenue**

**III. Agenda Items**

**A. Preliminary Review**

Final Development Plan, Advanced Drive PUD, Planned Unit Development, commercial building #4

**Background Information**

This agenda item is a request for final approval of a final development plan for the construction of a speculative commercial building on the west side of Advanced Drive in the Stoltz Industrial Park. This is the fourth such approved requested by the property owner and developer, Cincinnati Commercial Contracting. This is the fourth such building proposed by the applicant since 2020.

The subject property is located approximately 500 feet northeast of the terminus of Advanced Drive.

The applicant is proposing to construct a one-story, 20,200-square foot flexible space commercial building for a future user. No address has been assigned for the property at this point in time; addresses are typically not assigned by the Engineering Department until further into the site development process. The proposed commercial building will be located on 6.72 acres of undeveloped land; the property owner plans to develop their remaining 1.506 acres of undeveloped land, located immediately to the south, as soon as practicable.

The subject property has frontage on Advanced Drive. Adjacent land includes other properties in the Stoltz Industrial Park are occupied by the following businesses: to the north Hanover Products at 125 Advanced Drive and the developing property at 175 Advanced Drive, Armstrong Trailer. The latter was approved by Planning Commission at their November 10th. To the east and southeast are Advanced Interiors at 240-250 Advanced Drive, and to the south A-1 Mechanical at 235 Advanced Drive. The subject property also borders to the west undeveloped property in Franklin Township with frontage and access provided from Sharts Road to the west.

The subject property is zoned PUD, Planned Unit Development, and is part of the Advanced Drive PUD that coincides with the Stoltz Industrial Park. The Advanced Drive PUD was created in 1997 to accommodate light industrial development of the area in collaboration with the Stoltz estate. The remainder of the estate is now Clearcreek Park. The PUD zoning designation permits the use of the property for office and light industrial development. Land to the west in Franklin Township was recently rezoned to accommodate a residential subdivision.

**Staff Comments**

City staff has the following comments regarding this agenda item:

1. Provide a color rendition of proposed building exterior.
2. Provide elevations for proposed dumpster screening.

3. Parking supply may need to be adjusted when end user is identified.
4. Following preliminary review by Planning Commission, prepare a landscaping plan for review for compliance with Chapter 1280, Landscaping. Mark existing trees 4 inches DBH or larger for credit against landscaping requirements.
5. Revise lighting plan to address photometric analysis which exceeds 6.0 foot-candle maximum in the proposed loading area and on the southwest and southeast corners of the building, an. Also revise the plans to comply with the maximum to minimum (10:1) and average to minimum (4:1) ratios permitted by code.
6. Construction requirements within the existing pipeline easement area to be worked out with pipeline companies.
7. Identify Benchmark.
8. Provide detention basin calculations for review
9. Final revised and approved site plan shall be submitted to include all comments by staff and planning commission to have signature of the owner or duly authorized officer and stamped certified by a professional engineer.
10. An "As Built" drawing showing as built location and elevations of all improvements shall be submitted prior to the issuance of an occupancy permit.
11. Clearcreek Fire District has no comments at this time.

**B. Recommendation for Adoption**  
**Springboro Master Plan**

**Background Information**

City staff is requesting that the Planning Commission review and approve a recommendation to City Council to adopt the Springboro Master Plan at their January 19, 2022 meeting. As indicated in staff's January 6th memorandum that accompanied the draft report distributed to members of Planning Commission, the plan would replace a similar plan adopted by City Council in 2009.

The plan has been under review by the City since August 2020. At that time City staff solicited proposals from consulting firms. The City received 17 proposals by the submittal deadline, and short listed from that number to three consulting teams that were interviewed with representatives of the Master Plan Steering Committee. The consulting team consisting of MKSK as lead consultant, supported by Epic Small, and The Greenway Collaborative, was selected to complete the plan.

MKSK began work in September 2020, and the nine-member steering committee began meeting in October 2020 and continued to work through the pandemic meeting in person and virtually, conducted two rounds of public input meetings, and two surveys. Planning Commission members Becky Iverson and Steve Harding served on the Steering Committee. At its meeting on December 14th, the Steering Committee approved a recommendation to adopt the plan. The draft dated December 20, 2021 includes a number of revisions incorporated into the document that were included in the motion.

Sean Hare of MKSK has been the City's primary point of contact on this project and will be participating in the meeting on January 19th by webinar. Along with City staff, he will review the planning process used to complete the plan, review plan recommendations, and answer any questions from members of Planning Commission.

Following action by the Planning Commission, the plan will be placed before Springboro City Council for review and adoption.

### **Staff Recommendation**

City Staff recommends Planning Commission to approve a recommendation to City Council to approve the Springboro Master Plan, draft dated December 20, 2021.

The information contained in this report is based on material provided to the City of Springboro as of Tuesday, January 11, 2022 at 12:00 p.m.

# APPLICATION—PLANNED UNIT DEVELOPMENTS CITY OF SPRINGBORO PLANNING COMMISSION

REZONING/GENERAL PLAN

FINAL DEVELOPMENT PLAN

RECORD PLAN

VARIANCE

The undersigned requests the approval identified above. Approvals subject to expiration provided for in Chapter 1266 of Planning & Zoning Code. For all approvals under this application, it is understood that it shall only authorize the approval described in this application, subject to any conditions or safeguards required by the Planning Commission, and/or City Council.

Owner  
 Agent  
 Lessee  
 Signed Purchase Contract

APPLICANT NAME:

CHRISTIAN STONE

Address

4779 REDBANK EXPRESSWAY

CINCINNATI, OH 45227

Telephone No.

( 513 ) 561-6633x107

Fax No.

( )

Email Address

CSTONE@CINCIGROUP.COM

PROPERTY OWNER NAME (IF OTHER): SPRINGBORO ADVANCE LLC

Address: 4779 REDBANK EXPRESSWAY

CINCINNATI, OH 45227

Telephone No. ( 513 ) 561-6633x107

Property Address or General Location:

WEST OFADVANCED DRIVE

Parcel Number(s): 0703321

Acreage: 6 AC

PUD Category:  Residential

Retail

Office

Manufacturing

Mixed Use

If Mixed Use, Acreage in Each Category: \_\_\_\_\_

For Residential Proposed Density \_\_\_\_\_ Number of Residential Units \_\_\_\_\_

Proposed Use: SPECULATIVE USE

The applicant or representative who is authorized to speak on behalf of the request must also be present at all meetings.



(Signature of Applicant and/or Agent)

12/27/2021

(Date)

CHRISTIAN STONE

Printed Name

RECEIVED

DEC 27 2021

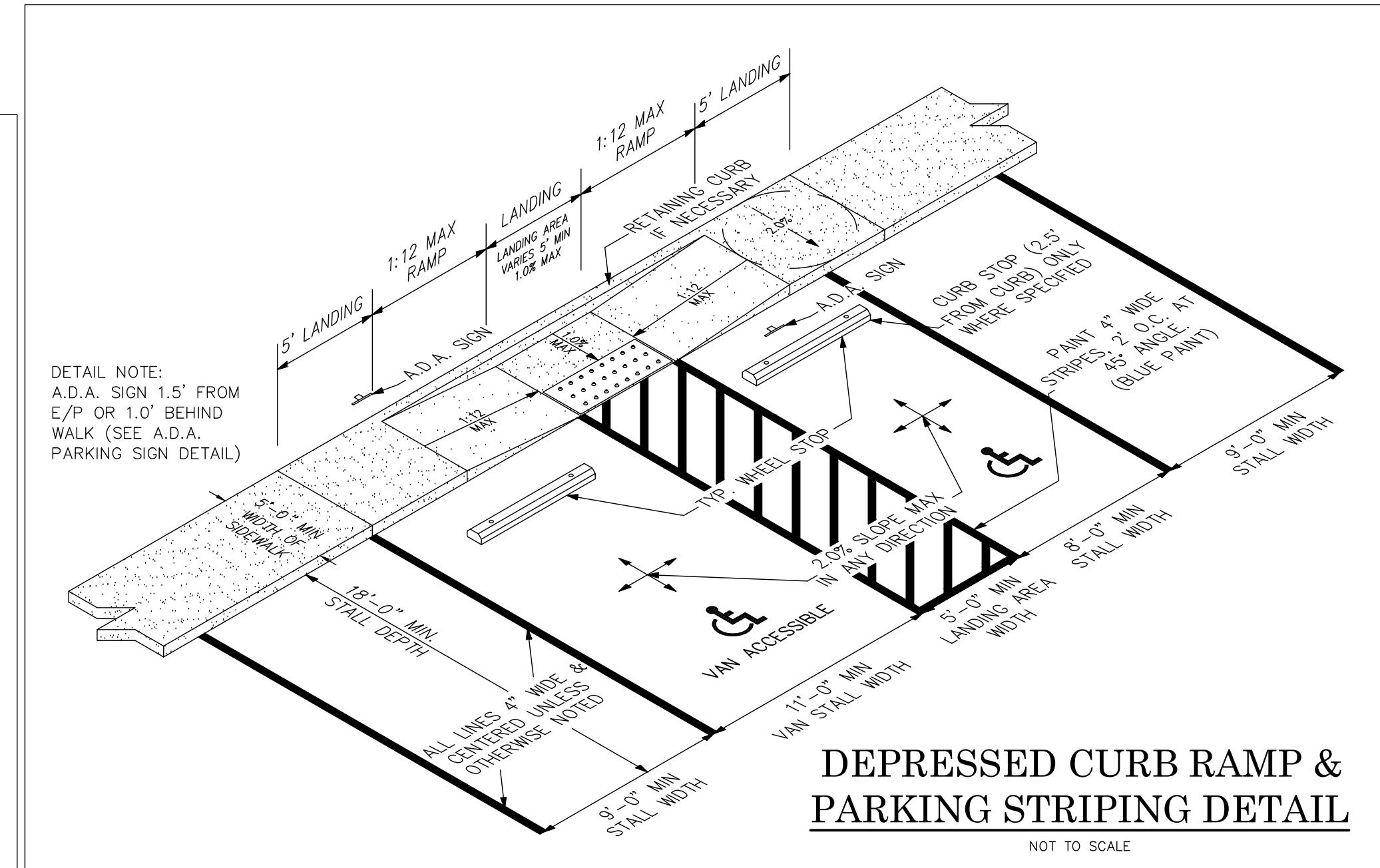
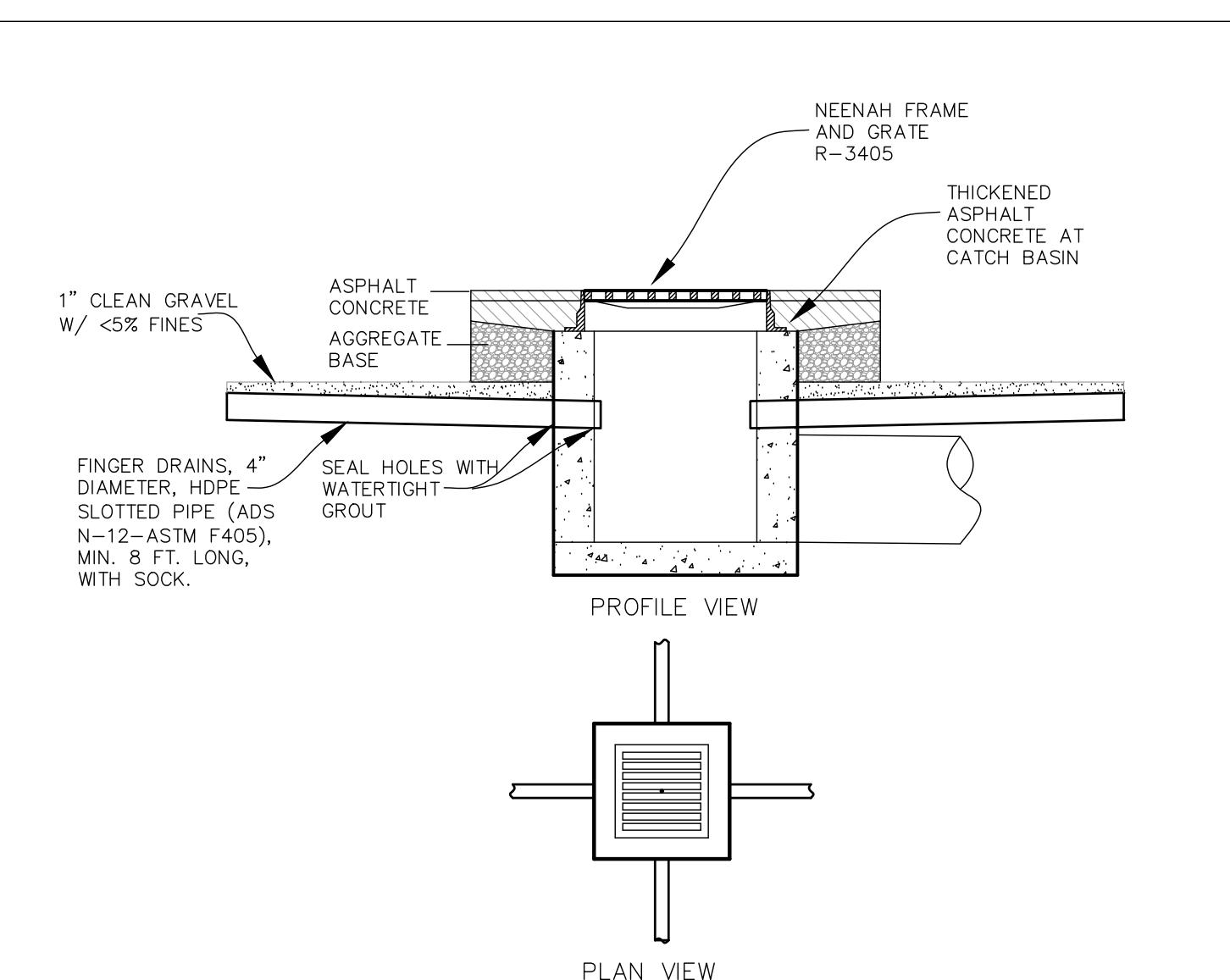
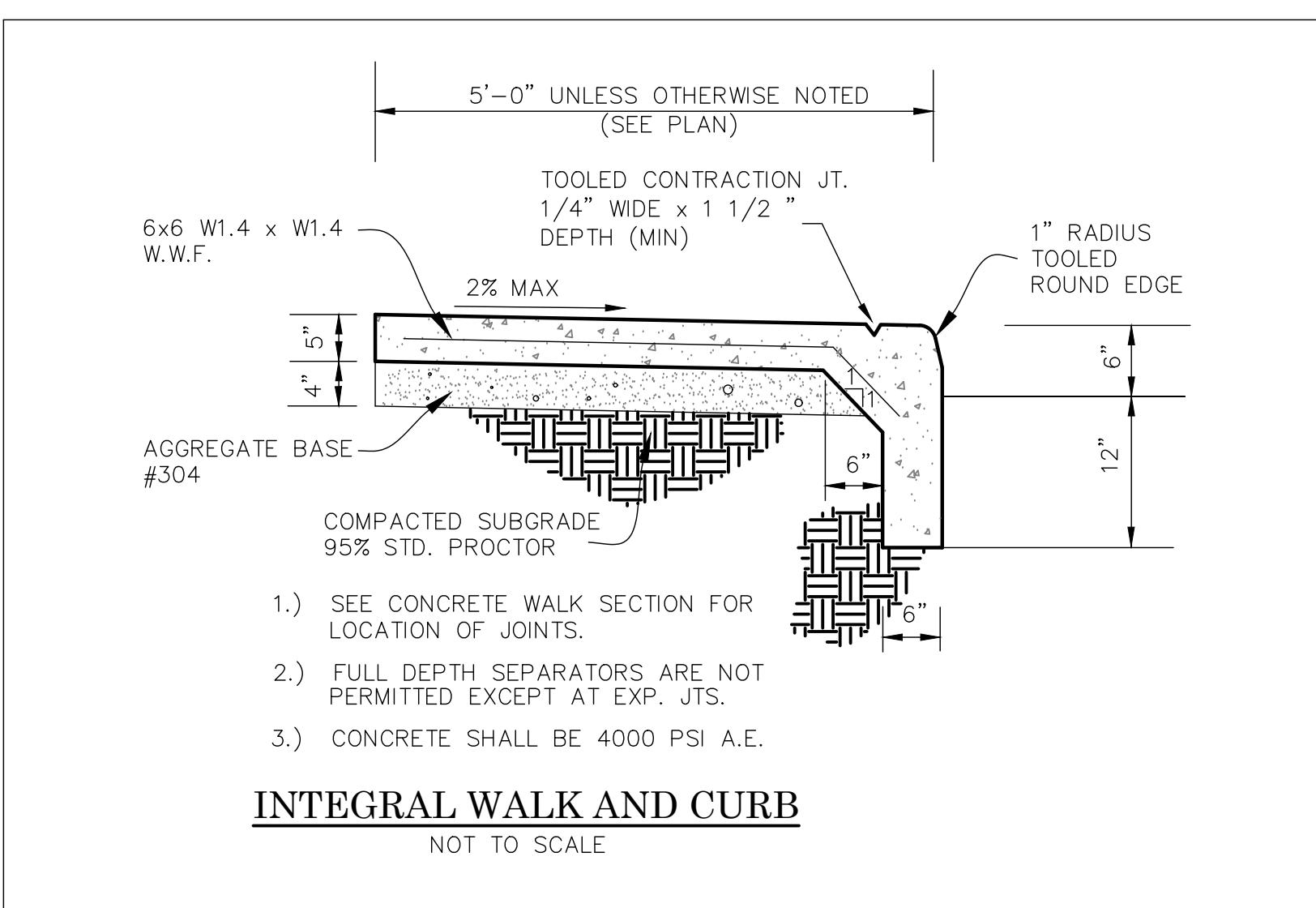
D.F.





ADVANCED DR. #4  
ADVANCED DR.  
SITE CONSTRUCTION  
DETAILS

ADVANCED DR., WARREN COUNTY, OHIO  
CITY OF SPRINGBORO, OHIO





## NOTES:

- CURRENT ZONING FOR THE PROPERTY IS "PUD". ANY SPECIFIC QUESTIONS REGARDING ZONING SHALL BE DIRECTED TO THE CITY OF SPRINGBORO ZONING DEPARTMENT.
- USE WILL BE COMMERCIAL AND IS UNDERSTOOD TO BE COMPATIBLE WITH CURRENT ZONING.
- PER FLOOD INSURANCE RATE MAP NUMBER 39165C0017E EFFECTIVE 12/17/2010. THE SUBJECT PROPERTY IS A NON-PRINTED MAP, LOCATED IN ZONE "X". ZONE "X" IS AN AREA OF MINIMAL FLOOD HAZARD.
- ALL DEMOLITION SHALL BE PERFORMED BY A LICENSED CONTRACTOR AND BE PERFORMED PER LOCAL REQUIREMENTS. ALL REMEDIATION WORK SHALL BE PROVIDED BY A PROPERLY LICENSED ABATEMENT CONTRACTOR. ALL DEBRIS IS TO BE DISPOSED OF IN A LAWFUL MANNER AND APPROVED BY OWNER.
- CONTRACTOR SHALL FIELD VERIFY AND MARK LOCATION OF EXISTING UTILITIES. CONTRACTOR SHALL NOTIFY THE OWNER AND ENGINEERING FIRM IF ANY UTILITIES OR UNDERGROUND FEATURES ARE ENCOUNTERED WHICH ARE NOT SHOWN ON THESE PLANS OR DIFFER IN LOCATION, HORIZONTAL OR VERTICAL.
- ANY PORTIONS OF WALK, CURB, OR PAVEMENT (OUTSIDE OF SITE CONSTRUCTION LIMITS) DAMAGED DURING DEMOLITION OR CONSTRUCTION SHALL BE REPLACED IN-KIND.
- CONTRACTOR SHALL INSTALL CONSTRUCTION BARRIER FENCE DURING AND AFTER DEMOLITION. ANY OPEN EXCAVATION SHALL BE PROPERLY BARRICADED AS REQUIRED BY LOCAL, STATE OR FEDERAL REGULATIONS.
- CONTRACTOR SHALL REMOVE EXISTING GROUND COVER ONLY AS NECESSARY FOR THE PROJECT PHASE CURRENTLY UNDER CONSTRUCTION.
- EXCAVATION AND DEMOLITION CONTRACTOR SHALL TAKE EXTREME CARE TO PREVENT MUD AND DEBRIS FROM ENTERING EXISTING STORM SEWERS AND WATER COURSES.
- THE CONTRACTOR SHALL KEEP EXISTING PAVEMENT SURROUNDING THE SITE "BROOM CLEAN" AND FREE OF SOIL OR AGGREGATE THAT MIGHT BE BROUGHT OFF-SITE FROM THE PROPERTY.
- THE CONTRACTOR IS RESPONSIBLE FOR KEEPING THE STREET/PARKING LOT CLEAN BY PREVENTING DEBRIS, MUD, DIRT, ETC. FROM BEING TRACKED ONTO THE STREET/PARKING LOT. THE CONTRACTOR IS RESPONSIBLE FOR CLEANING DEBRIS, MUD, ETC. FROM THE STREET IMMEDIATELY WHEN IT OCCURS AND SHALL INSPECT THE STREET AT THE END OF EACH WORKING DAY.

## REVISIONS

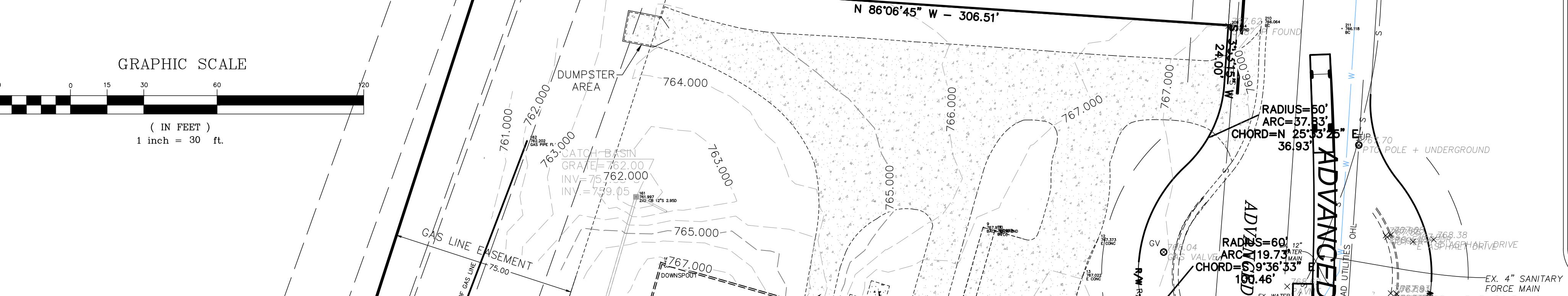
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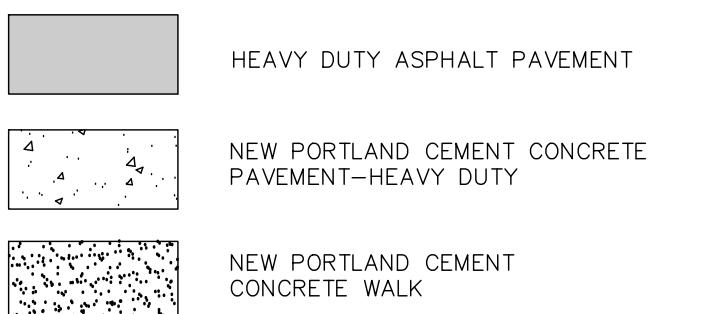
NO. &amp; DESCRIPTION

**EX. LOT  
6.72 ACRES  
(AFTER LOT  
SPLITS)**

GRAPHIC SCALE  
30 0 15 30 60  
( IN FEET )  
1 inch = 30 ft.



**LEGEND-PROP. FEATURES**



SEE LANDSCAPE PLAN, BY OTHERS. ALL AREAS NOT PAVING, OR BUILDING ARE TO BE COVERED WITH GRASS/TURF UNLESS OTHERWISE NOTED.

**NOTES:**

1. ALL DIMENSIONS ARE TO FACE OF CURB, CENTERLINE OF STRIPE, PROPERTY LINE OR R/W UNLESS OTHERWISE NOTED.
2. ALL CURB RADII ARE 3' UNLESS OTHERWISE NOTED.
3. ALL CONSTRUCTION STAGING ACTIVITIES MUST BE SELF-CONTAINED WITHIN THE LOT.
4. ALL STORAGE OF CONSTRUCTION MATERIALS, CONSTRUCTION STAGING AREA, AND THE PARKING OF CONSTRUCTION VEHICLES, INCLUDING VEHICLES OF WORKERS, SHALL OCCUR ONLY ON THE SUBJECT PROPERTY.
5. ONLY THE ACCESS POINTS LOCATED UPON THE PROPERTY SHALL BE USED BY ALL LABORERS, SUPPLIERS, CONTRACTORS AND OTHERS CONNECTED WITH SUCH CONSTRUCTION ACTIVITIES.
6. ALL CONCRETE FOR SITE WORK SHALL BE PER ODOT 452, CLASS "C", 4,000 PSI, 5-7% AIR ENTRAINED.
7. PROVIDE A TOOLED JOINT AT THE PERIMETER OF THE CONCRETE DRIVE.

**EVANS ENGINEERING**  
4240 AIRPORT ROAD, SUITE 211  
CINCINNATI, OHIO 45226  
(513) 321-2168



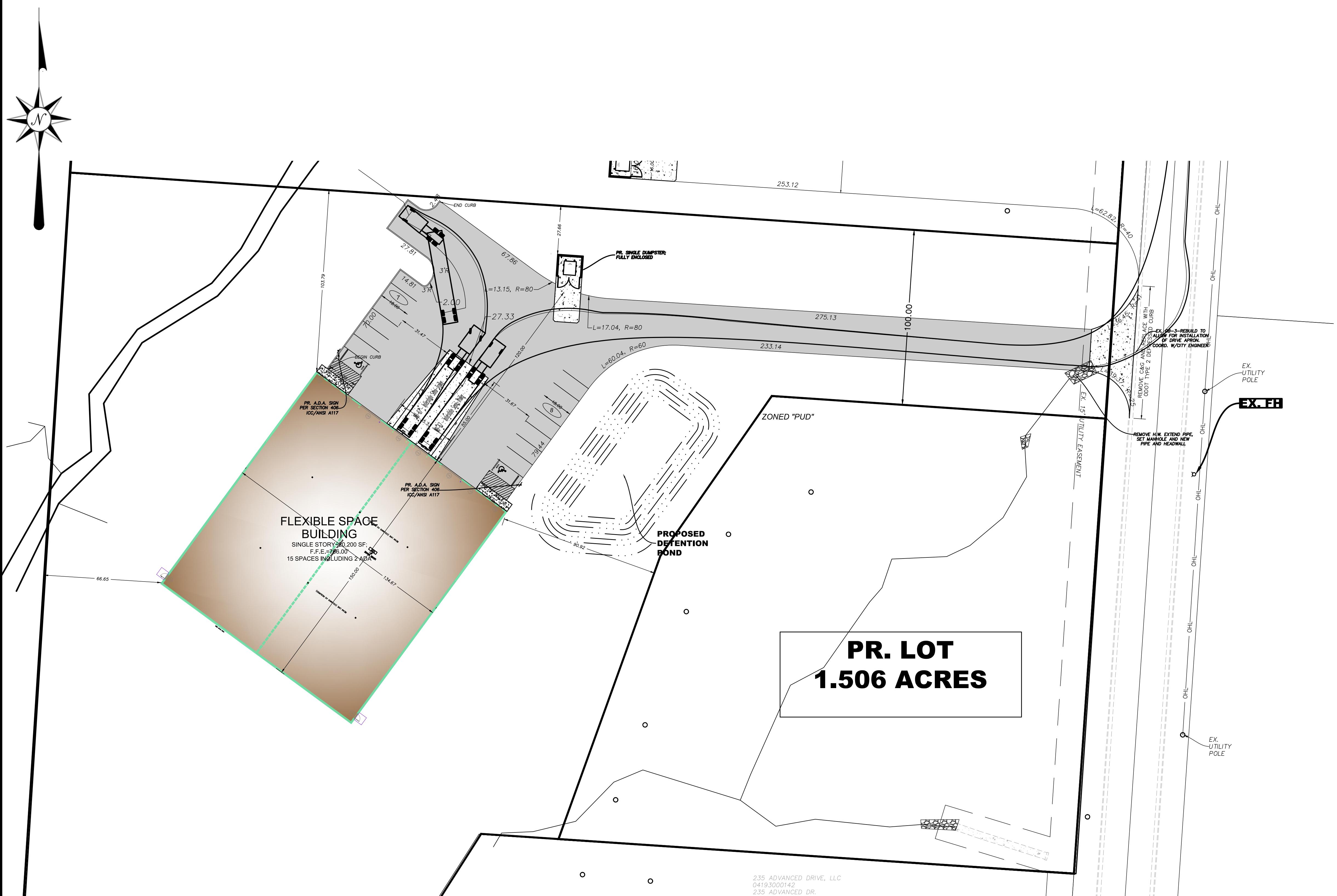
**SITE DIMENSION PLAN**  
ADVANCED DR. #4  
ADVANCED DR..  
CITY OF SPRINGBORO, WARREN COUNTY, OHIO

SCALE: HORIZ. VERT.  
1" = 30' N/A

JOB NO. 20-140  
DATE Dec. 27, 2021

**SHEET NO.**

C-3





SCALE: HORIZ. VERT.  
1"=30' N/A

JOB NO. 20-140  
DATE Dec. 27, 2021

SHEET NO.

C-3.1



## GRADING LEGEND

## CONTOUR LEGEND:

TC=TOP OF CURB ELEVATION  
P=FINISHED GRADE (PAVEMENT)  
F =FINISHED GRADE  
TW=TOP OF WALL  
BW=BOTTOM OF WALL

B/C=EXISTING BACK OF CURB OR  
B/W=EXISTING BACK OF WALL GR  
EX.=EXISTING GRADE

- EX. CONTOURS
- PR. CONTOURS (MAJOR)
- PR. CONTOURS (MINOR)

#### SPECIAL NOTES:

CONTRACTOR SHALL CONSULT WITH  
GEOTECHNICAL ENGINEER BEFORE  
COMMENCING EARTHMOVING ACTIVITIES

TOPSOIL ~6" SHOULD BE DISTRIBUTED BACK  
ACROSS LANDSCAPE AREAS PRIOR TO  
SEEDING.

CONTRACTOR SHALL DISPOSE OF EXCESS MATERIAL IN ACCORDANCE WITH ALL LOCAL AND STATE CODES AND PERMIT REQUIREMENTS. EXPORTED MATERIAL SHALL BE TRANSPORTED TO AN APPROVED FILL AREA.

DETENTION CONTROL STRUCTURE  
STRUCTURE NO. (2)  
MODIFIED ODOT CB 2-6  
SCALE: 1" = 4'  


— 4

## EROSION AND SEDIMENT CONTROL NOTES:

### PRE-CONSTRUCTION:

1. THE CONTRACTOR SHALL NOTIFY THE WARREN COUNTY ENGINEER AT LEAST FIVE (5) DAYS BEFORE COMMENCING ANY LAND DISTURBING ACTIVITIES AND, UNLESS WAIVED BY THE LOCAL AUTHORITY, WILL BE REQUIRED TO HOLD A PRE-CONSTRUCTION MEETING BETWEEN PROJECT REPRESENTATIVES FROM THE LOCAL AUTHORITY.

2. THE CONTRACTOR SHALL NOTIFY LOCAL AUTHORITY BY TELEPHONE AT THE FOLLOWING POINTS:

- A. REQUIRED PRE-CONSTRUCTION MEETING
- B. FOLLOWING INSTALLATION OF SEDIMENT CONTROL MEASURES.
- C. PRIOR TO REMOVAL OR MODIFICATION OF ANY SEDIMENT CONTROL STRUCTURE.
- D. PRIOR TO REMOVAL OF ALL SEDIMENT CONTROL DEVICES
- E. PRIOR TO FINAL ACCEPTANCE

3. A COPY OF THE APPROVED SEDIMENT AND EROSION CONTROL PLAN MUST BE AVAILABLE AT THE PROJECT SITE AT ALL TIMES.

4. THE CONTRACTOR SHALL CONSTRUCT ALL EROSION AND SEDIMENT CONTROL MEASURES PER THE APPROVED PLAN AND CONSTRUCTION SEQUENCE AND SHALL HAVE THEM INSPECTED AND APPROVED BY A LOCAL AUTHORITY REPRESENTATIVE PRIOR TO BEGINNING ANY OTHER LAND DISTURBANCES.

5. THE CONTRACTOR SHALL ENSURE THAT ALL RUNOFF FROM DISTURBED AREAS IS DIRECTED TO THE SEDIMENT CONTROL DEVICES AND SHALL NOT REMOVE AN EROSION OR SEDIMENT CONTROL MEASURE WITHOUT PRIOR PERMISSION FROM A LOCAL AUTHORITY REPRESENTATIVE.

6. THE CONTRACTOR MUST OBTAIN APPROVAL FROM THE LOCAL AUTHORITY BEFORE ANY CHANGES TO THE SEDIMENT CONTROL PLAN AND/OR SEQUENCE OF CONSTRUCTION ARE IMPLEMENTED UNLESS IMMEDIATE ACTION IS NECESSARY. IN THIS CASE, THE CONTRACTOR SHALL IMPLEMENT APPROPRIATE BEST MANAGEMENT PRACTICES TO ELIMINATE THE POTENTIAL FOR ACCELERATED EROSION AND/OR SEDIMENT POLLUTION.

7. THE CONTRACTOR SHALL INSPECT DAILY AND MAINTAIN CONTINUOUSLY IN AN EFFECTIVE OPERATING CONDITION ALL EROSION AND SEDIMENT CONTROL MEASURES UNTIL SUCH TIMES AS THEY ARE REMOVED WITH PRIOR PERMISSION FROM A LOCAL AUTHORITY REPRESENTATIVE. THE CONTRACTOR SHALL KEEP WRITTEN RECORDS OF ALL SEDIMENT AND EROSION CONTROL INSPECTIONS AND MAINTENANCE FOR THE DURATION OF THE PROJECT. THIS INFORMATION MUST BE MADE AVAILABLE TO A LOCAL AUTHORITY REPRESENTATIVES UPON REQUEST.

### SAFETY:

8. THE LOCAL AUTHORITY SITE REPRESENTATIVE ALWAYS HAS THE OPTION OF REQUIRING ADDITIONAL SAFETY OR SEDIMENT CONTROL MEASURES IF DEEMED NECESSARY.

9. WHERE DEEMED APPROPRIATE BY THE ENGINEER OR INSPECTOR, BASINS AND TRAPS MAY NEED TO BE SURROUNDED WITH AN APPROVED SAFETY FENCE. THE FENCE MUST CONFORM TO LOCAL ORDINANCES AND REGULATIONS. THE DEVELOPER OR CONTRACTOR SHALL NOT PUBLISH OR PUBLISHING SAFETY REQUIREMENTS WHERE SAFETY FENCE IS DEEMED APPROPRIATE AND LOCAL ORDINANCES DO NOT SPECIFY FENCING SIZES AND TYPES. THE FOLLOWING SHALL BE USED AS A MINIMUM STANDARD: THE SAFETY FENCE MUST BE MADE OF WELDED WIRE AND AT LEAST FORTY TWO (42) INCHES HIGH, HAVE POSTS SPACED NO FARTHER APART THAN EIGHT (8) FEET, HAVE MESH OPENINGS NO GREATER THAN TWO (2) INCHES IN WIDTH AND FOUR (4) INCHES IN HEIGHT WITH A MINIMUM OF 14 GAUGE WIRE. SAFETY FENCE MUST BE MAINTAINED AND IN GOOD CONDITION AT ALL TIMES.

10. STORM DRAIN INLETS IN NON-SUMP AREAS SHALL HAVE TEMPORARY ASPHALT PADS CONSTRUCTED AT THE TIME OF BASE PAVING TO DIRECT GUTTER FLOW INTO THE INLETS TO AVOID SURCHARGING AND OVERFLOW OF INLETS IN SUMP AREAS.

11. STOCKPILE SLOPES SHALL NOT BE STEEPER THAN 2:1.

### STABILIZED CONSTRUCTION ENTRANCE:

12. THE CONTRACTOR SHALL PROTECT ALL POINTS OF CONSTRUCTION INGRESS AND EGRESS TO PREVENT THE DEPOSITION OF MATERIALS onto PUBLIC ROADS. ALL MATERIALS DEPOSITED onto PUBLIC ROADS SHALL BE REMOVED IMMEDIATELY USING A STREET SWEEPER OR SCRAPER. DEBRIS SHALL NOT BE WASHED OFF PAVED SURFACES OR INTO STORM DRAINS.

13. CONSTRUCTION ENTRANCES SHOULD NOT BE RELIED UPON TO REMOVE MUD FROM VEHICLES. VEHICLES THAT ENTER AND LEAVE THE SITE SHALL BE RESTRICTED FROM MUDGY AREAS OR CLEANED BEFORE LEAVING SITE.

### SEDIMENT TRAPS AND BASINS:

14. SEDIMENT TRAPS OR BASINS ARE NOT PERMITTED WITHIN TWENTY (20) FEET OR AND EXISTING OR PROPOSED FOUNDATION OR TRAFFIC AREA. NO STRUCTURE MAY BE CONSTRUCTED WITHIN TWENTY (20) FEET OF AN ACTIVE SEDIMENT TRAP OR BASIN.

15. SEDIMENT TRAPS AND BASINS MUST HAVE STABLE INFLOW AND OUTFLOW POINTS SO THAT WATER CAN DISCHARGE WITHOUT CAUSING EROSION.

16. SEDIMENT BASINS/TRAPS SHALL NOT BE GREATER THAN FOUR (4) FEET IN DEPTH.

17. SEDIMENT MUST BE CLEANED, AND THE TRAP/BASIN RESTORED TO ITS ORIGINAL DIMENSIONS, WHEN ACCUMULATION REACHES A HEIGHT HALF-WAY UP TO THE TOP OF THE DESIGNED HOLDING AREA.

18. SEDIMENT REMOVED FROM TRAPS AND BASINS SHALL BE PLACED AND STABILIZED IN APPROVED AREAS, BUT NOT WITHIN A FLOODPLAIN, WETLAND OR VEGETATION PRESERVATION AREA.

19. WHEN PUMPING SEDIMENT LADEN WATER, THE DISCHARGE MUST BE DIRECTED TO A SEDIMENT TRAPPING DEVICE PRIOR TO DISCHARGE TO A FUNCTIONAL STORM SYSTEM, STABLE GROUND SURFACE, OR RELEASE FROM THE SITE.

20. SEDIMENT BASINS MUST BE REMOVED WITHIN THIRTY SIX (36) MONTHS AFTER THEIR CONSTRUCTION.

21. OHIO DAM SAFETY LAWS APPLY TO BASINS LARGER THAN FIFTEEN (15) ACRE-FEET (24,000 CY) AS MEASURED TO THE TOP OF THE HOLDING AREA.

### TEMPORARY & PERMANENT STABILIZATION:

22. ALL CRITICAL SLOPES (3:1 OR STEEPER) SHALL BE STABILIZED WITH SOD OR SEED AS SOON AS POSSIBLE BUT NO LATER THAN SEVEN (7) CALENDAR DAYS AFTER ACHIEVING FINAL GRADE.

23. ALL AREAS NOT DRAINING TO A FUNCTIONING SEDIMENT BASIN MUST BE FINAL GRADED AND STABILIZED WITH SOD OR SEED WITHIN SEVEN (7) CALENDAR DAYS OF ACHIEVING FINAL GRADE.

24. ALL AREAS WITHIN FIFTY (50) FEET OF A STREAM MUST BE STABILIZED WITHIN TWO (2) CALENDAR DAYS OF ACHIEVING FINAL GRADE.

25. ALL AREAS THAT ARE TO REMAIN IDLE, INCLUDING STOCK PILES, FOR FOURTEEN (14) CALENDAR DAYS MUST BE STABILIZED WITH SEED OR SOD.

26. WHEN THE PROPERTY IS BROUGHT TO FINISHED GRADE DURING THE MONTHS OF NOVEMBER THROUGH FEBRUARY, AND PERMANENT STABILIZATION IS FOUND TO BE IMPRACTICAL, TEMPORARY SEED AND ANCHORED MULCH SHALL BE APPLIED TO ALL DISTURBED AREAS. THE FINAL PERMANENT STABILIZATION SUCH PROPERTY SHALL BE APPLIED BY MARCH 15 OR EARLIER IF GROUND AND WEATHER CONDITIONS ALLOW.

27. PERMANENT SWALES OR OTHER POINTS OF CONCENTRATED WATER FLOW SHALL BE STABILIZED WITH SOD OR SEED WITH AN APPROVED EROSION CONTROL MATTING, RIP-RAP, OR BY OTHER APPROVED STABILIZATION MEASURES WITHIN TWO (2) DAYS OR ACHIEVING FINAL GRADE.

28. TEMPORARY SEDIMENT CONTROL DEVICES MAY BE REMOVED THIRTY (30) CALENDAR DAYS FOLLOWING ESTABLISHMENT OF PERMANENT STABILIZATION IN ALL CONTRIBUTORY DRAINAGE AREAS.

29. STORMWATER MANAGEMENT STRUCTURES, USED TEMPORARILY FOR SEDIMENT CONTROL, SHALL BE CONVERTED TO THE PERMANENT CONFIGURATION THIRTY (30) CALENDAR DAYS FOLLOWING ESTABLISHMENT OF PERMANENT STABILIZATION IN ALL CONTRIBUTORY DRAINAGE AREAS.

30. FOR FINISHED GRADING, THE CONTRACTOR SHALL PROVIDE ADEQUATE GRADIENTS TO PREVENT WATER FROM POLENGING FOR MORE THAN TWENTY FOUR (24) HOURS AFTER THE END OF A RAINFALL EVENT. DRAINAGE COURSES AND SWALE FLOW AREAS MAY TAKE AS LONG AS FORTY EIGHT (48) HOURS AFTER THE END OF A RAINFALL EVENT TO DRAIN. AREAS DESIGNED TO HAVE STANDING WATER (I.E. RETENTION PONDS) DO NOT HAVE TO MEET THIS REQUIREMENT.

31. ALL WASTE AND BORROW AREAS OFF-SITE MUST BE PROTECTED BY SEDIMENT CONTROL MEASURES AND STABILIZED.

### OTHER:

32. NO SOLID OR LIQUID WASTE, INCLUDING BUILDING MATERIALS, SHALL BE DISCHARGED IN STORM WATER RUNOFF. THE CONTRACTOR MUST IMPLEMENT ALL NECESSARY CONTROL MEASURES TO PREVENT THE DISCHARGE OF POLLUTANTS TO THE DRAINAGE SYSTEM OF THE SITE OR SURFACE WATERS. UNDER NO CIRCUMSTANCE SHALL CONCRETE TRUCKS WASH OUT DIRECTLY INTO A DRAINAGE CHANNEL, STORM SEWER OR SURFACE WATER.

33. SEDIMENT MUST BE CLEANED FROM SILT FENCES AND MULCH BERMS WHEN ACCUMULATION REACHES A HEIGHT OF HALF-WAY UP TO THE TOP OF THE FENCE/BERM.

34. SEDIMENT REMOVED FROM SILT FENCES AND MULCH BERMS SHALL BE PLACED AND STABILIZED IN APPROVED AREAS, BUT NOT WITHIN A FLOODPLAIN, WETLAND OR VEGETATION PRESERVATION AREA.

35. ALL SLOPES STEEPER THAN 3:1 REQUIRED GRADE TREATMENT, EITHER STAIR-STEP GRADING, GROOVING, FURROWING, OR TRACKING IF THEY ARE TO BE STABILIZED WITH VEGETATION.

36. AREAS WITH GRADES LESS STEEP THAN 3:1 SHOULD HAVE THE SOIL SURFACE LIGHTLY ROUGHENED AND LOOSE TO A DEPTH OF TWO (2) TO FOUR (4) INCHES PRIOR TO SEEDING.

37. CONSTRUCTION AND DEMOLITION DEBRIS MUST BE DISPOSED OF IN ACCORDANCE WITH LOCAL AND STATE STATUTES.

## ADDITIONAL STORMWATER POLLUTION PREVENTION NOTES:

UNLESS OTHERWISE NOTED, STANDARDS AND SPECIFICATIONS ESTABLISHED IN THE LATEST EDITION OF THE OHIO DEPARTMENT OF NATURAL RESOURCES "RAINFALL AND LAND DEVELOPMENT" MANUAL, CURRENT EDITION, SHALL GOVERN THE EROSION AND SEDIMENT CONTROL INSTALLATIONS SPECIFIED ON THIS PLAN.

THE DEVELOPER AND CONTRACTOR SHALL ABBIDE BY THE RULES AND REGULATIONS SET FORTH IN THE OHIO EPA PERMIT NO. OHCO00004-AUTHORIZATION FOR STORM WATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITY UNDER THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES).

CONTRACTOR SHALL REMOVE EXISTING GROUND COVER ONLY AS NECESSARY FOR THE PROJECT PHASE CURRENTLY UNDER CONSTRUCTION.

SEDIMENT CONTROL STRUCTURES SHALL BE FUNCTIONAL THROUGHOUT THE COURSE OF EARTH DISTURBING ACTIVITY. SEDIMENT BASINS AND PERIMETER SEDIMENT BARRIERS SHALL BE IMPLEMENTED PRIOR TO GRADING AND WITHIN SEVEN DAYS FROM THE START OF GRUBBING. THEY SHALL CONTINUE TO FUNCTION UNTIL THE UP SLOPE DEVELOPMENT AREA IS RESTABLISHED. AS CONSTRUCTION PROGRESSES AND THE TOPOGRAPHY IS ALTERED, APPROPRIATE CONTROLS MUST BE CONSTRUCTED OR EXISTING CONTROLS ALTERED TO ADDRESS THE CHANGING DRAINAGE PATTERNS.

SILT STOCKPILE MUST BE STABILIZED AND PROTECTED WITH SEDIMENT TRAPPING TO PREVENT SOIL LOSS.

SILT FENCES AND "INLET FILTERS" ARE TO BE CONTINUOUSLY MAINTAINED BY THE DEVELOPER AND/OR CONTRACTOR UNTIL ALL DANGER OF EROSION/SEDIMENTATION OCCURRING HAS BEEN ELIMINATED.

ALL GROUND SURFACE AREAS THAT HAVE BEEN EXPOSED OR LEFT BARE AS A RESULT OF CONSTRUCTION AND ARE TO FINAL GRADE AND TO REMAIN SO, SHALL BE SEDED AND MULCHED AS SOON AS PRACTICAL IN ACCORDANCE WITH STATE OF OHIO SPECIFICATION ITEM 659, AND PER TABLE LISTED ON THIS SHEET "STABILIZATION."

EXCAVATION CONTRACTOR SHALL TAKE EXTREME CARE TO PREVENT MUD AND DEBRIS FROM ENTERING EXISTING STORM SEWERS AND WATER COURSES.

THE CONTRACTOR SHALL KEEP EXISTING PAVEMENT SURROUNDING THE SITE "BROOM CLEAN" AND FREE OF SOIL OR AGGREGATE THAT MIGHT BE BROUGHT OFF-SITE.

THE CONTRACTOR IS RESPONSIBLE FOR KEEPING THE STREET/PARKING LOT CLEAN BY PREVENTING DEBRIS, MUD, DIRT, ETC. FROM BEING TRACKED ONTO THE STREET/PARKING LOT. THE CONTRACTOR IS RESPONSIBLE FOR CLEANING DEBRIS, MUD, ETC. FROM THE STREET IMMEDIATELY WHEN IT OCCURS AND SHALL INSPECT THE STREET AT THE END OF EACH WORKING DAY.

THE DEVELOPER AND/OR CONTRACTOR SHALL PERFORM REGULAR STREET SWEEPING TO MINIMIZE SEDIMENTS TO THE PROPOSED STORM SEWER SYSTEM.

UPON REQUEST OF THE OHIO EPA, OR LOCAL JURISDICTION, THE CONTRACTOR SHALL PROVIDE ALL NPDES PERMIT REPORTS AND A COPY OF THE STORM WATER POLLUTION PREVENTION PLAN. THE CONTRACTOR SHALL ALLOW THE OHIO EPA, OR LOCAL JURISDICTION, TO ENTER THE SITE TO INSPECT AND MONITOR ALL EROSION CONTROL MEASURES.

DUMPSTERS AND PORT-A-LETS ARE NOT TO BE CLOSER THAN 20' FROM THE PROPERTY LINES.

SPECIAL NOTES:

1. DURING CONSTRUCTION, THE PROPERTY MUST HAVE A MINIMUM TWENTY (20) FOOT WIDE CONSTRUCTION ENTRANCE MAINTAINED OF STONE MATERIAL.

2. AT THE CONSTRUCTION ENTRANCE TO THE PROPERTY, THERE MUST BE A WATER SOURCE AND TIRES OF CONSTRUCTION VEHICLES MUST BE RINSED TO MINIMIZE ANY DIRT WHICH MIGRATE FROM THE PROPERTY.

3. DURING CONSTRUCTION, DEBRIS MUST BE REMOVED AS APPROPRIATE, BUT AT LEAST WEEKLY.

4. DURING CONSTRUCTION, EROSION CONTROL MUST BE MAINTAINED ON THE PROPERTY INCLUDING PERIMETER CONTROL, E.G. STRAW BALE BARRIERS, EROSION FENCING, ETC.

5. ALL AREAS THAT ARE TO REMAIN IDLE, INCLUDING STOCK PILES, FOR FOURTEEN (14) CALENDAR DAYS MUST BE STABILIZED WITH SEED OR SOD.

6. WHEN THE PROPERTY IS BROUGHT TO FINISHED GRADE DURING THE MONTHS OF NOVEMBER THROUGH FEBRUARY, AND PERMANENT STABILIZATION IS FOUND TO BE IMPRACTICAL, TEMPORARY SEED AND ANCHORED MULCH SHALL BE APPLIED TO ALL DISTURBED AREAS. THE FINAL PERMANENT STABILIZATION SUCH PROPERTY SHALL BE APPLIED BY MARCH 15 OR EARLIER IF GROUND AND WEATHER CONDITIONS ALLOW.

7. PERMANENT SWALES OR OTHER POINTS OF CONCENTRATED WATER FLOW SHALL BE STABILIZED WITH SOD OR SEED WITH AN APPROVED EROSION CONTROL MATTING, RIP-RAP, OR BY OTHER APPROVED STABILIZATION MEASURES WITHIN TWO (2) DAYS OR ACHIEVING FINAL GRADE.

8. TEMPORARY SEDIMENT CONTROL DEVICES MAY BE REMOVED THIRTY (30) CALENDAR DAYS FOLLOWING ESTABLISHMENT OF PERMANENT STABILIZATION IN ALL CONTRIBUTORY DRAINAGE AREAS.

9. STORMWATER MANAGEMENT STRUCTURES, USED TEMPORARILY FOR SEDIMENT CONTROL, SHALL BE CONVERTED TO THE PERMANENT CONFIGURATION THIRTY (30) CALENDAR DAYS FOLLOWING ESTABLISHMENT OF PERMANENT STABILIZATION IN ALL CONTRIBUTORY DRAINAGE AREAS.

## TEMPORARY AND PERMANENT SEEDING:

### 1.1 SEEDED PREPARATION:

A. LIME (IN LIEU OF A SOIL TEST RECOMMENDATION) ON ACID SOIL ( $pH=5.5$  OR LESS) AND SUBSOIL AT A RATE OF 100 POUNDS PER 1000 SF, OR TWO (2) TONS PER ACRE OF AGRICULTURAL GROUND LIMESTONE.

B. FERTILIZER (IN LIEU OF A SOIL TEST RECOMMENDATION) SHALL BE APPLIED AT A RATE OF 12-15 POUNDS (25 POUNDS FOR PERMANENT SEEDING) PER 1000 SF OF 10-10-10 OR 12-12-12 ANALYSIS OR EQUIVALENT.

### 1.2 SEEDING:

#### 1. TEMPORARY SEEDING MIXTURE:

SEEDING PERIOD TYPE RATE (1000 SF)

SPRING AND SUMMER	1. OATS	3 LBS
	2. PEREN. RYEGRASS	1 LBS
	3. TALL FESCUE	1 LBS

#### 2. PERMANENT SEEDING MIXTURE:

SEEDING PERIOD TYPE RATE (1000 SF)

SPRING, SUMMER, AND FALL	1. CREEPING RED FESCUE	0.5 LBS
	DOMESTIC RYEGRASS	0.25 LBS
	KENTUCKY BLUEGRASS	0.25 LBS
	2. TALL FESCUE	1 LBS

#### 2-1 SEEDING FOR STEEP BANKS OR CUTS:

SPRING, SUMMER, AND FALL

SPRING, SUMMER, AND FALL	1. TALL FESCUE	1 LBS
	2. CROWNFETCH	0.25 LBS
	3. FLAT FESCUE	0.50 LBS

#### 2-2 SEEDING FOR WATERWAYS AND ROAD DITCHES:

SPRING, SUMMER, AND FALL

SPRING, SUMMER, AND FALL	1.

ElumTools General Use Global Illuminance Results							
Calculation Points Name	Average	Maximum	Minimum	Avg/Min	Max/Min	Max/Avg	Min/M
Detail Filled Region	1.8 fc	9.5 fc	0.1 fc	20.6	112.2	5.4	



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# ADVANCED DRIVE #4

# NEW PROSPECTIVE BUILDING

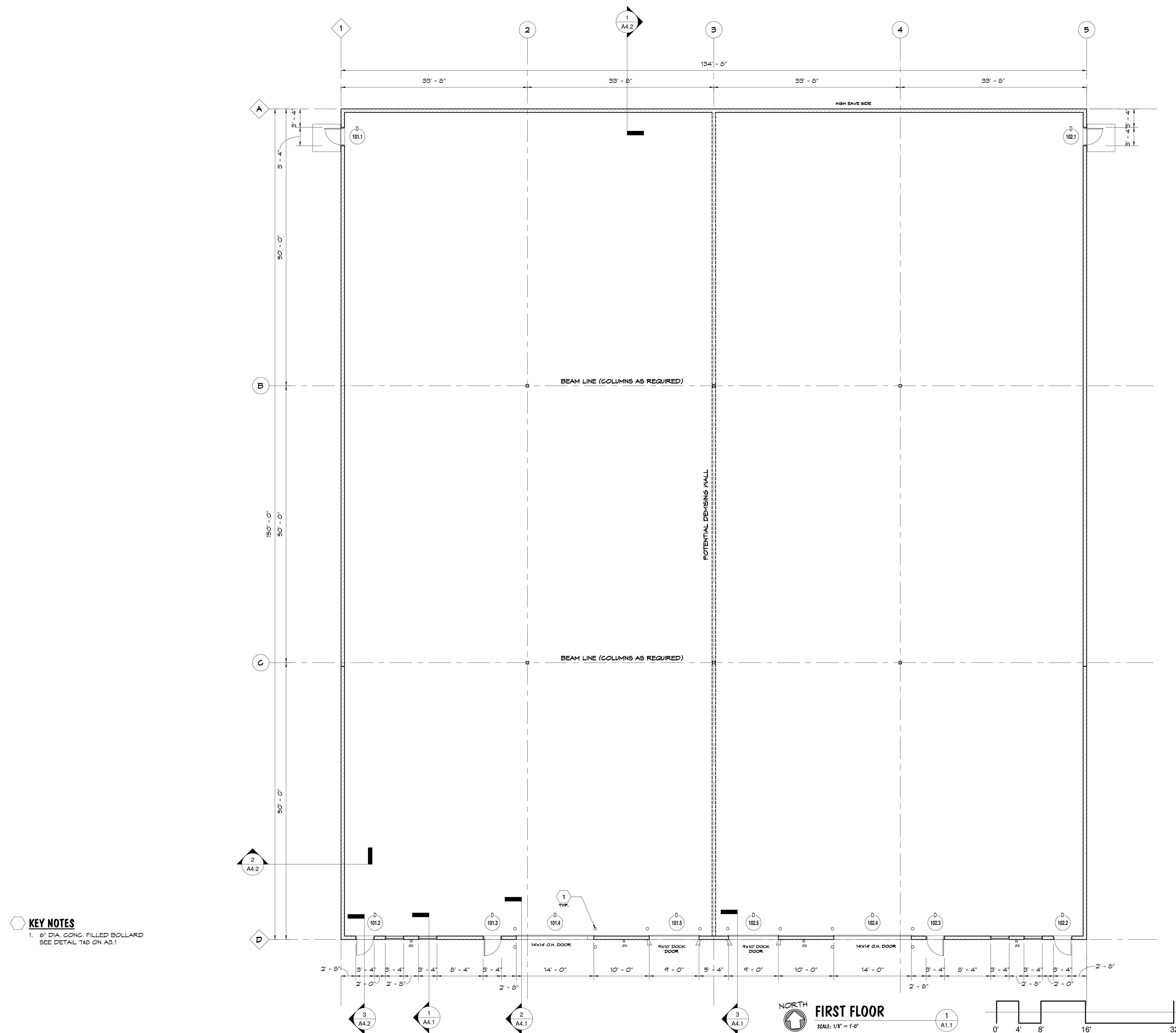
**ADVANCED DRIVE,  
SPRINGBORO, OH 45066**

**P101**

## PHOTOMETRIC PLAN

DATE: 12/27/2021	DRAWN BY: CCC
---------------------	------------------

## 1 Photometric Plan



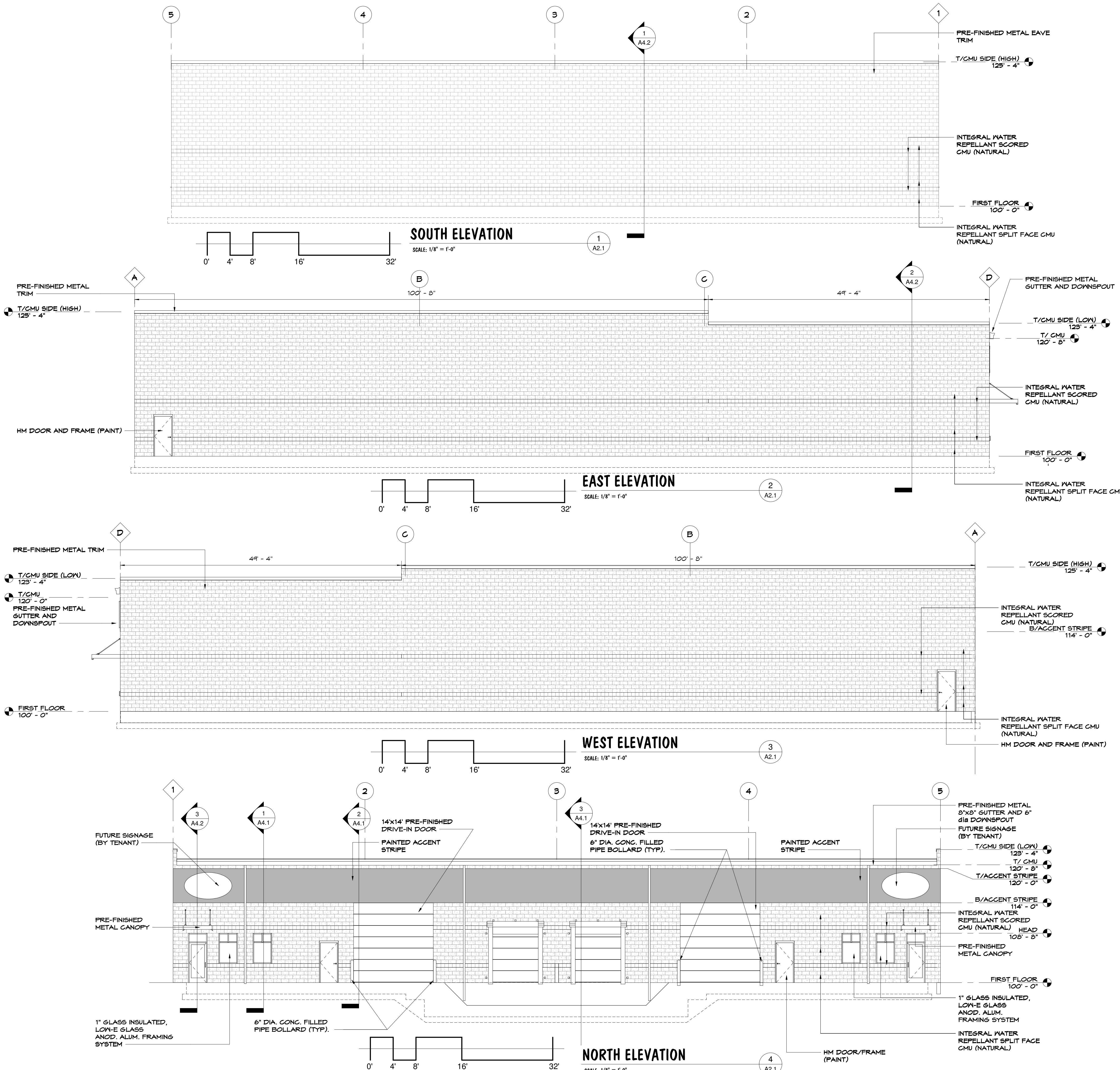
**Proposed Building For:  
Spec Building - Lot 4  
Advanced Drive  
Springboro, Ohio 45066**

**K|B|A**  
K B A Incorporated ARCHITECTS  
CINCINNATI OHIO

29 HIGH STREET  
Milford, Ohio 45150  
513.752.7800  
Fax: 513.752.7833  
www.KBInc.com

**SHEET CONTENTS:**  
FLOOR PLAN

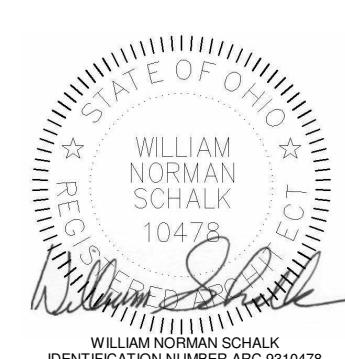
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Your Choice for Quality  
4779 Red Bank Expressway Cincinnati, Ohio 45227  
phone (513) 561-6633 fax (513) 561-3554



**Proposed Building For:  
Spec Building - Lot 4  
Advanced Drive  
Springboro, Ohio 45066**

REV. DATE CK'D

Drawn By: WNS Checked By: WNS



Date: 12.22.21 Job No: 21.200

**A2.1**

**K|B|A**  
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CINCINNATI OHIO

29 HIGH STREET  
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**SHEET CONTENTS:**  
ELEVATIONS

**CC** CINCINNATI COMMERCIAL CONTRACTING  
Your Choice for Quality  
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phone (513) 561-6633 fax (513) 561-3554



Color: Bronze

Weight: 34.8 lbs

Project:

Type:

Prepared By:

Date:

**Driver Info**

Type	Constant Current	Watts	50W
120V	0.46A	Color Temp	3000K (Warm)
208V	0.27A	Color Accuracy	71 CRI
240V	0.23A	L70 Lifespan	100,000 Hours
277V	0.20A	Lumens	7,127
		Input Watts	54.81W
		Efficacy	130 lm/W

**Technical Specifications****Compliance****UL Listed:**

Suitable for wet locations as a downlight

**IESNA LM-79 & IESNA LM-80 Testing:**

RAB LED luminaires and LED components have been tested by an independent laboratory in accordance with IESNA LM-79 and LM-80.

**Dark Sky Conformance:**

Conforms to (allows for conformance to) the requirements for the IDA's "Fixture Seal of Approval" as of March 1, 2016.

**DLC Listed:**

This product is listed by Design Lights Consortium (DLC) as an ultra-efficient premium product that qualifies for the highest tier of rebates from DLC Member Utilities.

DLC Product Code: PKY4PVJR

**Performance****Lifespan:**

100,000-Hour LED lifespan based on IES LM-80 results and TM-21 calculations

**Construction****IES Classification:**

The Type IV distribution (also known as a Forward Throw) is especially suited for mounting on the sides of buildings and walls, and for illuminating the perimeter of parking areas. It produces a semicircular distribution with essentially the same candlepower at lateral angles from 90° to 270°.

**IP Rating:**

Ingress protection rating of IP66 for dust and water

**Ambient Temperature:**

Suitable for use in up to 40°C (104°F)

**Cold Weather Starting:**

Minimum starting temperature is -40°C (-40°F)

**Thermal Management:**

Superior thermal management design with external Air-Flow fins provides maximum operational life, even in high ambient temperature environments

**Housing:**

Die-cast aluminum housing, lens frame and mounting arm

**Mounting:**

Heavy-duty mounting arm with "O" ring seal &amp; stainless steel screws

**Reflector:**

Specular vacuum-metallized polycarbonate

**Gaskets:**

High-temperature silicone gaskets

**Technical Specifications (continued)****Construction****Finish:**

Formulated for high durability and long-lasting color

**Green Technology:**

Mercury and UV free. RoHS-compliant components.

**LED Characteristics****LEDs:**

Multi-chip, high-output, long-life LEDs

**Color Consistency:**

3-step MacAdam Ellipse binning to achieve consistent fixture-to-fixture color

**Color Stability:**

LED color temperature is warrantied to shift no more than 200K in color temperature over a 5-year period

**Color Uniformity:**

RAB's range of Correlated Color Temperature follows the guidelines of the American National Standard for Specifications for the Chromaticity of Solid State Lighting (SSL) Products, ANSI C78.377-2017.

**Electrical****Driver:**

Constant Current, Class 2, 1400mA, 100-277V, 50-60Hz, 0.8A, Power Factor 99%

**THD:**

6.1% at 120V, 9.4% at 277V

**Power Factor:**

99.6% at 120V, 96% at 277V

**Surge Protection:**

6kV surge suppression protection tested in accordance with IEEE/ANSI C62.41.2.

**Other****Patents:**

The WPLED™ design is protected by patents pending in the U.S., Canada, China, Taiwan and Mexico.

**Warranty:**

RAB warrants that our LED products will be free from defects in materials and workmanship for a period of five (5) years from the date of delivery to the end user, including coverage of light output, color stability, driver performance and fixture finish. RAB's warranty is subject to all terms and conditions found at [rablighting.com/warranty](http://rablighting.com/warranty).

**Equivalency:**

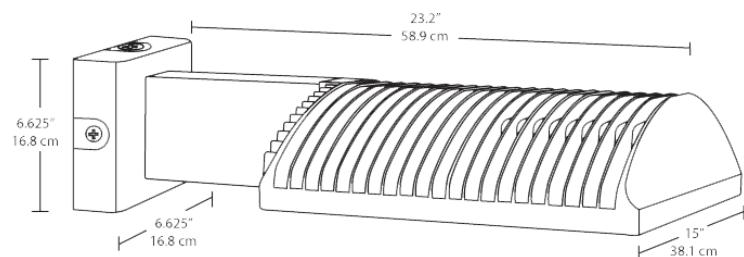
Equivalent to 250W Metal Halide

**Buy American Act Compliance:**

RAB values USA manufacturing! Upon request, RAB may be able to manufacture this product to be compliant with the Buy American Act (BAA). Please contact customer service to request a quote for the product to be made BAA compliant.

**Optical****BUG Rating:**

B0 U0 G1

**Dimensions****Features**

High performance LED light engine

Maintains 70% of initial lumens at 100,000-hours

Weatherproof high temperature silicone gaskets

Superior heat sinking with die cast aluminum housing and external fins

Replaces 400W MH

100 up to 277 Volts

5-Year, No-Compromise Warranty

**Ordering Matrix**

Family	Optics	Wattage	Color Temp	Mounting	Finish	Driver Options	Other Options
WPLED	4T	50	Y				
4T = Type IV 3T = Type III 2T = Type II	50 = 50W 78 = 78W 105 = 105W 125 = 125W 150 = 150W	50 = 5000K (Cool) N = 4000K (Neutral) Y = 3000K (Warm)	Blank = 5000K FX = Flat Mount	Blank = Arm FX = Flat Mount	W = White Blank = Bronze	Blank = Standard /480 = 480V /BL = Bi-Level /D10 = 0-10V Dimming /480/D10 = 480V 0-10V Dimming	Blank = Standard /PC = 120V Photocell /PC2 = 277V Photocell /PCT = 120-277V Twistlock Photocell /PCT4 = 480V Twistlock Photocell /PCS = 120V Swivel Photocell /PCS2 = 277V Swivel Photocell /PCS4 = 480V Swivel Photocell /WS = Multi-Level Motion Sensor /WS2 = Multi-Level Motion Sensor (20 ft. mt. ht.) /WS4 = Multi-Level Motion Sensor (40 ft. mt. ht.) /LC = Lightcloud® Controller



Specification grade area lights available in IES Type IV distributions. Best-in-class 5-G vibration rating. 5-year, limited warranty.

Color: Bronze

Weight: 20.9 lbs

**Project:**

**Type:**

**Prepared By:**

**Date:**

**Driver Info**

Type	Constant Current	Watts	65W
120V	0.55A	Color Temp	3000K (Warm)
208V	0.33A	Color Accuracy	71 CRI
240V	0.29A	L70 Lifespan	100,000 Hours
277V	0.25A	Lumens	6,300
		Input Watts	65.4W
		Efficacy	96.3 lm/W

## Technical Specifications

### Compliance

#### UL Listed:

Suitable for wet locations

#### IESNA LM-79 & IESNA LM-80 Testing:

RAB LED luminaires and LED components have been tested by an independent laboratory in accordance with IESNA LM-79 and LM-80.

#### Dark Sky Conformance:

Conforms to (allows for conformance to) the requirements for the IDA's "Fixture Seal of Approval" as of March 1, 2016.

#### DLC Listed:

This product is on the Design Lights Consortium (DLC) Qualified Products List and is eligible for rebates from DLC Member Utilities.

DLC Product Code: PTFWA65X

### Performance

#### Lifespan:

100,000-Hour LED lifespan based on IES LM-80 results and TM-21 calculations

### Construction

#### IES Classification:

The Type IV distribution (also known as a Forward Throw) produces a semicircular distribute, and is especially suited for mounting on sides of buildings or walls and perimeter of parking areas

#### Effective Projected Area:

EPA = 0.8

#### Vibration Rating:

Industry-leading 5G vibration rating per ANSI C136.31

#### Cold Weather Starting:

Minimum starting temperature is -40°C (-40°F)

#### Ambient Temperature:

Suitable for use in up to 40°C (104°F)

### Housing:

Precision die-cast aluminum housing

#### Mounting:

Mounts to RAB square poles. Mounts to all square and round poles if optional Universal Pole Adaptor is used.

#### Lens:

Clear acrylic lens with integrated optics

#### IP Rating:

Ingress protection rating of IP66 for dust and water

**Technical Specifications (continued)****Construction****Gaskets:**

High-temperature silicone gaskets

**Finish:**

Formulated for high durability and long-lasting color

**Green Technology:**

Mercury and UV free. RoHS-compliant components.

**LED Characteristics****LEDs:**

Long-life, high-efficacy, surface-mount LEDs

**Color Uniformity:**

RAB's range of Correlated Color Temperature follows the guidelines of the American National Standard for Specifications for the Chromaticity of Solid State Lighting (SSL) Products, ANSI C78.377-2017.

**Electrical****Driver:** Constant Current, Class 2, 100-277V, 50/60 Hz, 120V: 0.55A, 208V: 0.33A, 240V: 0.29A, 277V: 0.25A**THD:**

7.2% at 120, 10.6% at 277V

**Power Factor:**

99.6% at 120V, 95.4% at 277V

**Surge Protection:**

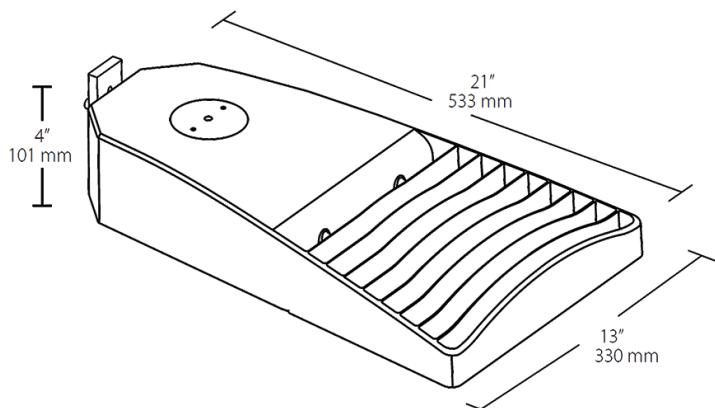
L-N 4kV; L-L, L-GND 6kV

**Other****5 Yr Limited Warranty:**Fixture operation and paint finish are covered for a period of 5-years. [See our full warranty here.](#)**Equivalency:**

Equivalent to 150W Metal Halide

**Buy American Act Compliance:**

RAB values USA manufacturing! Upon request, RAB may be able to manufacture this product to be compliant with the Buy American Act (BAA). Please contact customer service to request a quote for the product to be made BAA compliant.

**Dimensions****Features**

100,000-hour LED lifespan

Type IV distribution

**Ordering Matrix**

Family	IES Type	Wattage	Color Temp	Finish	Driver Options	Mounting	Sensor Options	Accessories	Other Options
LOT	4T	65	Y		/D10				
	<b>5T =</b> Type V	<b>65 = 65W</b> <b>110 =</b> 110W	<b>Blank =</b> 5000K (Cool)	<b>Blank =</b> Bronze	<b>/D10 =</b> 120-277V, 0-10V Dimming (standard)	<b>Blank =</b> Mounts to RAB square poles	<b>Blank = None</b> <b>/PCT =</b> 120-277V Twistlock PC	<b>Blank = None</b> <b>/HS =</b> 2 House-Side-Shields <sup>1</sup>	<b>Blank =</b> Standard
	<b>4T =</b> Type IV	<b>110W</b> <b>160 =</b> 160W	<b>N = 4000K</b> (Neutral)	<b>W =</b> White	<b>/480/D10 =</b> 480V, 0-10V Dimming	<b>/UPA =</b> Universal Pole Adaptor	<b>/PCT4 =</b> 480V Twistlock PC <b>/WS2 =</b> Wattstopper Sensor + 20ft lens, 120-277V <b>/WS4 =</b> Wattstopper Sensor + 40ft lens, 120-277V <b>/5PR =</b> 5-Pin Receptacle, no PCT <b>/7PR =</b> 7-Pin Receptacle, no PCT		<b>USA =</b> BAA Compliant
	<b>3T =</b> Type III		<b>Y = 3000K</b> (Warm)				<b>/BL =</b> Bi-Level Dimming, 120-277V		
	<b>2T =</b>								
	Type II								

<sup>1</sup> Available for Types II, III and IV only



Square steel poles drilled for 2 Area Lights at 180°. Designed for ground mounting. Poles are stocked nationwide for quick shipment. Protective packaging ensures poles arrive at the job site good as new.

Color: Bronze

Weight: 136.7 lbs

Project:

Type:

Prepared By:

Date:

## Technical Specifications

### Compliance

#### CSA Listed:

Suitable for wet locations

### Construction

#### Shaft:

46,000 p.s.i. minimum yield.

#### Hand Holes:

Reinforced with grounding lug and removable cover

#### Base Plates:

Slotted base plates 36,000 p.s.i.

#### Shipping Protection:

All poles are shipped in individual corrugated cartons to prevent finish damage

### Color:

Bronze powder coating

### Shaft Size:

4"

### Height:

20 FT

### Hand Hole Dimensions:

3" x 5"

### Weight:

137 lbs

### Bolt Circle:

8 1/2"

### Gauge:

11

### Base Dimension:

8"

### Wall Thickness:

1/8"

## Technical Specifications (continued)

### Construction

#### Anchor Bolt:

Galvanized anchor bolts and galvanized hardware and anchor bolt template. All bolts have a 3" hook.

#### Anchor Bolt Templates:

WARNING Template must be printed on 11" x 17" sheet for actual size. CHECK SCALE BEFORE USING. Templates shipped with anchor bolts and available [online](#).

### Pre-Shipped Anchor Bolts:

Bolts can be pre-shipped upon request for additional freight charge

#### Max EPA's/Max Weights:

70MPH 10.7 ft./360 lb.  
80MPH 7.0 ft./350 lb.  
90MPH 4.3 ft./350 lb.  
100MPH 2.5 ft./350 lb.  
110MPH 1.1 ft./350 lb.  
120MPH 0.1 ft./340lb

### Other

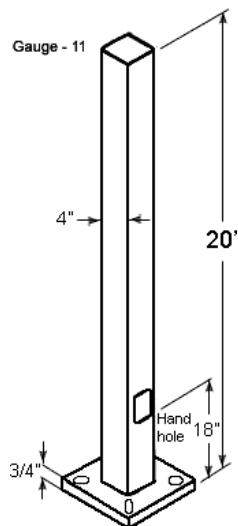
#### Terms of Sale:

Pole Terms of Sale is available [online](#).

#### Buy American Act Compliance:

RAB values USA manufacturing! Upon request, RAB may be able to manufacture this product to be compliant with the Buy American Act (BAA). Please contact customer service to request a quote for the product to be made BAA compliant.

### Dimensions



### Features

Designed for ground mounting

Heavy duty TGIC polyester coating

Reinforced hand holes with grounding lug and removable cover for easy wiring access

Pole caps, base covers & bolts are sold separately

Custom manufactured for each application

# Memo

To: Members, Springboro Planning Commission  
From: Dan Boron, Staff Liaison  
Date: January 6, 2022  
Re: Springboro Master Plan Review and Adoption

---

A draft copy of the Springboro Master Plan is enclosed. The Planning Commission will be asked to review and recommend for adoption the Master Plan at its Wednesday, January 19, 2022 meeting. The plan would replace a similar plan adopted by City Council in 2009 and would be used by City Council, Planning Commission, and City staff to make long range decisions on development and redevelopment in the City. City staff is distributing the report now in order to provide members with the time to review the plan, and ask any questions of City staff well in advance of the meeting.

The Master Plan has been under review by the City since August 2020. At that time City staff solicited proposals from consulting firms, shortlisted to three consulting teams from among 17 proposals received, and conducted interviews along with representatives of the Master Plan Steering Committee, selecting a team consisting of MKSK as lead consultant, supported by Epic Small, and The Greenway Collaborative, to complete the plan. MKSK began work in September 2020, and the nine-member steering committee began meeting in October 2020 and continued to work through the pandemic meeting in person and virtually, conducted two rounds of public input meetings, and two surveys. Planning Commission members Becky Iverson and Steve Harding served on the Steering Committee. At its meeting on December 14th, the Steering Committee approved a recommendation to adopt the plan included in this packet that includes a number of revisions incorporated into the document.

Representatives from the MKSK team will be participating in the meeting on January 19th to present along with City staff the planning process used to complete the plan and its recommendations.

The remainder of the packet for the January 19th meeting will be distributed next week.

If you have any questions about the attachments or tomorrow's meeting please do not hesitate in reaching out to me at [danb@cityofspringboro.com](mailto:danb@cityofspringboro.com) or (937)748-6183. Thank you.

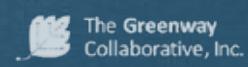


# SPRINGBORO MASTER PLAN

12/20/2021 DRAFT



MKSK



# ACKNOWLEDGMENTS

## **City of Springboro**

Chris Pozzuto, City Manager

Dan Boron, AICP, City Planner

## **Steering Committee**

Becky Iverson, Chair, Member, City Council

Steve Harding, Member, City Council

Matt Clark, Administrator, Clearcreek Township

Suzanne Geisler, Park Board

Larry Hook, Superintendent, Springboro Community Schools

Jim Hough, Springboro Chamber of Commerce

Deann Hurtado, Secretary

Matt Leedy, Architectural Review Board

Chris Reid, Mayor's Advisory Council

John Agenbroad, Mayor - Ex Officio Member

Chris Pozzuto, City Manager - Ex Officio Member

Dan Boron, Community Planner - Ex Officio Member

## **Consultants**

Chris Hermann, Principal, AICP | MKSK

Sean Hare, Project Manager, AICP | MKSK

Juliana Silveira, Planner | MKSK

Catherine Clarke, Planner | MKSK

Norman Cox | The Greenway Collaborative

Jessica Mathews | EPIC Small

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# **SECTION 1**

# **EXECUTIVE**

# **SUMMARY**



# Executive Summary

The Springboro Master Plan is an update to a previous land use plan for the City of Springboro. The city has established itself as a premier residential community in the heart of southwestern Ohio. Known for its strong neighborhoods and schools, and its proximity to both Dayton and Cincinnati, Springboro has become an ideal place to live.

As Springboro's reputation has grown, so has its population and the complexities of that group. No longer is Springboro the home or the traditional nuclear family. Today, the city is attracting more diverse residents who are looking for Springboro to offer even more than it currently does.

This Springboro Master Plan focuses on just that, building on what has made Springboro such an ideal place for decades, to increase the different opportunities for living, shopping, and spending time. The plan is guided by four primary themes: Growth, Experience, Connectivity, and Identity, each of which is supported by multiple objectives.

These four themes, combined with an analysis of the city and community and stakeholder engagement has led to the recommendations in Section 5 of this document. The recommendations are in three main categories: Future Land Use, Connectivity, and Placemaking. These categories help depict a future vision for Springboro that celebrates the unique history and character of the community, and recognizes the opportunities to enhance and improve the way of life for both existing and new residents for the next decade.







## **SECTION 2**

# **MASTER PLAN THEMES**



# Master Plan Themes

The Springboro Master Plan is guided by a series of 4 Themes. These themes were developed after receiving input and guidance from the public, key stakeholders, and city staff. The four themes are Growth, Experience, Connectivity, and Identity. Each theme is further supported by multiple objectives that provide more specific clarity and direction. These Four Themes were developed early in the master planning process and provided guidance to the planning team when identifying future recommendations and initiatives to be a part of the Master Plan.

*“New growth should reflect the character of the current city.”*

*“Focus more on the connectivity and the identity than the growth.”*

# 4 Themes

**86%** of survey  
respondents said  
these themes  
properly reflect  
the future vision of  
Springboro



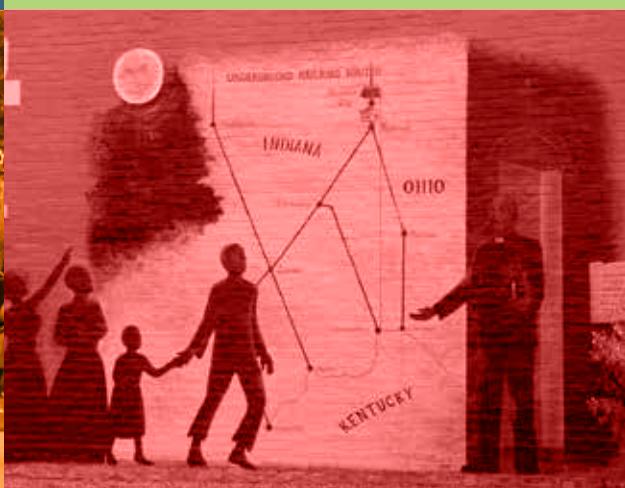
**GROWTH**



**EXPERIENCE**



**CONNECTIVITY**



**IDENTITY**



## GROWTH

The opportunity for growth within Springboro is a critical piece of the city's future.

Springboro is primarily a residential city, composed of strong neighborhoods that define its character. Looking forward, Springboro has limited opportunities for future population growth and thus needs to be strategic in future development planning. Time and time again members of the community shared their concerns about Springboro's capability to be an "age-in-place" community. This means a community in which there are enough different housing options to accommodate people in different stages of their lives in order to be able to live in the same community continuously, if they so choose. This does not just mean senior living opportunities but also appropriate housing options for younger single people, retirees, empty-nesters and others that may want to live in Springboro because of all it has to offer but doesn't need or desire a larger single-family home. Any future development will also have potential impacts on the existing infrastructure in the city and will need to be cognizant of those impacts.

## Objectives

As part of the Growth theme, three objectives were identified. They are listed below in no particular order.

- » Maintain and enhance our strong neighborhoods.
- » Promote an age-in-place community through development standards, community assets, and more diversity in new housing stock (type, amenities, walkability).
- » Recognize and act strategically on the few growth opportunities left for the city, including greenfield sites and infill/redevelopment opportunities.

*"A thriving, multi-generational community."*

*"Safe, fun, affordable, interactive, specialty..."*



## EXPERIENCE

While Springboro is predominantly a residential community, these residential neighborhoods are supported by several prominent commercial areas that offer the city places to spend time and promote a high quality of life. These areas include the historic downtown and the many shopping centers along State Routes 73 and 741. The Experience theme can best be understood as direction for the city to continue to enhance and improve these commercial areas to attract new and existing residents to them, along with employers, workers, and visitors of the city. Additionally, part of what makes Springboro unique is its history. The story of Springboro is one that should be shared and reinforced throughout the community.

*“No time to be timid...”*

*“The best place to live in SW Ohio...”*

*“A meeting place for friends and family...”*

## Objectives

The Experience Theme has three objectives that further clarify its importance.

- » Support and recruit businesses and experiences that will help Springboro stand out and attract residents, employers, and visitors.
- » Continue to reinforce and improve historic downtown businesses and experiences.
- » Continue to anticipate and meet the quality of life needs of the city.



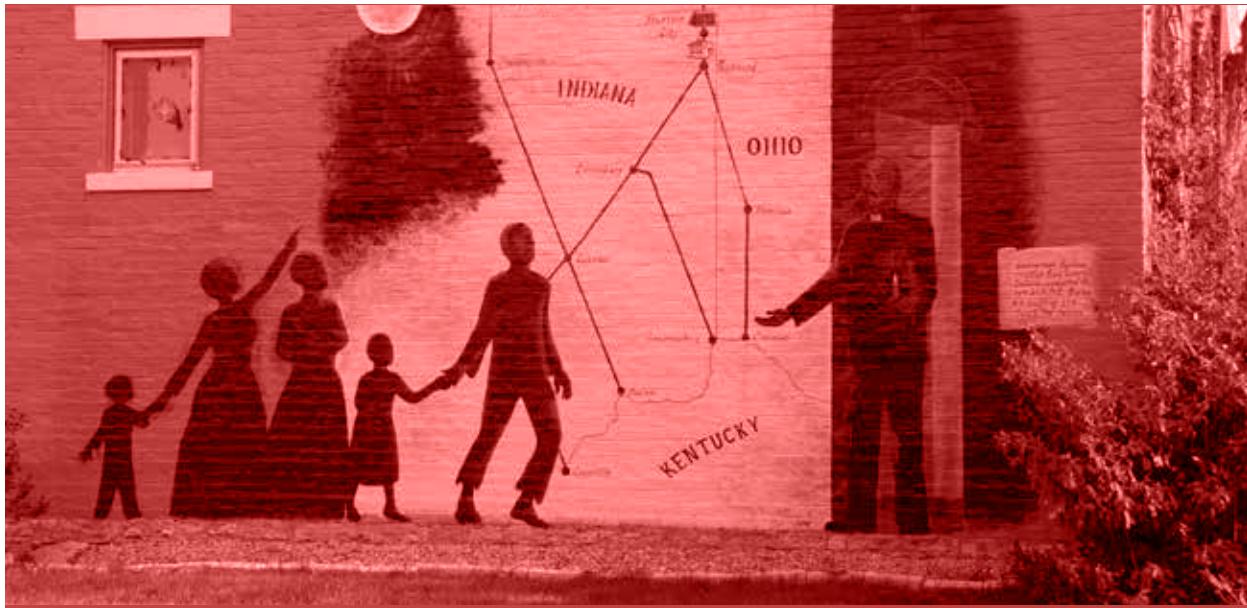
## CONNECTIVITY

Springboro has become a leader in connectivity planning in the region. The Bicycle and Pedestrian Plan has guided investment in active transportation infrastructure throughout the city. The community continues to support these initiatives and they are a major boost to the quality of life and what makes Springboro an attractive place to live. In addition to larger projects, there is a strong desire from residents to increase walkability within neighborhoods, and around the historic downtown and Wright Station.

### Objectives

The objectives of the Connectivity Theme are:

- » Continue to implement the vision for active transportation modes set out in the Bicycle and Pedestrian Plan.
- » Improve connectivity between historic downtown and Wright Station.
- » Work with regional partners to continue to improve and interconnect the overall city street network.



## IDENTITY

One of the defining characteristics of Springboro is the inherent character already in place. The character of the community has been developed by its rich history and strong sense of community. Residents made it clear that celebrating the unique qualities of Springboro should be a key theme of this plan. Enhancing the identity of a community can be done in many ways. Public art, infrastructure, architecture, and community events are just some of the ways identity can be established and improved.

## Objectives

The objectives of the Identity Theme are:

- » Celebrate the City's history through storytelling and placemaking.
- » Develop a cohesive strategy for reinforcing and enhancing the visual identity of Springboro.
- » Continue to improve and develop community assets as a major attraction for Springboro.

*“A desired small-town, historical destination.”*

*“Embracing livability, arts, and history...”*



A large, semi-transparent teal arrow graphic points from the bottom left towards the center of the page.

## **SECTION 3**

# **COMMUNITY & STAKEHOLDER ENGAGEMENT**



# Community & Stakeholder Engagement

**“ Cities have the capability of providing something for everybody, only because, and only when, they are created by everybody.**

*Jane Jacobs*

Community and stakeholder engagement is critical to developing a master plan that properly reflects the vision of the city and ensures that there is momentum to carry the plan from creation into implementation. In creating this plan, the consultant team utilized three channels for engagement. First, city staff from various departments provided insight to the process. Second, a planning committee was established that included various stakeholders from the community. These stakeholders represented residents, business owners, schools, Clearcreek Township, and other interest groups. This committee participated in five meetings throughout the planning process, providing key feedback and direction to the plan. The last group that was engaged was the community at large. In November of 2020, an open house was held at Heatherwoode Golf Club to discuss initial goals for the plan. Throughout the plan, two online public surveys were administered and summaries of the key takeaways from this engagement is summarized below. In all, there were almost 1,000 engagement interactions throughout the process.

**780 Total  
Respondents  
(phases 1&2)**

**55 were non  
residents**

**Almost 40%  
of respondents  
go to parks within  
Springboro at least  
once a week.**



## PHASE 1 ENGAGEMENT

In the first phase of the planning process, our goal was to learn about Springboro both from a physical analysis and from the input of the community and stakeholders about life in Springboro and their vision for the future of their city. An initial online survey was administered from November 1st to December 15th of 2020 to gain insights on several categories as it related to the existing conditions of the city and to gather insights on the goals of the community for the future of Springboro. The first survey yielded 430 responses from city residents and the key takeaways are as follow.

### What areas (or aspects) of Springboro are working well?

- » The common themes were the Historic Downtown, the Parks, Wright Station, Transportation, Businesses and Shopping areas, and Bicycle infrastructure.

### What areas (or aspects) of Springboro are NOT working well?

- » The common themes were related to parking issues, traffic, concerns over new development, and Route 741 (traffic and speed).

### Parks and Trails.

- » The most common requests for future park improvements included more trails, more and improved playgrounds and splash pads, dog parks, as well as creating more natural areas.

### Housing and Population Growth.

- » When asked about future housing development, residents responded most favorably to Smaller Single-Family homes, followed by Larger Single-Family homes,

Townhomes, and Senior Living. Many comments followed up by communicating a desire to see housing more friendly to young professionals, single people, young families, and older retirees.

#### Retaining Residents.

- » The survey asked questions about how long respondents intended to remain in Springboro and if they were moving soon, why that would be the case. Most respondents intended to stay in their current residence for over 10 years, which is a strong sign that Springboro is meeting the needs and desires of most of its residents. Even amongst respondents who intended to move within the next 5 years, 65% wanted to remain in Springboro. The largest cohort of those electing to move within 5 years were looking for a smaller residence.

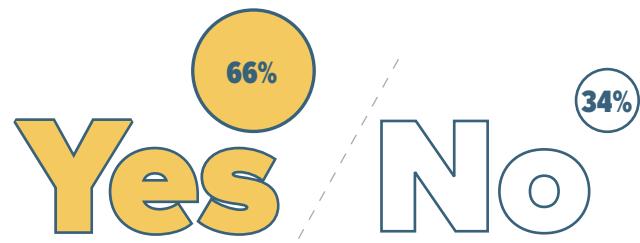
#### Entertainment & Things to Do.

- » Residents are eager to spend more time and nights out in Springboro. They are primarily looking for High-End Dining, Cafes, Entertainment Venues, and Breweries as destinations to keep them in Springboro as opposed to leaving the city for entertainment and experiences.

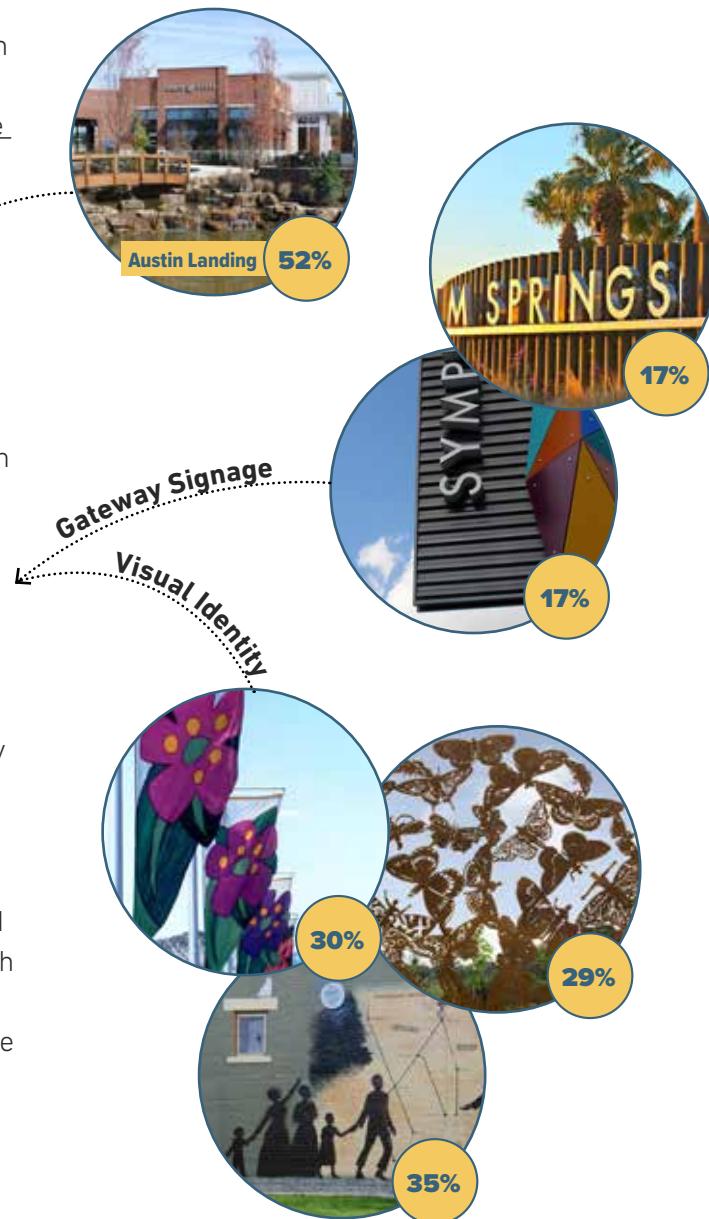
#### Public Art.

- » There is a real interest by the community in public art within Springboro. The most common themes in their preference for public art was to acknowledge the history of the community as well as preserving and paying homage to its character with new installations.

The feedback from the first public survey and the open house, along with conversations with the Planning Committee, helped identify the plan's themes and objectives that would guide the plan's recommendations.



**63% of  
respondents  
support age friendly  
development**





## PHASE 2 ENGAGEMENT

After the development of initial plan recommendations, a second online survey was developed to solicit additional feedback and guidance from the community. There were over 350 responses to this second survey from residents of Springboro. Understanding the common goals and vision for the community in the first phase is a critical step, however, conducting additional engagement is necessary to validate our interpretation of that vision as it took shape in various planning recommendations. The key takeaways of the second survey and feedback from the planning committee are summarized below.

### Top themes from written answers.

- » The top themes from written answers in the second survey were to Control Growth, to Protect Schools, Support Parks, Examine new Housing Types, and Calm Traffic.

### The Plan's Themes and Objectives.

- » 86% of respondents felt the themes and objectives for the plan (Section 2 of the plan) properly reflect the future vision for Springboro.

### Housing.

- » Housing, new development, and population growth was a major focus of the second survey as it is a critical issue for the city going forward. With few new development opportunities within the boundaries of Springboro, the city needs to be smart and act strategically when it faces new development proposals to ensure the plans are reflective of the vision of the residents.

Respondents also provided direction on where they preferred to see new types of housing within the city. Denser housing typologies like townhomes and small apartment buildings were most encouraged along SR73 and SR741 as well as some encouragement for these in the Historic Downtown area.

### Age-in-Place.

63% of respondents agreed that the city should focus on utilizing new housing development in the city to make Springboro more age-in-place friendly.

### Connectivity.

The survey provided residents with 7 goals related to connectivity to determine which the community would receive favorably if they were pursued. 6 of the 7 were received favorably and are included in Section 5 of this plan.

### Placemaking and Public Art.

The survey also presented a series of placemaking and public art ideas to gather input on which of these would be received favorably, if pursued by the city. 5 of the 7 were received favorably. The other 2 were not included in this plan document based on feedback from the residents. Placemaking recommendations can be found in Section 5 of this plan.

*“Housing affordability is a huge concern for both young and aging population, as well as general working class population.”*



## Growth Scenarios Exercise

One exercise presented in the online survey was a hypothetical growth scenario exercise which offered residents a glimpse of the impact of future development on population growth, attraction for new retail establishments, and the impact on schools.

Respondents were presented with three growth scenarios for the city. Each scenario looked at developing 50% or less of the available land in city for residential uses. Between 30-40% of the currently undeveloped land would remain conserved as natural areas or farmland in this exercise.

To simplify the exercise, we primarily looked at the impacts of residential development on the city and used four different housing densities to communicate the differences. The four housing typologies used were: single family (based on existing single-family densities) at 4 dwelling units per acre (DU/A), cluster housing at 8 DU/A, townhomes at 15 DU/A, and apartments/flats at 25 DU/A. Based on the percentage of new housing devoted to each category we can approximate the number of new housing units, new residents, and thus the impacts of the new development on retail attraction, schools, and the capability for Springboro to meet the age-in-place goals of the community.

Retail and restaurant attraction is often based on the number of households and then the population within a given geographic area. The impact of housing development on schools is best understood by examining what types of households will exist within each type of development. Single-family housing is most likely to include families with children, whereas rental properties (especially higher-end market rate rentals) are more likely to attract younger single people and seniors or

retirees and thus contribute less to the school population despite providing a higher density of units. Based on the current housing stock in the city (which is approximately 90% single family) meeting the age-in-place goals of the community would require offering more diversity in housing options.

The three scenarios can be understood as:

**Scenario 1** applies existing housing typology percentages to new development. This would mean 90% of new housing is single-family. This scenario offers the least number of new households and new residents to the city because of the lower density. This also means that the potential to attract new retail and to address the age-in-place goals of the community are low. Also, since single family homes are most likely to bring more children to the city, this will have a significant impact on the school system.

**Scenario 2** focuses on creating the most diversity in housing stock available in the city. This scenario offers a moratorium on new single-family housing and focuses all new development on denser housing typologies. This scenario brings the most new housing units and most new residents to the city. The potential for new retail attraction and addressing the age-in-place goals are high in this scenario. The impact on schools is rated as low because the household types that typically live in these types of housing are not families with school age populations.

**Scenario 3** took a more balanced approach. It proposed 20% of new residential development in the city would be traditional single-family housing, but the other 80% would be made up of denser housing options (predominantly cluster housing and townhomes). This scenario lands in the middle of the three in terms of new housing units and new residents

#### SCENARIO 1

Focuses on continuing development patterns as they currently are. This would mean almost exclusively developing detached single-family, with very small amounts of cluster housing or townhomes.

##### DEVELOPMENT BREAKDOWN

30% Conservation  
20% Commercial  
50% Residential

##### RESIDENTIAL BREAKDOWN

90% Single Family  
5% Cluster Housing  
5% Townhomes  
0% Apartments

##### YIELDS

980+ New Units  
2,800+ New Residents

##### RETAIL ATTRACTION POTENTIAL

Low

##### IMPACT ON SCHOOLS

High

##### ADDRESS AGE-IN-PLACE

Low

#### SCENARIO 2

Goal of this scenario is to maximize the diversity of housing stock in the city. This operates under the notion that there is enough detached single-family housing currently available.

##### DEVELOPMENT BREAKDOWN

30% Conservation  
20% Commercial  
50% Residential

##### RESIDENTIAL BREAKDOWN

0% Single Family  
40% Cluster Housing  
30% Townhomes  
30% Apartments

##### YIELDS

3,400+ New Units  
8,700+ New Residents

##### RETAIL ATTRACTION POTENTIAL

High

##### IMPACT ON SCHOOLS

Low

##### ADDRESS AGE-IN-PLACE

High

#### SCENARIO 3

This scenario diversifies housing stock for younger and older populations while acknowledging that single-family development will still happen.

##### DEVELOPMENT BREAKDOWN

40% Conservation  
15% Commercial  
45% Residential

##### RESIDENTIAL BREAKDOWN

20% Single Family  
40% Cluster Housing  
30% Townhomes  
10% Apartments

##### YIELDS

2,100+ New Units  
5,400+ New Residents

##### RETAIL ATTRACTION POTENTIAL

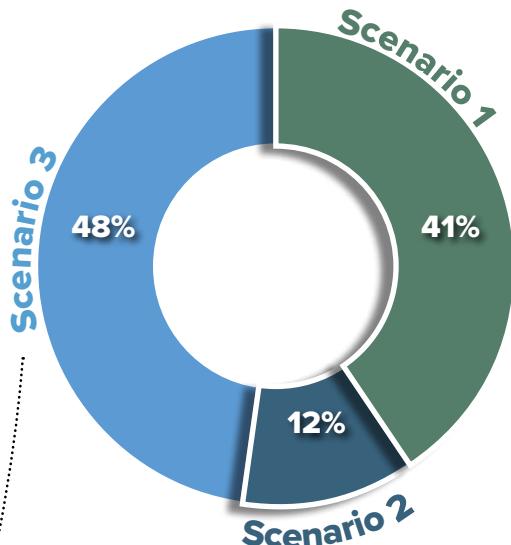
Medium

##### IMPACT ON SCHOOLS

Low-Medium

##### ADDRESS AGE-IN-PLACE

Medium



*“I really like the idea of moving towards housing types other than single-family homes. I would just want to see that new apartments being built are exceptionally nice and/or unique—not just a giant box that a bunch of people live in.”*

*“Schools being able to handle growth in all scenarios are my biggest concern.”*

*“We need more neighborhoods geared towards 55+.”*

added. It is rated a medium for its potential to attract new retail and address the age-in-place goal. It was rated as a low-medium impact on schools because while it provides some new single-family housing, it primarily focuses on housing types that are typically not occupied by school age populations.

When presented with these three growth scenarios, respondents were asked which scenario they felt was most appropriate for Springboro moving forward and the results can be seen at right. The most popular scenario was Scenario 3 with 48% of respondents selecting this as the most appropriate one for the future of Springboro. 60% of residents felt that something other than Scenario 1 was most appropriate. This is interesting because Scenario 1 represents a continuation of the current housing stock breakdown. Some of the comments provided by respondents with this exercise suggested that they were acceptant of denser housing so long as they were high quality in design, and that they address the age-in-place problem facing the city.

This was an incredibly important exercise for the city's residents and staff to understand the long-term impacts of different development types on a variety of aspects of life within Springboro. However, this scenario was one hypothetical exercise and is not establishing an official policy for the City, but rather provides some guidance to decision makers when development proposals are submitted.

Engagement throughout the process has not only shaped what was included in this plan's recommendations but also will guide future decision makers of the city when reviewing development proposals as well as determining various initiatives and projects to pursue.





## **SECTION 4**

# **EXISTING CONDITIONS**



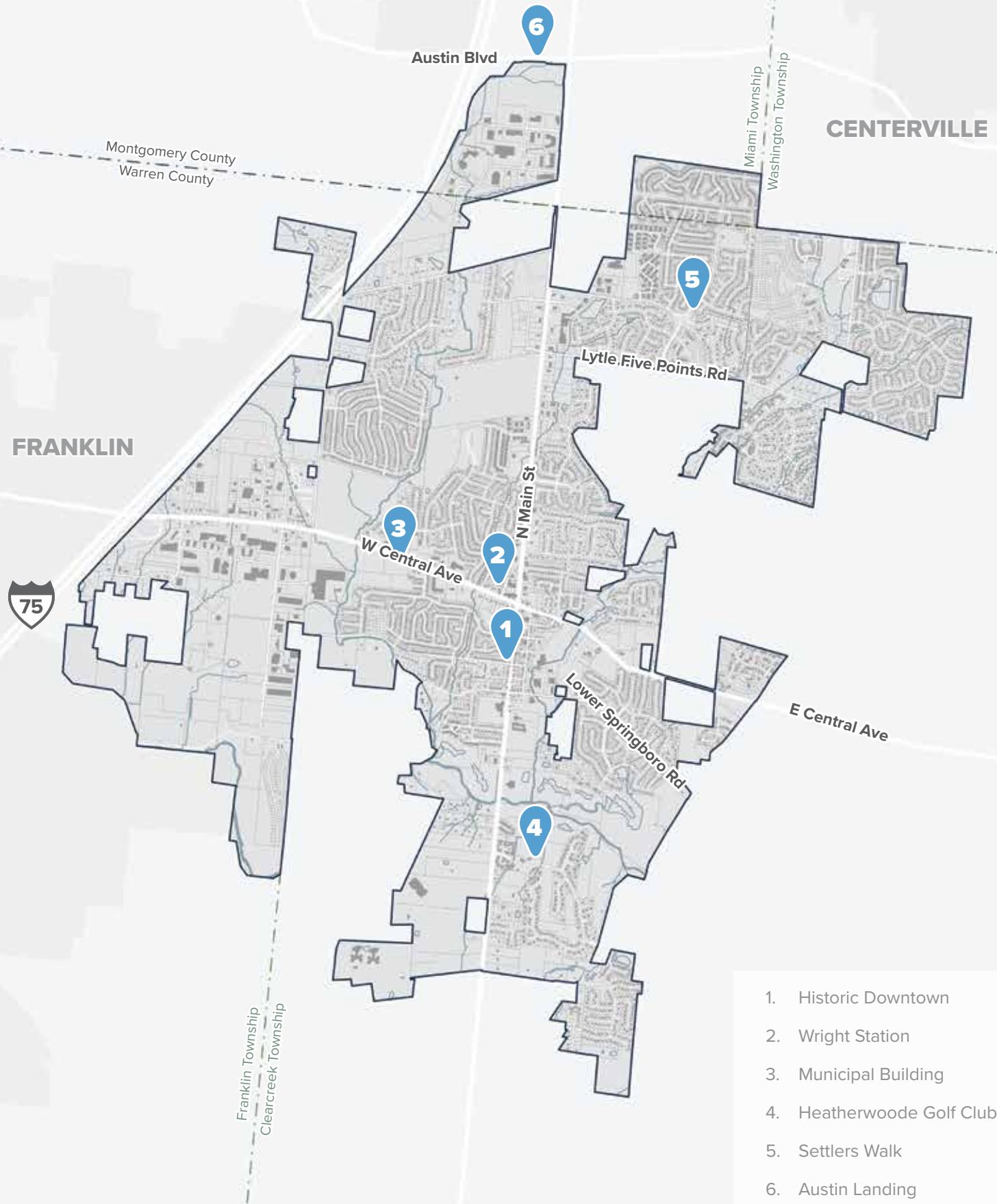
# Existing Conditions

As part of the master planning process, the planning team conducted an analysis of the existing conditions of Springboro. This analysis examined current land uses, parks and trails, housing and growth, demographics, Springboro's regional context, and its history and identity. This foundational analysis, in conjunction with the results of stakeholder and public engagement helped to inform and guide the plan recommendations found in Section 5.

This analysis was conducted to help answer the following questions:

- » How is land currently used in Springboro?
- » What is Springboro's place in the region?
- » What can we learn about Springboro's population of residents?
- » What makes Springboro unique?

# CITY OF SPRINGBORO



1. Historic Downtown
2. Wright Station
3. Municipal Building
4. Heatherwoode Golf Club
5. Settlers Walk
6. Austin Landing

## EXISTING LAND USE

The map on the opposite page shows the existing land uses by parcel for the City of Springboro. These land use categories can be understood as:

- » Residential (yellow), which includes all residential properties regardless of housing type. This means it includes single-family, apartments, senior living, and others.
- » The Park (dark green) category includes all public parks within the City's boundary.
- » Agricultural (light green) use includes existing farmland as well as some undeveloped parcels within the city.
- » Institutional/Public (blue) category includes schools, government buildings and other public uses.
- » Industrial parcels (purple) include industrial businesses located within the City.
- » Commercial (red) parcels include businesses including retail, restaurants, offices and more.
- » Public Utilities (light gray) includes public utility facilities.

When looking at the breakdown of existing land uses by acreage, we get the following pie graph. 50% of the acreage of the city is currently residential in use. This makes sense considering the bedroom community lifestyle that has arisen here in Springboro. Interestingly, all other uses are well balanced in terms of how much acreage they consume with them all around 10%. This furthers the general lifestyle of Springboro being primarily focused on the residential neighborhoods of the city.

RESIDENTIAL



PARK

LAND USE PATTERN  
AGRICULTURAL/  
UNDEVELOPEDINSTITUTIONAL/  
PUBLIC

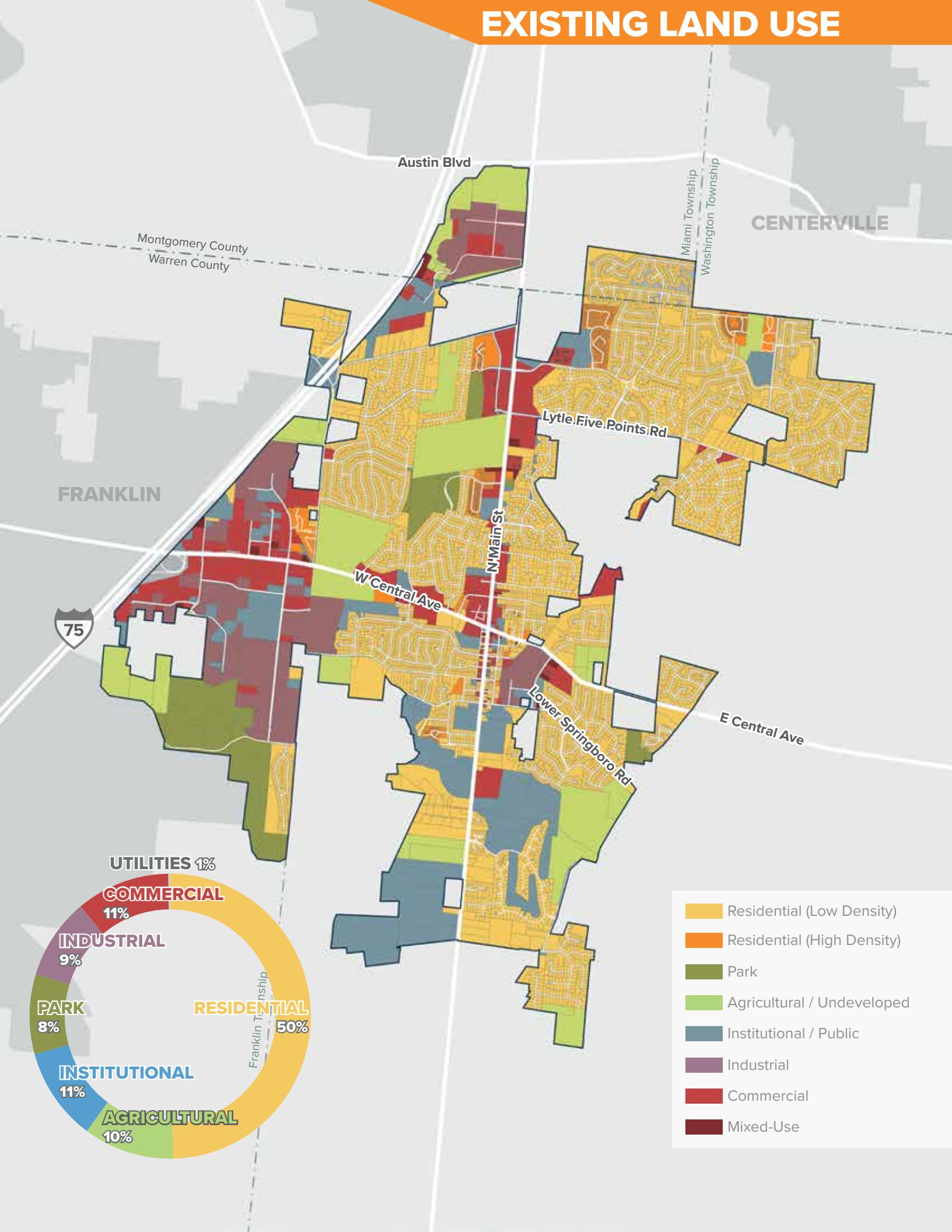
INDUSTRIAL



COMMERCIAL



# EXISTING LAND USE



## PARKS AND TRAILS

Springboro's parks are a commonly praised amenity and feature of the community. Parks serve our community to provide access to nature, host sporting and community events and serve as a primary gathering space. Springboro has gone a step further to develop a future vision for their parks that includes trails that connect the various parks to one another. 40% of residents are going to a park in Springboro at least once a week. That is a sign of high usage and shows how important the parks system is to life in this community.

The trail and bikeway planning efforts that began in the Bicycle and Pedestrian Plan for Springboro will help to connect parks to neighborhoods and other community destinations. The Central Greenway, as planned in the Bicycle and Pedestrian Plan will connect Clearcreek Park, Community Park, North Park, and Gardner Park to one another and surrounding neighborhoods. The Central Greenway would drive further attendance to the parks and create a unique community asset for existing and future residents of Springboro.

*"Allow for more parks and integrated green space. The age in place concept will be bolstered as people of all ages will want options to exercise, play, or relax."*



KACIE JANE PARK

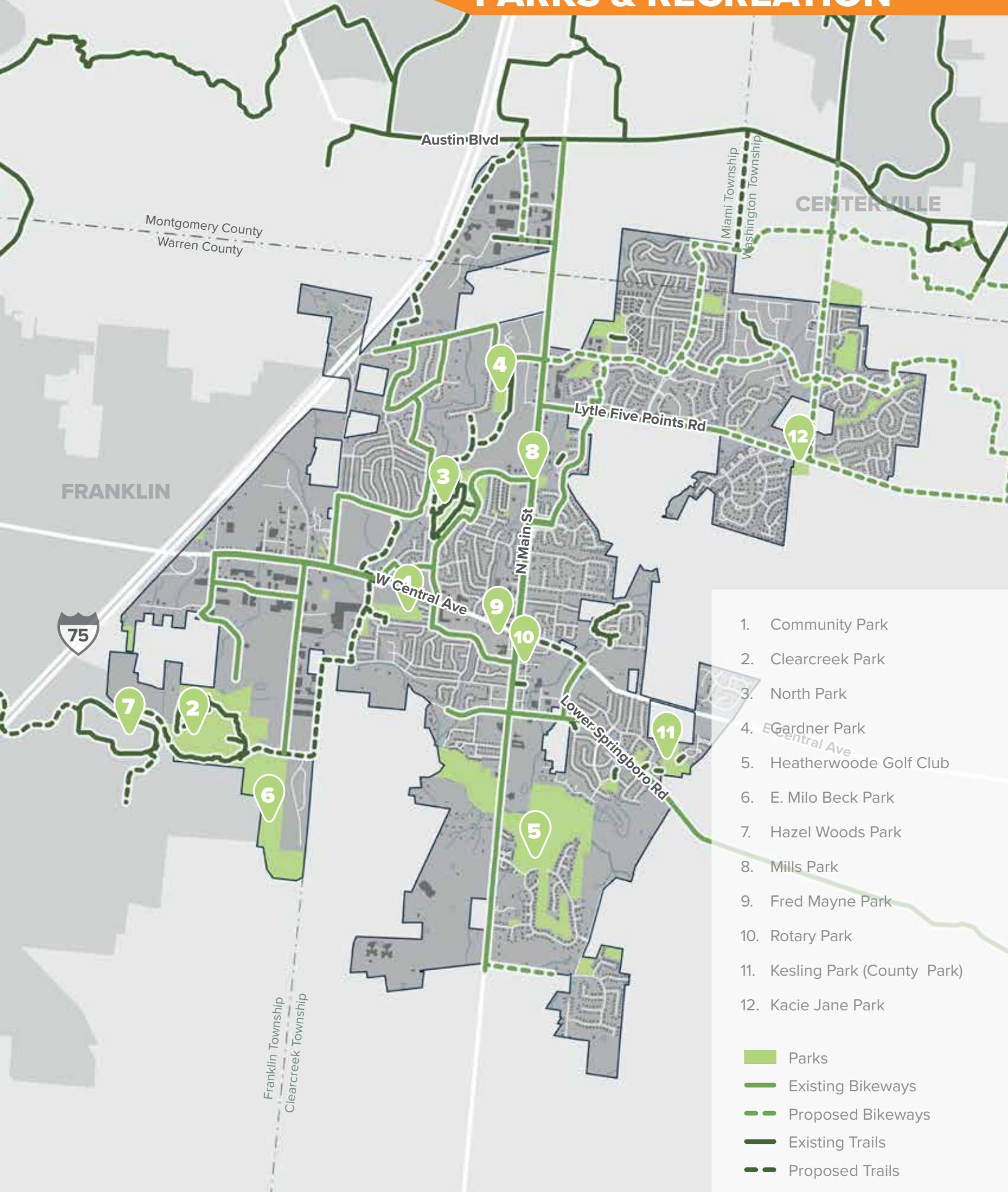


E. MILO BECK PARK



NORTH PARK

# PARKS & RECREATION

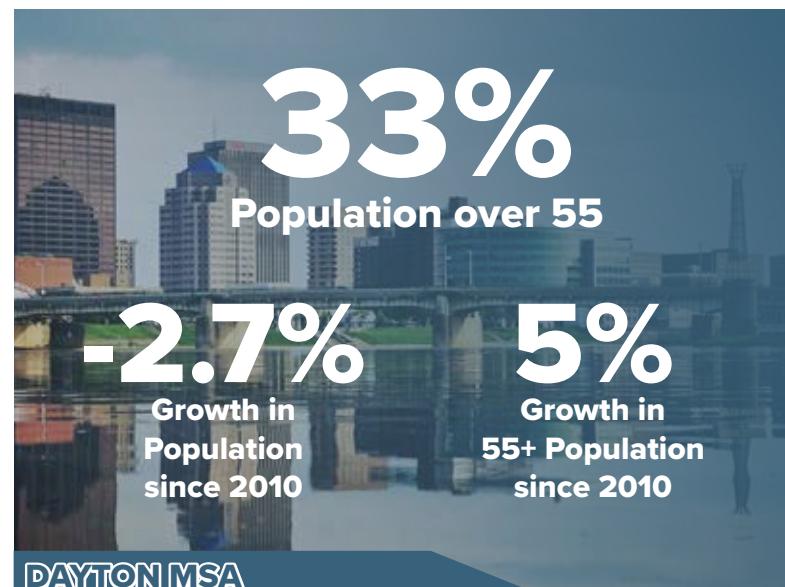


## REGIONAL CONTEXT

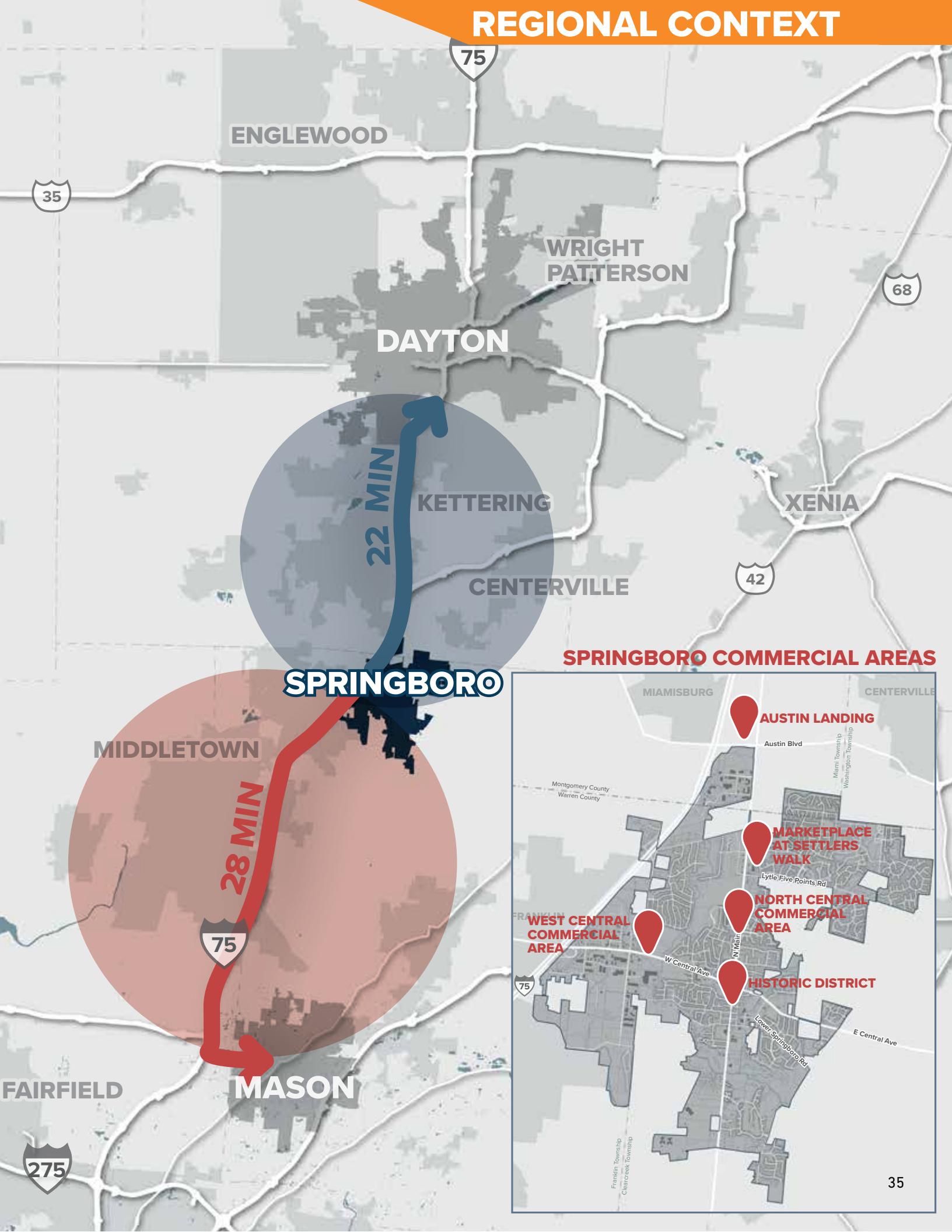
Springboro sits within a unique setting in Southwest Ohio. Located along the Interstate 75 corridor, between Cincinnati and Dayton, Springboro benefits from this unique setting. Springboro is within a 30 minute drive of downtown Dayton and the Cincinnati region. This provides employment, entertainment, and shopping options for Springboro residents. However, public engagement suggests that residents would like more options within the City so they don't have to travel so far for shopping, entertainment, and other unique experiences. The map on the opposite page shows Springboro's relationship to Dayton and the greater Cincinnati region.

Due to its proximity to two larger cities, we can utilize demographic trends of these larger regions to help inform our understanding of Springboro as well. One of the key points brought up through stakeholder and public engagement was the aging population in Springboro and the need to diversify housing options for different lifestyles of residents. This is backed up by the data we see in the Southwest Ohio region. Between the Cincinnati and Dayton Metropolitan Statistical Areas (Springboro is technically in both as the Warren-Montgomery County boundary is the divide) there is at least a 5% growth of population 55 and older despite overall population growth trends. Cincinnati population is growing at 4.2% since 2010, while Dayton is shrinking at 2.7% overall since 2010. Despite this fact, both MSAs have similar growth rates in 55-plus population which suggests there is certainly a need for housing that accommodates older

populations. This includes more than senior living facilities, it could include smaller downsizing opportunities, townhomes, condos, and apartments.



# REGIONAL CONTEXT



## DEMOGRAPHICS

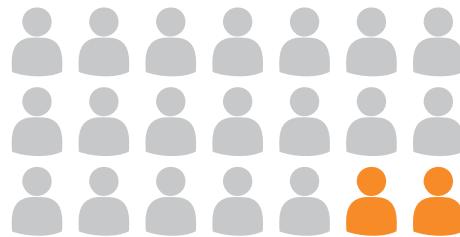
Part of the analysis for the master plan involves examining the current population of Springboro from data sources like the US Census and the American Community survey. These pieces of data can help us to identify trends and key commonalities amongst residents that may guide future recommendations of this Plan.

Getting close to 20,000 people, an annual growth rate of 0.95%, and an increasing aging population, Springboro is home to a highly educated population, reflected on the above national average median household income and high paying jobs.

Some of the key takeaways from the demographic analysis can be found below.



**53.8%**  
of 25+ population  
have a college  
degree



**19,062**  
total population

**9.4** PERCENT  
GROWTH  
since 2010

50+ population  
has increased  
**OVER 30%**  
since 2010

median household  
income

**\$107K**

## top occupations



**Management**



**Healthcare**



**Education**



**Office**

# HOUSING

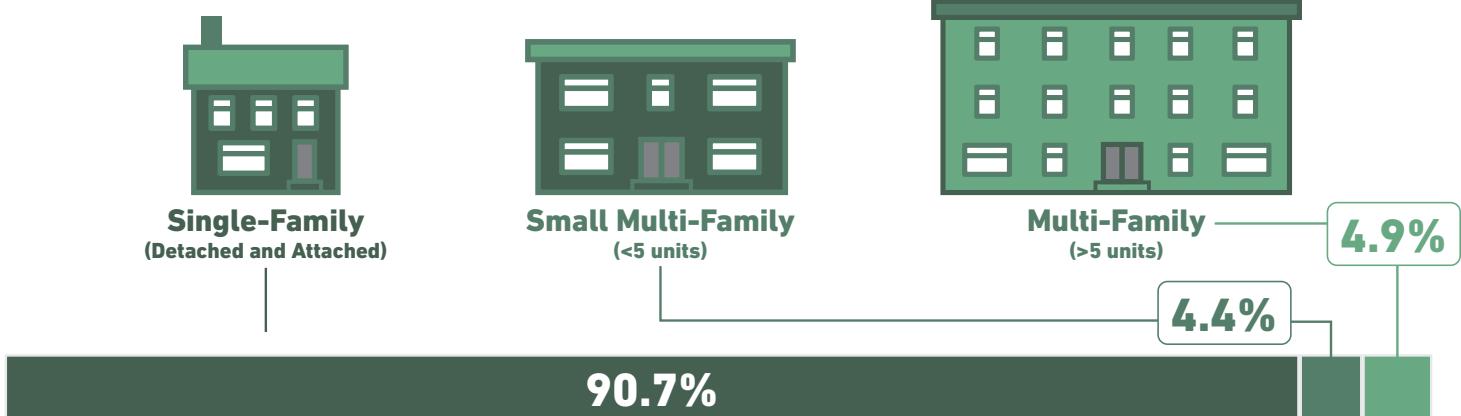
Housing is another key component of this plan and so our planning team was certain to also look at data related to the existing housing stock within the city. Throughout the engagement process, housing was a key subject. Opportunities for new housing, what types of new housing could come to the city, affordability, and size were all key issues that were brought up. The data presented below highlights a few of those key data points.

First, it is important to note that single family housing makes up over 90% of all housing units within the city. This is significant, especially, considering the age-in-place goals of the community. Larger single family housing does not always accommodate different living lifestyles or all stages of one's

life. This includes both younger and older populations who may not desire or need a large house or property that requires more time, resources to maintain and purchase. Small multi-family (less than 5 units per building) and multi-family (more than 5 units per building) each make up less than 5% of the total housing stock currently available within Springboro. In order to become more friendly for age-in-place lifestyles, there needs to be an increase in different types of housing that better accommodate households at various points in their lives.

The median home value in Springboro was \$229,771 at the time of this research. Only 16.9% of homes were valued at less than \$200,000 which presents a potential for an affordability issues for those looking for starter home or downsizing to a smaller home.

## housing types



median home value

**\$227,600**

16.9% of homes valued  
at < \$200,000

**2.85** average  
household size

# INFRASTRUCTURE

Infrastructure is a critical component to the current and future livelihood of the city. Infrastructure connects the city and provides residents and businesses with necessary basic services. This plan recognizes the critical impacts of planning for future infrastructure improvements as a means for making the city a more competitive place to attract jobs, businesses, and residents.

Infrastructure includes a variety of components, whose current and future conditions are described below.

## Transportation

The City has completed a thoroughfare plan in 2001 and has continued to work with ODOT, Clearcreek Township, and Warren County to maintain and improve roads within the city. Roads within the city are currently operating under their maximum capacity, which suggests impact from any form of new development would not necessarily require expansion of roadways.

Transportation goes beyond just roads for automobiles and also includes sidewalks and bicycle facilities. Not all residents, employees, and visitors of the city will drive and it is important to ensure there is a network of transportation options that allow for multiple modes of transportation.

The City recently completed an update to the Bike and Pedestrian plan which identifies future improvements to the active transportation networks within the city.

## Electric and Water

The entire city is adequately serviced with regards to access to the electric grid and water service.

## Wastewater

The city's current stormwater and wastewater treatment facilities are operating below capacity and similar to the transportation network have additional capacity to accommodate future development.

## Fiber Optic / Broadband

In 2020, the City completed a Fiber Master Plan to identify the potential for expanding and improving the broadband network within the city. Broadband is essential for employers, education, and residents alike.

## Infrastructure & Development

Mentioned multiple times in this section is the capacity for new development to be served by infrastructure within the City. However, it is important to keep in mind that all new development will be subject to examination of its unique impacts on the various infrastructure of the city.



WARPED WING

30 Years Young

1990-2020







## **SECTION 5**

# **PLAN RECOMMENDATIONS**



# Plan Recommendations

This section covers the recommendations of this Master Plan. The plan's recommendations focus on Future Land Uses, development and character, Connectivity and Transportation, and Placemaking and Identity. Section 6, following this section, covers an implementation strategy for these plan recommendations. These plan recommendations will guide policy makers and city staff regarding future land use, zoning, and development proposals, as well as guide them on pursuing funding sources for implementation.

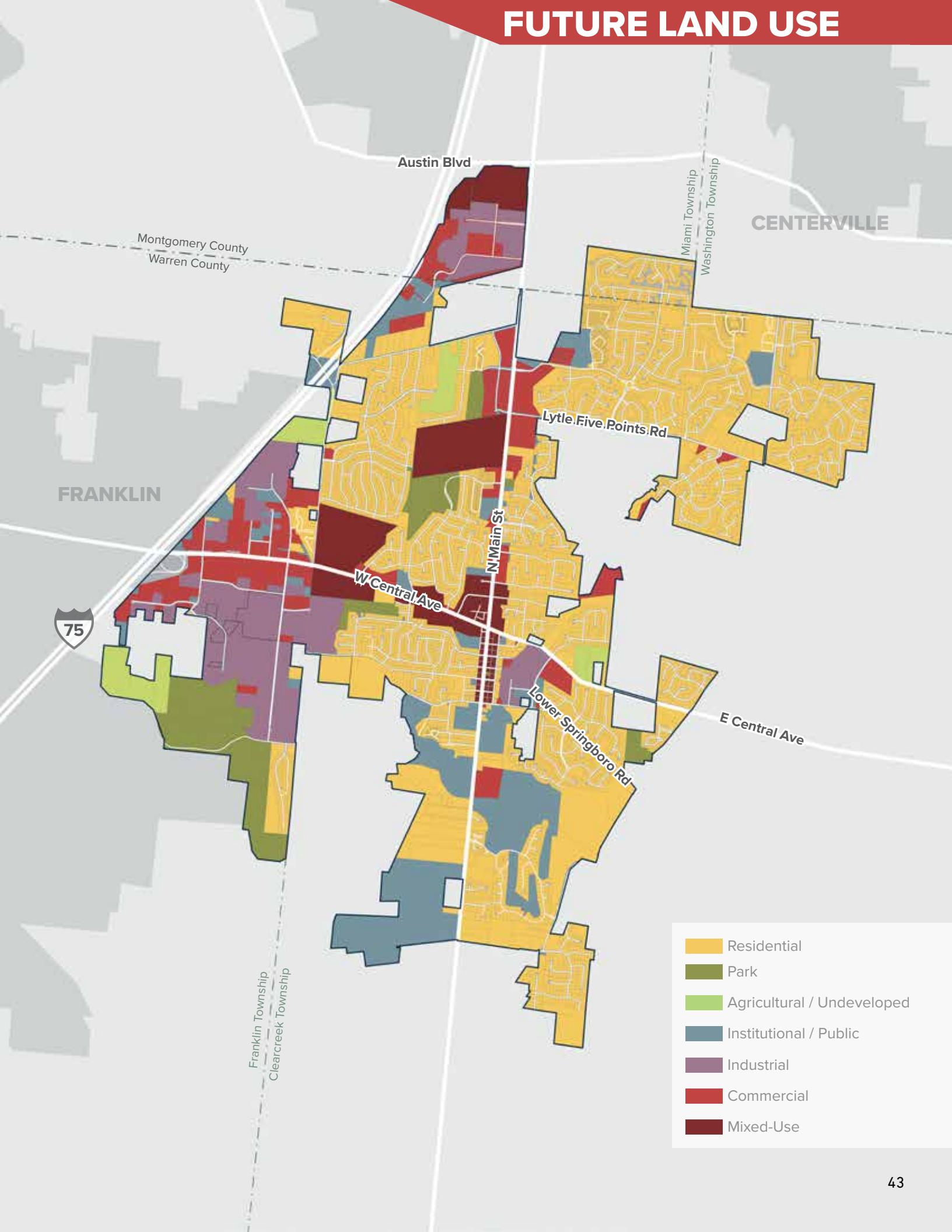
## A. Future Land Use

On the opposite page is the future land use map for Springboro. This map shows the intended future land use set out by parcel for the whole city. The map uses the same classification system as the Current Land Use Map in Section 4 with the addition of a Mixed-Use Development category (Dark Red). This classification can best be described as encouraging a mixture of uses on that parcel in the future. This mixture of uses may occur within a single structure or, if the site is large enough, across different buildings on the same parcel. Most of the city retains its existing land uses in the Future Land Use map with a few exceptions. The primary areas that change land uses include some notable potential development sites within the city as well as some areas that will now allow for more of a mix of uses between residential and commercial.

Further explanation of future land uses and the intended character for development within the city can be found on the following pages.

Future land use changes will need to be guided and coordinated by an updated zoning map and code that encourages the preferred uses and character.

# FUTURE LAND USE



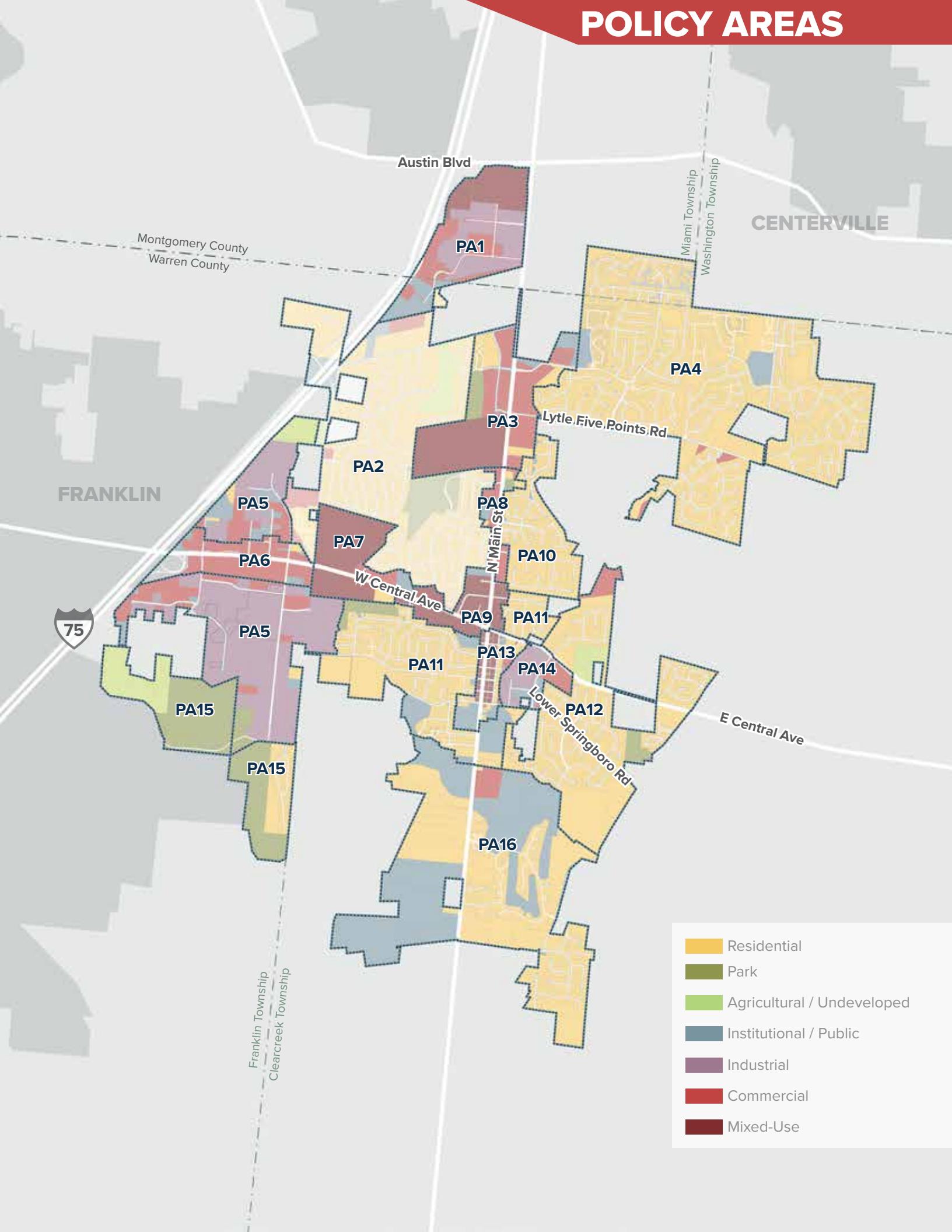
## B. Policy Areas

The following pages provide a more detailed examination of the current conditions and intended vision for the Future of Springboro. The plan breaks down the future land use map of the city into **16 Policy Areas**. These policy areas are classified based on location, character and the uses of a given area within the city. For each policy area, there is an overview of existing conditions

and future vision, a list of Preferred Future Characteristics, Actions and Needs, an Implementation Recommendation, and relevant benchmark imagery for future conditions.

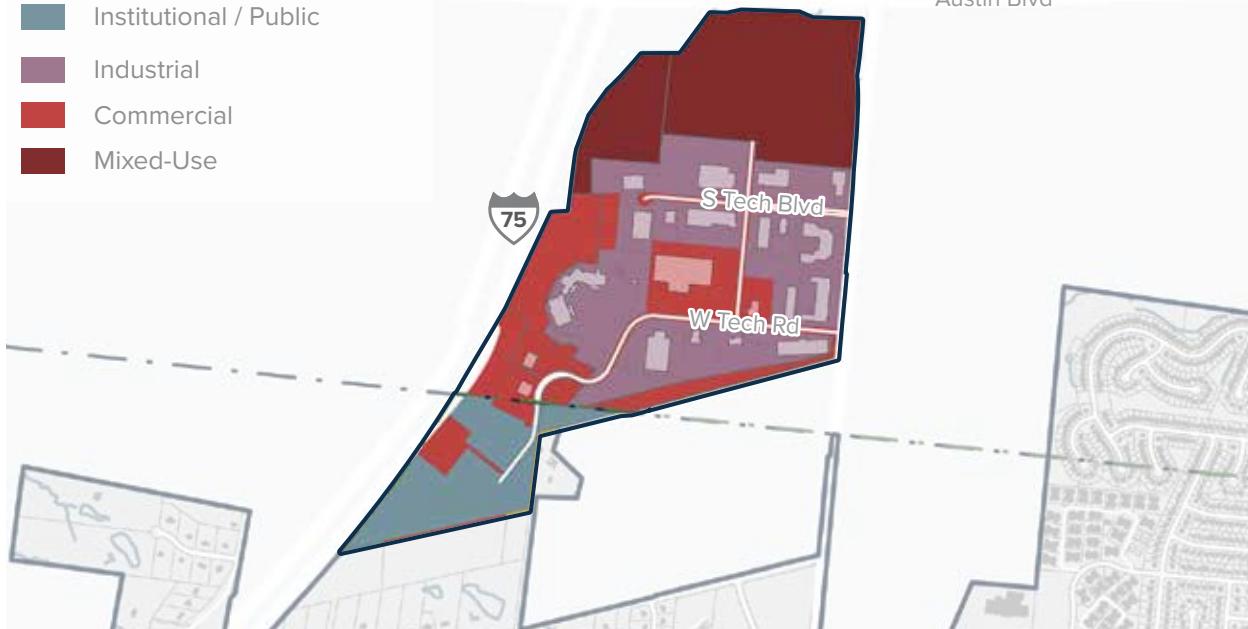


# POLICY AREAS



**FUTURE LAND USE**

- Institutional / Public
- Industrial
- Commercial
- Mixed-Use



## POLICY AREA 1

### SOUTH TECH BUSINESS PARK

South Tech Business Park Policy Area is located at the northern end limits of the city's boundary. With immediate access to I-75 provided by the Austin Boulevard Interchange completed 10 years ago, this area has become a promising commercial and light industrial business park for the city. Recent developments in this area including Dayton Children's South Campus expansion, and The Ascent, have made this a prominent employment hub for the city. As a result, future uses should be commercial or light industrial with a focus on employment opportunities as this is the most compatible uses for the area and broadens the City tax base. Undeveloped land fronting Austin Boulevard along with ready-to-build parcels within the business park offer opportunities for new future development. Reuse of existing buildings and already developed parcels should be promoted where possible.

A summary of the existing character and issues regarding the South Tech Business Park Policy Area includes the following:

- » The current development pattern is a mix of light industrial, office, recreation and a small portion of retail.
- » Significant areas of land are available for development including land with I-75 frontage.
- » Development of this area will be based on strict design guidelines as a part of the adopted South Tech Business Park Land Use Plan and the multi-jurisdictional Austin Road Development Plan.
- » The area is characterized as new and modern with respect to development.
- » The area is visible from the Austin Road/I-75 Interchange.
- » The area is directly west of the Dayton Wright Brothers Airport, an active general aviation airport.
- » The entire Policy Area is served by the Montgomery County Sanitary Engineering Department for water and sanitary sewer.

## Preferred Future Characteristics, Actions and Needs

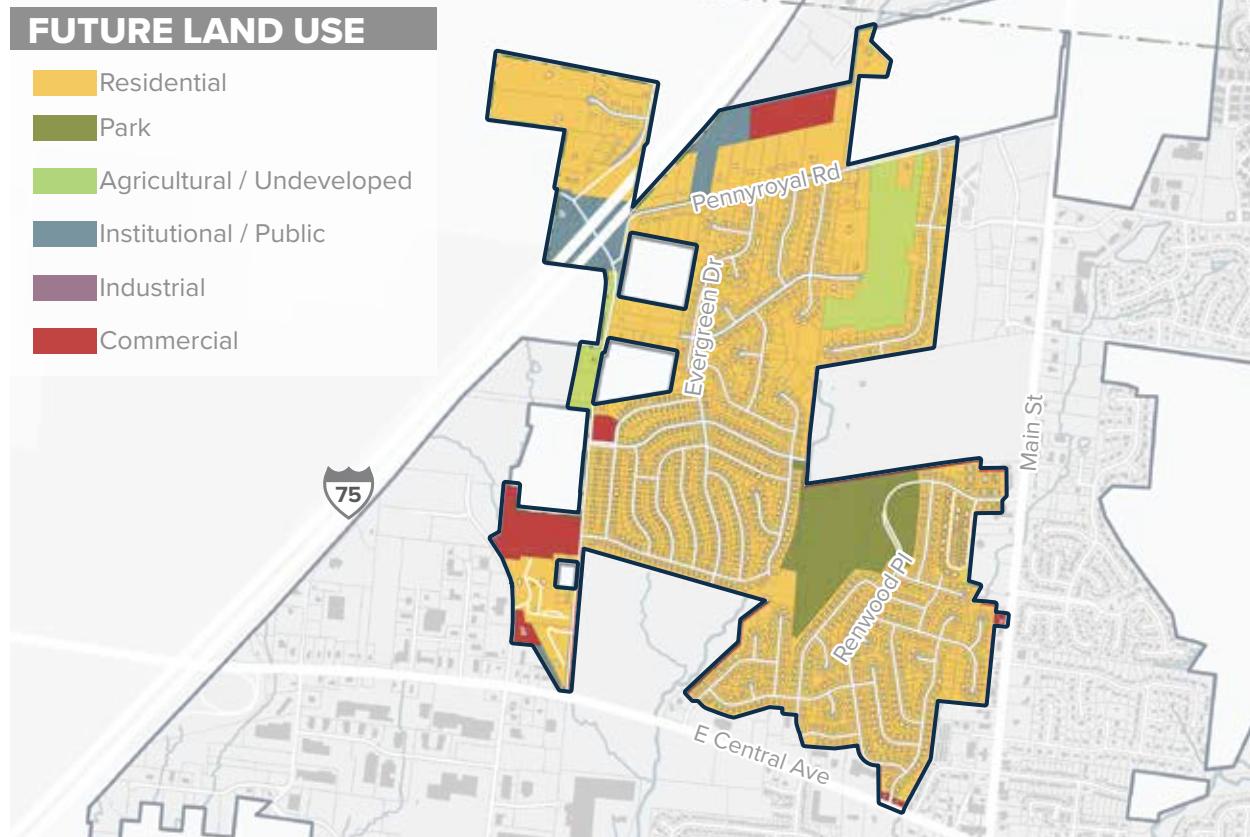
- » There shall be an emphasis on providing a mix of light manufacturing uses with supporting office and retail uses.
- » Green spaces and landscaping shall be incorporated into all new development and redevelopment projects, including streetscapes, to reduce impervious surfaces, reduce stormwater run off and soften the visual quality of development.
- » All new development and redevelopment efforts shall promote external and internal connectivity through the installation of sidewalks, walking trails, roadway systems and bicycle trails.
- » Proposals for new development and redeveloped properties shall consider the impacts of traffic circulation and incorporate access management techniques including, but not limited to: consolidation of curb cuts, shared parking, frontage roads and parking lot interconnect.
- » Visually pleasing gateway/entrance should continue to be developed for all major entrances into this Policy Area. Such gateways should emphasize the entrance into the City of Springboro and could contain design elements such as landscape beds, signage, and special lighting or a larger element such as a small park.
- » The City should continue to be an active participant in the promotion of coordinating design elements, both on public and private properties, with surrounding jurisdictions. In particular, coordination should occur between the

South Tech Business Park Policy Area and the Austin Road Interchange Area to promote a cohesive design atmosphere.

## Implementation Recommendations

Development and redevelopment activities, including land uses, shall be specifically pursuant to the South Tech Business Park proposed Land Use Plan and the current zoning requirements previously adopted by the City of Springboro City Council and Planning Commission.





## POLICY AREA 2

### NW RESIDENTIAL NEIGHBORHOODS

Anchored by North Park, this policy area primarily consists of numerous residential neighborhoods and a few isolated commercial and institutional/public parcels. The residential neighborhoods in this policy area include the Royal Springs, Royal Springs Meadows, Tamarack, Royal Tamarack, Springwood, Springbrook, and the Deer Trail/Hunter Springs subdivisions. These subdivisions are characterized by a mix of single-family detached residential dwelling styles (ranch, tri-level and two-story). In the future, the current land uses of this policy area remain as the intended future use. The residential neighborhoods are built out and will continue to remain as single-family

neighborhoods in the future. It is worth noting the proximity of this policy area to two prominent future development sites in the Easton Farm and Eastbrook farm.

A summary of the existing character and issues relating to the Northwest Residential Neighborhoods Policy Area is as follows:

- » The Policy Area is predominately built out within the City jurisdictional boundary. However, a few larger lot Township parcels remain available for development.
- » The area is comprised almost entirely of detached, single-family residential dwellings and City parks: North Park and Foliage Lane Park. Most of the residential dwellings in this Policy Area range between twenty and fifty years in age.
- » The Policy Area has a diversity of housing styles including ranch, tri-level and two-story single-family dwellings.

- » Many of the homes in this Policy Area are considered to be moderately priced by residents and real estate agents. Most of the subdivisions in this Policy Area are adequately interconnected—with sidewalks, paths and roadways. One exception is the connection between the Tamarack neighborhood with the Springwood and Springbrook subdivisions that are not physically connected by roadway or sidewalk. The future implementation of the Bicycle and Pedestrian Plan, and the Central Greenway trail will help to make a trail connection between these two areas.
- » There are some multifamily residential properties as well as the Enclave senior living facility located along Clearcreek Franklin Road.

## Preferred Future Characteristics, Actions and Needs

- » The City should require conformance to the current physical character of housing stock as infill redevelopment or new development occurs on available and annexed parcels. For example, in a neighborhood of ranch style homes, newly developed or redeveloped properties, especially infill housing, should not be two story homes. In cases where redevelopment or development is not an infill development (three or more contiguous parcels), it may be acceptable to establish a different physical housing type.
- » There is potential to provide reasonably priced housing options for newer families as the neighborhoods in this Policy Area age with the current housing stock available in the Policy Area. Discussion

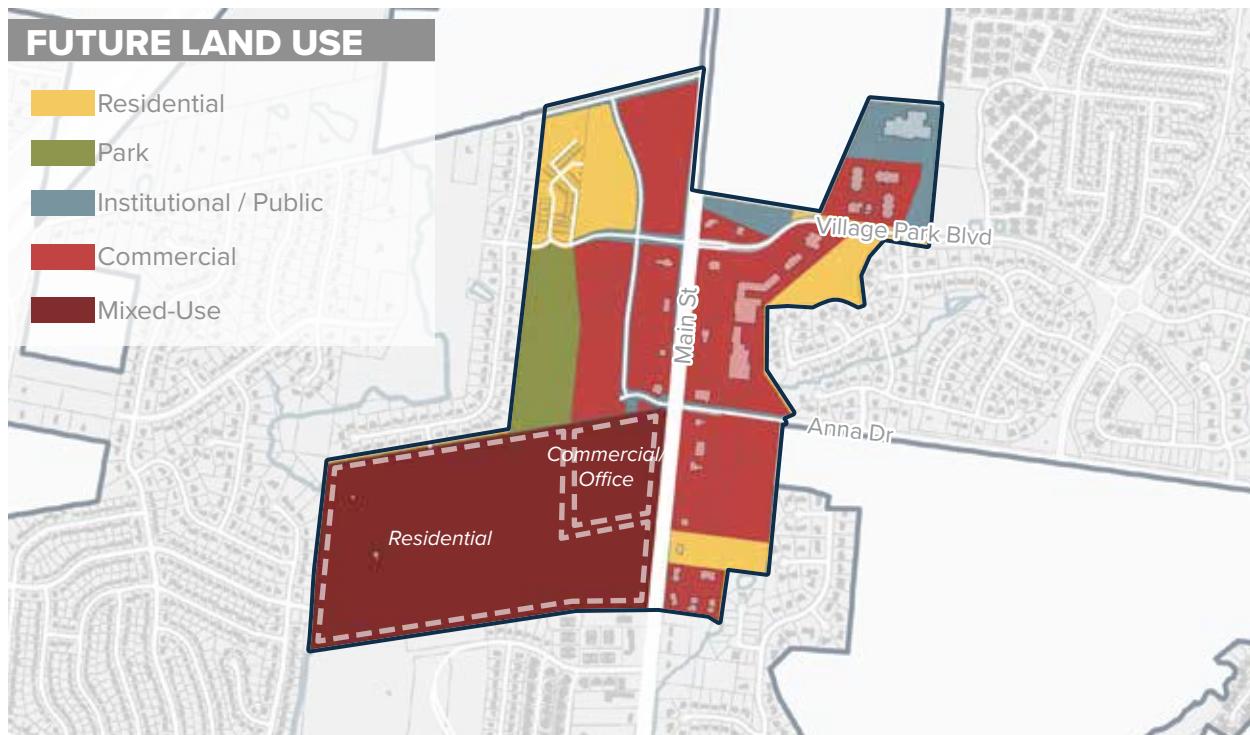
with residential developers and the real estate community as to the marketability of new dwelling types in this area should occur as parcels become available for development and redevelopment to vary the price ranges available to the buyer.

- » A priority for the City should be the development of a physical connection—walking trail, path or sidewalk—that links North Park in Policy Area 2 and Gardner Park in Policy Area 3.

## Implementation Recommendations

Emphasis by the City should be placed on the enforced maintenance of property in this Policy Area. Development and redevelopment activities should be limited to residential uses that complement the existing residential uses with respect to housing type and size, materials used and lot coverage.





## POLICY AREA 3

### NORTH S.R. 741 CORRIDOR

Located between Tamarack Trail and Pennyroyal Road, the North S.R. 741 Corridor Policy Area lies at the northern gateway into the City of Springboro. This area contains a mixture of residential and commercial uses including the Marketplace at Settlers Walk at the northeastern corner of the intersection of S.R. 741 and Lytle-Five Points Road. This Policy Area also includes the largest undeveloped parcel of land in the City, the Easton Farm, which is approximately 103 acres in size. The Easton Farm is designated as "Mixed-Use" future land use, indicating that the future development of the site would include both residential and commercial uses. The specific residential typologies and commercial uses should be in alignment with the vision set out in this plan as a result of community and stakeholder input.

A summary of the existing character and issues relating to the North S.R. 741 Corridor Policy Area is as follows:

- » The Policy Area currently includes a large amount of developable land including the Easton Farm. This parcel is directly accessible from S.R. 741, Anna Drive, Fox Trail Drive and Tamarack Trail.
- » The Policy Area can be best described as a mixed-use area including retail, office, City parks and the Coffman Family YMCA. This development is primarily contained along S.R. 741 between Pennyroyal Road and the northern border of Policy Area 8.
- » This area contains all necessary infrastructures for existing and future development needs.
- » Newer existing developments have been well designed and are primarily conventional in nature with parking lots in the front of the building, at the street.
- » The Policy Area is surrounded by

- residential subdivisions to the east (Settlers Walk) and to the southeast (Woodland Greens, McCray Farms).
- » This Policy Area is bisected by S.R. 741 that affords easy access to all parcels. It is the primary north-south transportation corridor in the community.

## Preferred Future Characteristics, Actions and Needs

- » The City prefers the continuation of the mixed-use development pattern as new development and redevelopment occurs on the Easton Farm property, with non-residential uses fronting on S.R. 741. Development pattern may occur in a similar layout to what is depicted on the diagram on page 50.
- » The density in any portion of a development should generally reflect that of the parcels abutting that portion of the development site.
- » Promote the walkability in the Policy Area through the continued implementation and use of paths and sidewalks to link uses that front along S.R. 741 to those parcels of land that do not front on S.R. 741.
- » Continue emphasis on high quality design features both on and off site. This can be accomplished through the application and enforcement of design guidelines in an overlay district or general design guidelines applicable to all retail areas of the City.
- » An emphasis on high quality streetscapes and greenspace that are well maintained is preferred.
- » The northern boundary of this Policy Area along S.R. 741 could include gateway

enhancements to welcome travelers into the city.

- » Provide a physical connection between the Village Park Planned Unit Development in this Policy Area and North Park in Policy Area 2 by a dedicated pedestrian/bicycle trail and roadway connections in the vicinity of Anna Drive and Tamarack Trail.

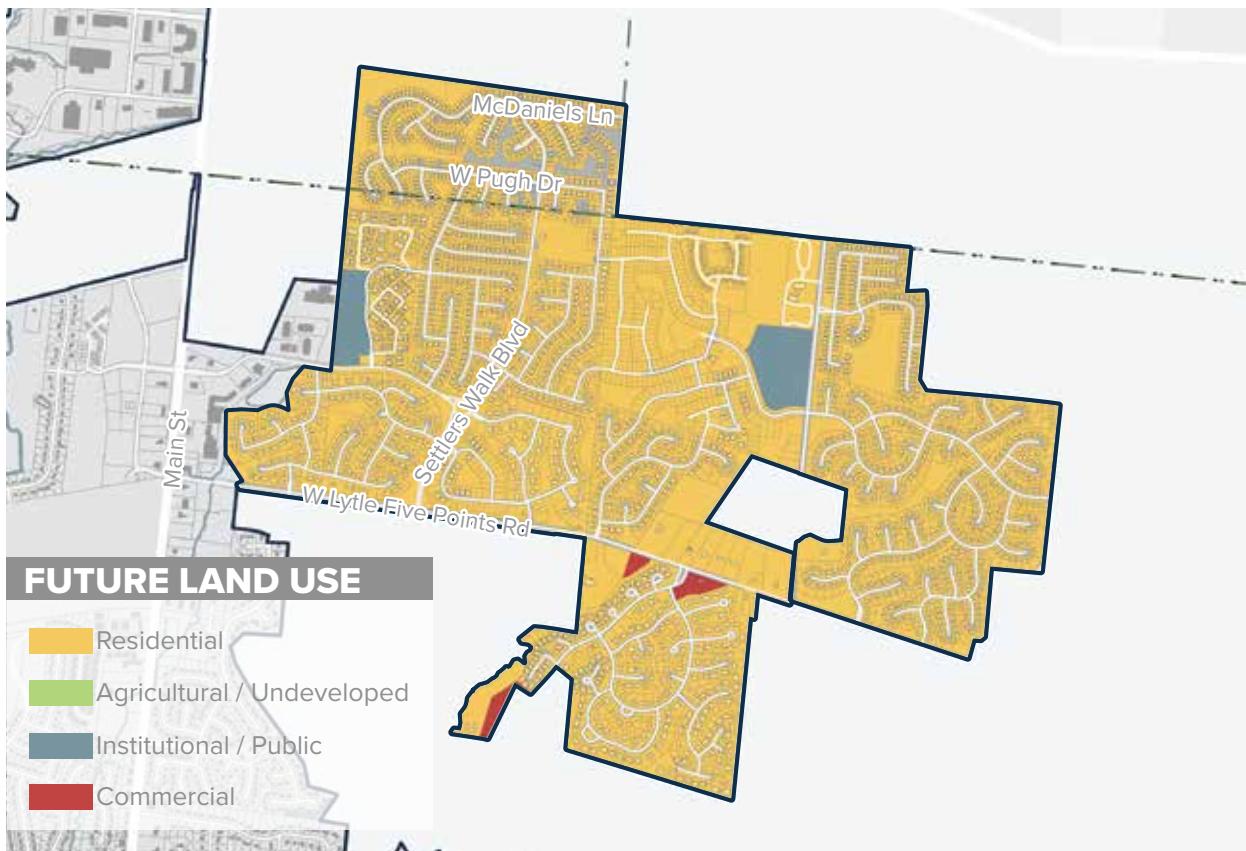
## Implementation Recommendations

Development and redevelopment activities in this Policy Area should emphasize mixed uses. Regulatory documents (zoning code, urban design regulations, streetscape plans, etc.) should be proactively reviewed by the City, prior to development requests, to ensure the promotion of a mixed-use environment, walkability and a visually pleasing atmosphere.

The Easton Farm is the largest single parcel development site in the City. Its development is a strategic move for the City and community as large development sites are rare within the City. It is crucial that development plans are reflective of the community's vision set out in this plan and generally reflect the surrounding neighborhoods.



**DOROTHY LANE MARKET IN P.A. 3**



## POLICY AREA 4

### NE RESIDENTIAL NEIGHBORHOODS

Located in the northeastern corner of the City, the Northeast Residential Neighborhoods Policy Area contains a large number of the detached and attached single-family residential dwellings in Springboro. Conveniently accessed by S.R. 741, Lytle-Five Points and Yankee Roads, this Policy Area contains the neighborhoods of Settlers Walk, The Springs, Laurel Glen, Brookside, Greenleaf and Sycamore Springs. The Policy Area is served by small private community parks and recreation areas, integrated into many of the subdivisions, and by retail and personal service uses to the west on S.R. 741 in Policy Area 3. The Policy Area is almost completely bordered on all sides by Clearcreek Township and Miami Township, Montgomery County.

A summary of the existing character and issues relating to the Northeast Residential Neighborhood Policy Area is as follows:

- » The Policy Area includes portions of the Settlers Walk neighborhood and all of the following neighborhoods: The Springs, Sycamore Springs, Brookside, Greenleaf and Laurel Glen.
- » There remains only a few number of developable lots in this Policy Area. Construction on existing neighborhoods in The Springs is nearing completion.
- » The Policy Area contains many of the newer residential dwellings in the City with most residential dwellings averaging less than 20 years in age.
- » Varying housing types exist in this Policy Area, including single-family detached dwellings, condominiums and multi-family dwellings.

- » Many of the neighborhoods in this Policy Area include private amenities such as neighborhood pools and small pocket parks.
- » Kacie Jane Park is located on Yankee Road, technically located outside of the city boundary in Clearcreek Township, and serves many of these neighborhoods.
- » Some neighborhoods in the Policy Area are physically divided from one another by arterial roads, which physically separate individual neighborhoods. For example, the Laurel Glen and Brookside neighborhoods are physically separated from The Springs, Sycamore Springs and Settlers Walk by Yankee Road.
- » The Bicycle and Pedestrian Plan proposes a series of trail connections, including the Central Greenway Trail, through this Policy Area to better connect this area to other parts of the City for non-auto oriented transportation.
- » When development and redevelopment are proposed, the City should emphasize infill that exhibits similar characteristics to the neighborhood in which the infill is occurring. This can be done through the strict application of the Planned Unit Development regulations, where applicable, or by proactively developing design guidelines that mirror the existing building design elements.

## Implementation Recommendations

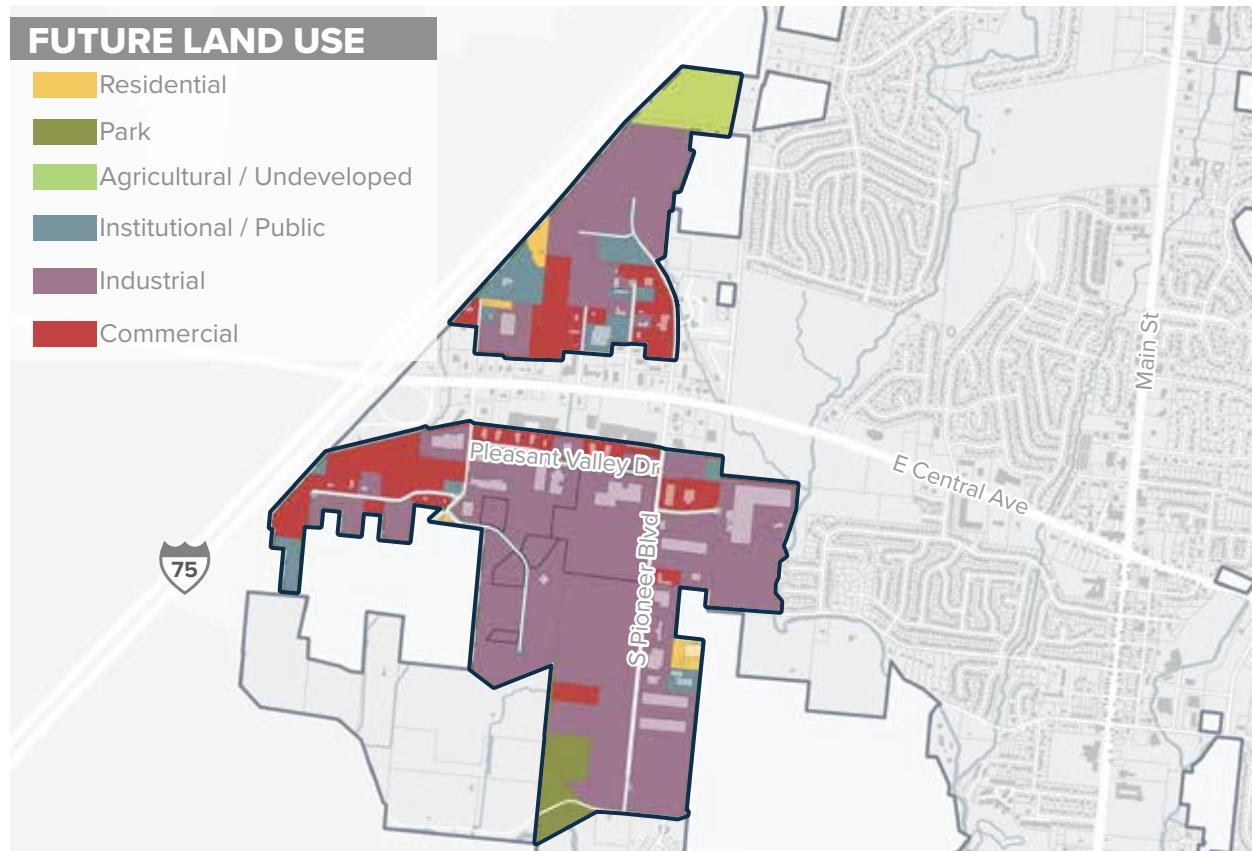
Development and redevelopment activities in this Policy Area should emphasize residential uses that conform to the existing character of the neighborhood in which the development or redevelopment is located. Similar building materials, building massing and orientation



**EXISTING SINGLE FAMILY HOME**



**THE FALLS AT SETTLER'S WALK**



## POLICY AREA 5

### SR 73/I-75 BUSINESS PARKS

Strategically located on the western fringes of the City, the S.R. 73/I-75 Business Park Policy Area is conveniently accessed by S.R. 73 and I-75. Physically divided by S.R. 73 and Policy Area 6 (I-75 Gateway), this Policy Area is the location for a variety of small light industrial and service related uses that serve Springboro, the surrounding region and a national market. This well-established area provides a good source of income tax generation for the City and includes the business parks of North Pioneer, South Pioneer, Commercial Way and Advance Drive/ Stoltz Industrial Park.

A summary of the existing character and issues relating to the S.R. 73/I-75 Business Park Policy Area is as follows:

#### Overall

- » The Policy Area is physically divided into two separate and distinct nodes north and south of S.R. 73. This area is physically divided by Policy Area 6, the I-75 Gateway.
- » The area contains a mix of light industrial, manufacturing and office uses.
- » Because the areas do not directly front on S.R. 73, the Policy Areas are not readily visible from the City's major east-west arterial corridor, S.R. 73, making it appropriate for non-retail development.
- » This area functions as a destination providing needed goods and services for the City and region.
- » A majority of the businesses in this Policy

Area, in particular those in the area south of S.R. 73, is a good source of income tax generation.

- » Land is available for development in both the northern and southern portions of this Policy Area.

### **Northern Portion of the Policy Area (North of S.R. 73)**

- » The northern portion of the Policy Area includes North Pioneer Drive, Tamarack Trail and Tahlequah Drive.
- » Poor aesthetics and road conditions plague the northern portion of the Policy Area.
- » There exists a mix of structures in the northern portion of the Policy Area; there are no cohesive design elements relating to form, color, building materials, size of structures or layout.
- » The northern portion of this Policy Area is characterized by poor access, including a lack of through streets. This issue is predominately due to the fact that the internal roadway system consists of private streets. Ultimately the lack of secondary access presents a potential safety issue for businesses located towards the end of North Pioneer Drive, which is almost a half-mile in length, should emergency vehicles need to gain access and street is blocked.

### **Southern Portion of the Policy Area (South of S.R. 73)**

- » The southern portion of the Policy Area includes South Pioneer Drive, Advanced Drive, Commercial Way and Pleasant Valley Drive.
- » There are few lots available for development in the southern portion of this Policy Area.

- » The southern portion of the Policy Area is characterized by a more visually pleasing aesthetic design and larger-scale light industrial development when compared to northern area.
- » Properties in the southern portion of the Policy Area are accessed by a visually pleasing boulevard that is well maintained.

## **Preferred Future Characteristics, Actions and Needs**

- » The City should focus efforts on incorporating internal access management techniques for the entire policy area.
- » The City should proactively focus on consolidating lots for redevelopment efforts. This will assist in attracting larger users to the City or providing additional space for those businesses that currently exist and are looking to expand.
- » The City should maintain the type and mix of uses as redevelopment and development efforts occur in this Policy Area. This includes light industrial, service and office type uses.
- » Focus on continued infrastructure maintenance.
- » In order to provide a higher quality visual environment, the City should require facade improvements where buildings are visible from public rights-of-way. This can be done through the creation and implementation of design guidelines as well as the active enforcement of property maintenance standards.
- » The City should focus efforts on the redevelopment of the Policy Area to implement better design standards.

Design standards should focus on the implementation of architectural features, quality site design, lighting, signage, and landscaping. Design standards can be implemented through the establishment of an overlay district or the application of a PUD to assist in this endeavor.

## Implementation Recommendations

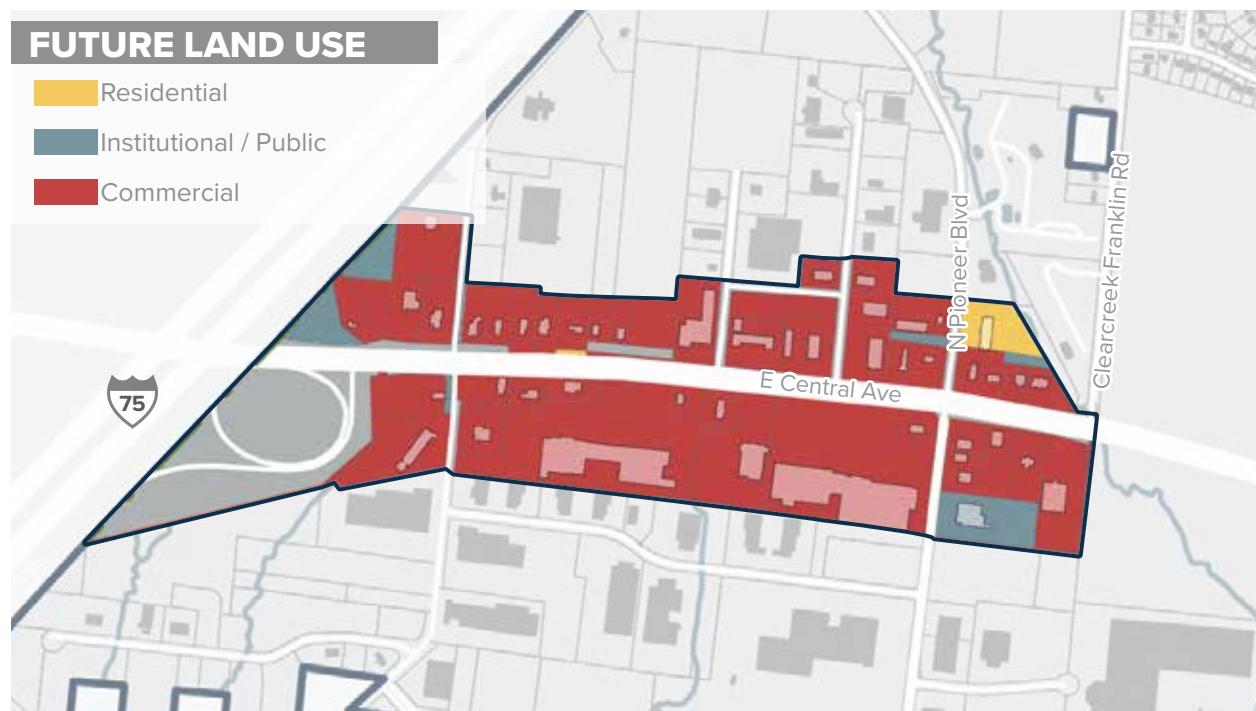
Development and redevelopment activities in this Policy Area should emphasize mixed uses that are cohesive in design elements and situated to promote access management. The proactive consolidation of smaller lots, as they become available, is essential to attracting larger businesses in this area or providing an expansion alternative to those businesses already in Springboro.



### IMPROVED AESTHETICS



### GATEWAY LANDSCAPING



## POLICY AREA 6

### I-75 GATEWAY

Visible from I-75, the I-75 Gateway Policy Area is the western retail entrance of the City. Currently, highway-oriented business uses, gas stations, shopping centers, restaurants and entertainment venues line West Central Avenue (S.R. 73) that serves as the major east/west arterial through the City. Portions of this Policy Area contain light industrial uses at the intersection of North Pioneer Boulevard and S.R. 73. The City has made some recent improvements in this area with the installation of wayfinding signage and the installation of a boulevard on S.R. 73 near the Pioneer Boulevard intersection. Future improvements to the streetscape and public realm aesthetics will be part of developing a visual gateway character for this area. For many, this is the first area in Springboro they see when traveling to the community and the visual character needs to be enhanced to better reflect the community.

A summary of the existing character and issues relating to the I-75 Gateway Policy Area is as follows:

- » This Policy Area contains all of the road frontage property along S.R. 73 between I-75 to Clearcreek-Franklin Road.
- » Because of its immediate presence along S.R. 73, this Policy Area functions as the major gateway into the City from the west with extremely high visibility.
- » Recent private investment in the area, including the new Kroger store in the former Kmart space, indicates that the market for services in this area is strong.
- » The Policy Area is characterized by strip mall and out lot development, fast food restaurants, gas stations, entertainment venues and small-scale office development. Because development has occurred over many decades and on varying sized lots, the area is inundated with multiple curb cuts.
- » The Policy Area includes the Midway

Shopping Center, an older plaza-style retail establishment with a variety of retail uses and non-retail uses as well.

- » This area is currently a healthy retail environment, but surrounding regional competition and new retail development being constructed outside of the City limits is causing increasing retail rivalry.
- » The western portion of the Policy Area is not aesthetically pleasing due to various highly visible vacant lots and buildings and the lack of cohesive design regulations. Both private property and the public right-of-way suffer from the lack of design regulation. As a result, the area contains a large number of utilitarian and moderately deteriorated buildings.
- » This area is exhibiting increasing traffic volume issues, truck and traffic volumes are noticeably higher at peak hours.

## Preferred Future Characteristics, Actions and Needs

- » As a major gateway into the City, the City should make it a high priority to create a visually pleasing streetscape and design standards for public and private property through the development of design guidelines and the application of a zoning corridor overlay district. Furthermore, the streetscape and boulevard improvements currently established to the east on S.R. 73 should be extended into this area.
- » The City should encourage the redevelopment of the Midway Shopping Center property into a cohesive development with focus on design elements including architectural features and landscaping. The City can take a proactive approach by rezoning this

property as a Planned Unit Development and establish a conceptual plan and design elements desired by the City.

- » The City should continue to work with the Ohio Department of Transportation (ODOT) and the City of Franklin with improvements to the roadway network surrounding the I-75/SR 73 interchange area as modified in the I-75 Interchange Modification Study by ODOT. These improvements will create a safer and more efficient flow at the interchange.
- » Use zoning techniques that permit the City to review and adopt an overall, cohesive plan (e.g. Planned Unit Development or zoning corridor overlay district) for this area to include a mix of office parks, retail and entertainment uses. A focus on physical design, access management and mix of land uses is important to this effort.
- » Limit access on S.R. 73 as identified in the City S.R. 73 Access Management Plan, including consolidation of curb cuts, shared parking between uses and establishing internal connections between parking lots on adjacent properties.
- » Provide for pedestrian access and movement in this area through the establishment of sidewalks and bikeways along S.R. 73 and from neighborhoods located directly northeast and southeast of this Policy Area.

## Implementation Recommendations

Because this is an important western gateway into the City from I-75, development and redevelopment activities in this Policy Area should emphasize mixed uses that are cohesive in design elements and situated to promote access management. A balanced

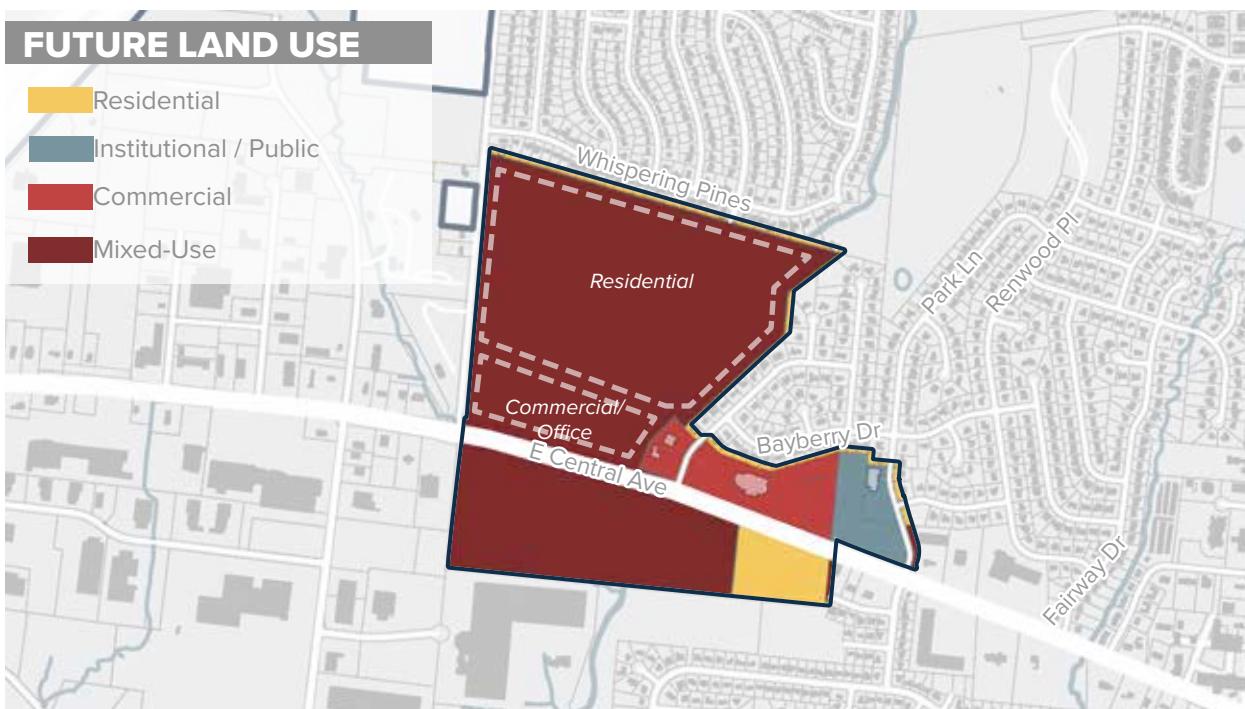
approach to improvements on private property and in the public right-of-way is necessary to promote a sense of place. Design of facilities geared to automobile access (drive through facilities gasoline pumps) should be located in such fashion that they are not readily visible from S.R. 73.



OUT-PARCEL DEVELOPMENT



STREET BANNERS AND LIGHTS



## POLICY AREA 7

### CITY SERVICES CENTER

Located just east of the I-75 Gateway Policy Area, the City Services Center is a transitional area between the highway-oriented development patterns that characterize the I-75 Gateway Area and the convenience retail focus of the SR 73/SR 741 Policy Areas at the S.R. 73/741 intersection. This area includes two large undeveloped parcels totaling approximately 118 acres, a number of small retail establishments and the site of the Springboro Municipal building and Public Library. The Eastbrook Farm site located at the northeast corner of S.R. 73 and Clearcreek Franklin Rd is another notable development site, like the Easton farm, that has been designated as mixed-use for future land use.

A summary of the existing character and

issues relating to the City Services Center Policy Area is as follows:

- » The Policy Area contains the properties along S.R. 73 between Clearcreek-Franklin Road east to Fairway Drive.
- » The area includes the largest portion of undeveloped property remaining on the S.R. 73 corridor in Springboro. This undeveloped property includes an 83-acre parcel, known as the Eastbrook Farm on the western side of the Policy Area, north of S.R. 73, as well as a 35-acre undeveloped parcel on the south side of S.R. 73.
- » There are landscaped medians on S.R. 73 that provide for a visually pleasing corridor.
- » The Policy Area has excellent interstate access.
- » Businesses in this Policy Area are relatively close to residential areas, particularly the neighborhoods in Policy Area 2 on the east end of the Policy Area.

- » This Policy Area includes three retail strip centers: Point Plaza, Drug Mart and Park Plaza.
- » There are gaps in the S.R. 73 sidewalk system, specifically on the south side of the thoroughfare.
- » The Policy Area includes health care facilities such as Surecare and Primed.

## Preferred Future Characteristics, Actions and Needs

- » The City should provide for better pedestrian access and movement in the establishment of sidewalks along S.R. 73. Likewise, better pedestrian linkages are needed from surrounding neighborhoods and lots to the S.R. 73 corridor as set out in the Bike and Pedestrian Plan
- » Retail redevelopment that can serve the needs of adjacent residential subdivisions should be provided through proper zoning and marketing efforts.
- » The City should encourage the connection of North Park to Community Park via a linkage along Twin Creek as part of the vision set out in the Bicycle and Pedestrian Plan.
- » The City should establish design guidelines for development and redevelopment efforts that focus on buildings oriented to S.R. 73 and building materials.
- » Require that new development and redevelopment efforts include mixed uses with housing. This can be accomplished through mixed use buildings or a mix of residential housing on individual lots.
- » A mixed-use development pattern may occur in a similar layout to what is

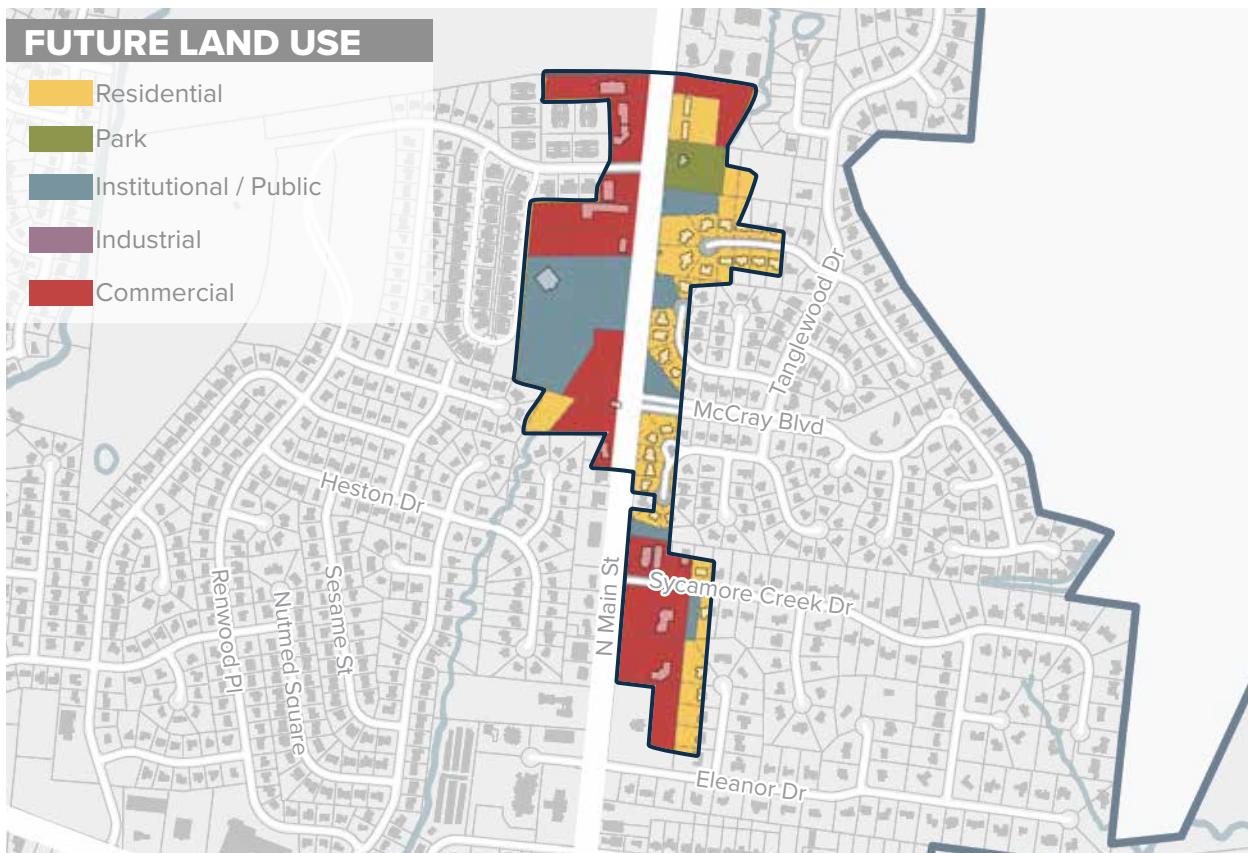
depicted on the diagram on page 60.

- » Maintain the natural buffer of the Twin Creek to buffer future Eastbrook Farm development from surrounding existing residential subdivisions.

## Implementation Recommendations

Development of the two large undeveloped parcels will play a critical role in defining the character of the S.R. 73 corridor. A mixed-use development pattern will provide the best opportunity to build the value of the S.R. 73 corridor while also buffering non-residential land uses from surrounding neighborhoods. Internal vehicular and pedestrian connectivity related to these developments will be essential, as will managing these properties' access to S.R. 73. High building and site design standards should characterize the properties, particularly those areas that are visible from the S.R. 73 right of way.





## POLICY AREA 8

### CENTRAL SR 741 CORRIDOR

Located just north of the SR 73/SR 741 Policy Area and immediately south of the North S.R. 741 Corridor Policy Area, the Central S.R. 741 Corridor is a transitional area characterized by extremely small, narrow parcels. While some of these parcels have undergone redevelopment, in some cases involving lot consolidation, other lots still include detached single-family dwellings or former residential buildings that have been converted to offices and other small retail uses. Improving the streetscape and visual character would aid in this transitional area feeling more connected with the adjacent Policy Areas.

A summary of the existing character and issues relating to the Central S.R. 741 Corridor Policy Area is as follows:

- » The Policy Area is nearly built out; a limited number of undeveloped parcels of land exist.
- » The area is bisected by S.R. 741, a major north/south thoroughfare extending from north of the SR 73/SR 741 Policy area at Eleanor Drive to south of Tamarack Trail.
- » The Policy Area is characterized by small office uses in converted, detached ranch dwelling structures on small lots (approximately  $\frac{1}{2}$  acre lots).
- » The Policy Area has a limited number of small retail uses.
- » Residential parcels within this Policy Area currently back onto S.R. 741 and provide no interaction with the major road.

- » Located along the east side of S.R. 741 is Mills Park.

## Preferred Future Characteristics, Actions and Needs

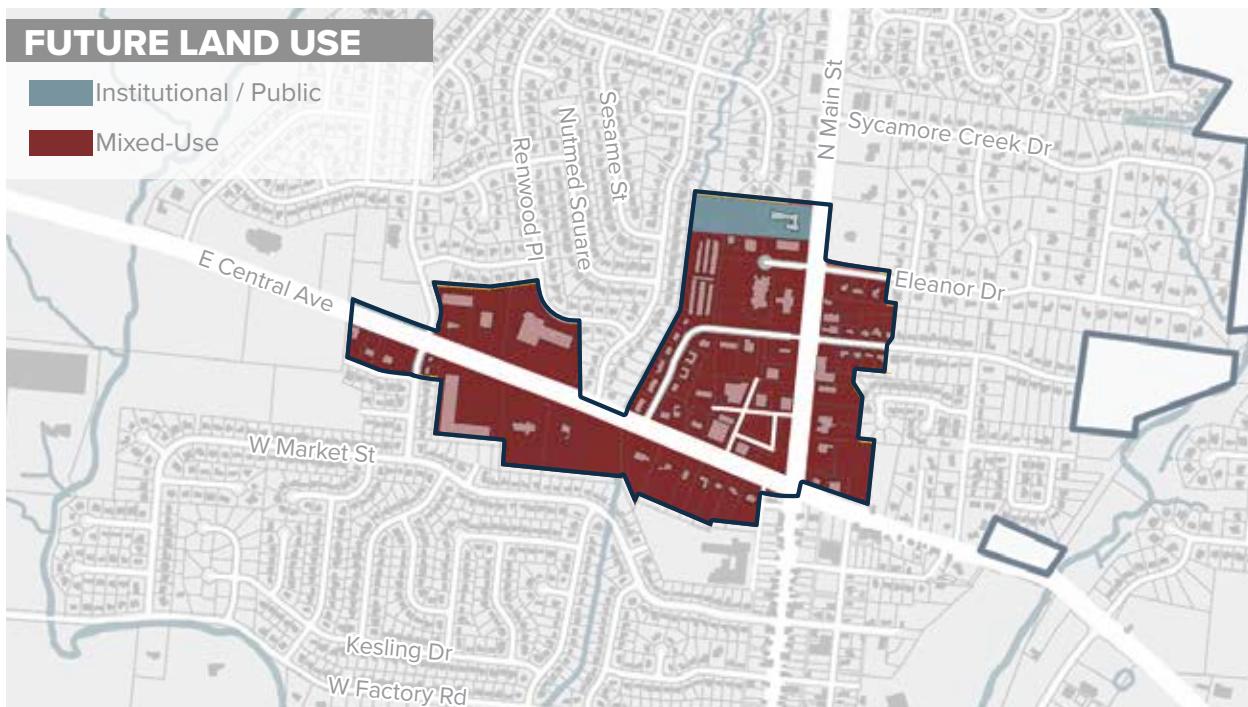
- » Allow lot consolidation for redevelopment to accommodate larger structures and uses that will promote curb cut reduction and improve site planning where the design, massing and scale of buildings are compatible with the surrounding built environment.
- » Permit conventional types of site design, but encourage architectural design that is compatible with and similar to Policy Area 3, to the north on S.R. 741, North S.R. 741 Corridor. Conventional design typically includes individual lots and buildings and parking lots placed between the building and street.
- » Promote convenience retail and service uses to support existing and future office uses on the north side of the policy area. A change in zoning districts may be necessary to accomplish this task.
- » Promote walkability in the area through Site Plan Review process and linkages to adjoining residential and non-residential areas.

## Implementation Recommendations

Lot consolidation will play a key role in facilitating the redevelopment and long-term viability of the Central S.R. 741 Corridor Policy Area; especially since physical lot expansion east or west will not be possible.



THE CINDAY ACADEMY



## POLICY AREA 9

### SR 73/SR 741

This Policy Area surrounds the S.R. 741/73 intersection and is arguably the most visible area of the City for Springboro residents. The area has undergone the most notable recent development in the City with the redevelopment of the NW corner of the S.R. 741/73 intersection being redeveloped as Wright Station. Wright Station is a mixed-use walkable town center development that includes commercial and community uses. Future development and redevelopment in surrounding areas should build on the successes and principles of the Wright Station development to expand the walkable character and experience of the SR 73/SR 741 area. The area also experiences high levels of peak hour traffic congestion and is currently subject to detailed zoning regulations, although the existing development largely predates these requirements.

A summary of the existing character and issues relating to the SR 73/SR 741 Policy Area is as follows:

- » Wright Station redevelopment is 50% complete and has brought new commercial and community uses to the area.
- » Much of the area is controlled by the Urban Village District (UVD).
- » This area tends to have smaller lots as compared to other retail oriented areas of the City.
- » The area is directly north of Historic Springboro (Part of the Historic Core Policy Area, #13).
- » The area's buildings include the Aaron Wright House, a local and federal landmark.

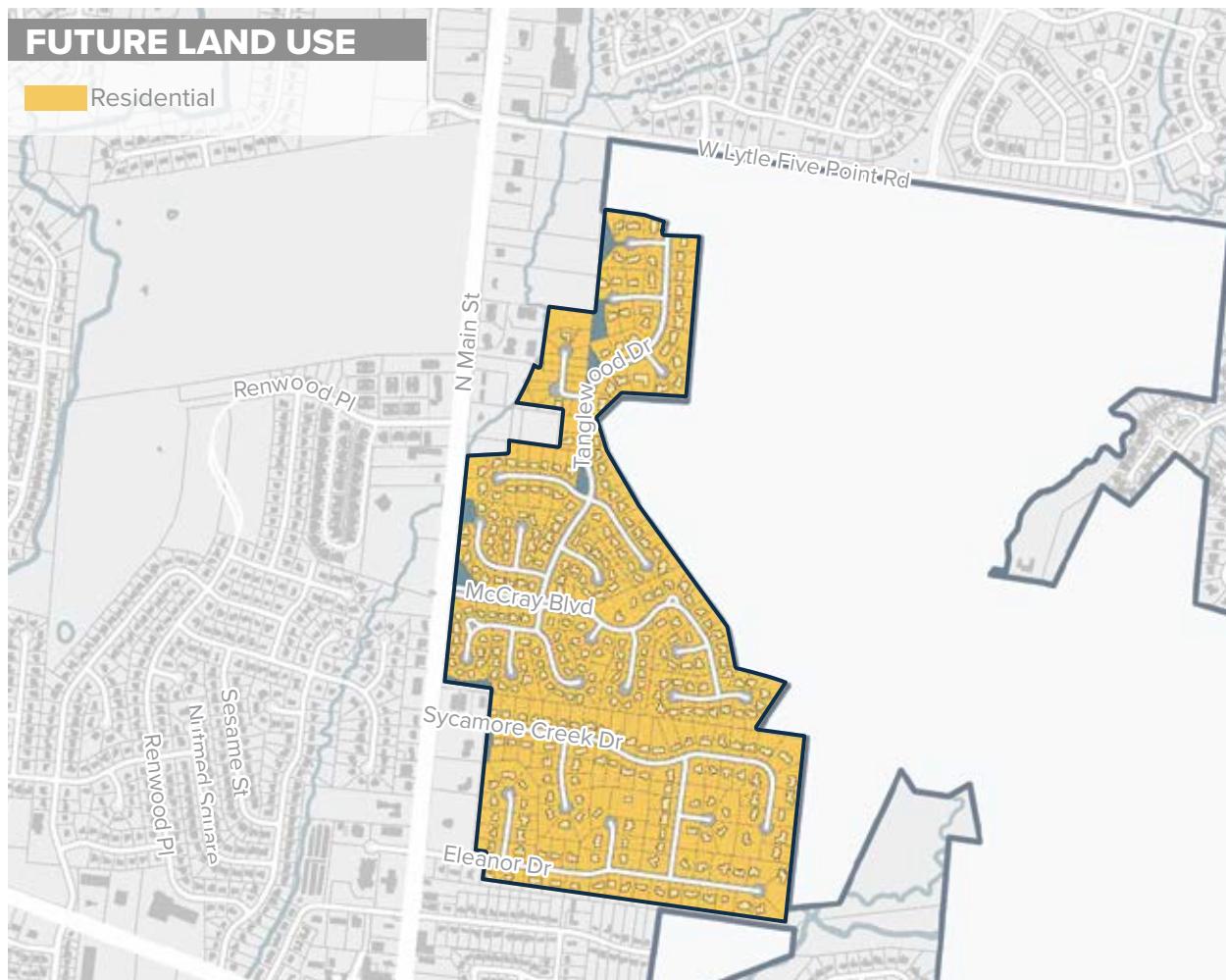
## Preferred Future Characteristics, Actions and Needs

- » Require development and redevelopment on properties that have direct frontage on S.R. 73 or S.R. 741 be physically oriented to S.R. 73 and S.R. 741.
- » Neighborhood-oriented retail must be accessible through sidewalks or trails from adjacent neighborhoods to promote safe pedestrian movement.
- » Limit access to S.R. 73 and 741 through internal access management.
- » Continue to promote mixed-use development in the area around the S.R. 73/741 intersection to make a coherent place building off the Wright Station development.
- » Improve the walkability and character of this Policy Area and the S.R. 73/741 intersection through placemaking and public art initiatives.
- » Improvements to the public realm should focus on connecting this Policy Area with the Historic Downtown to make one downtown experience.

## Implementation Recommendations

Improving the manner in which buildings and uses orient to the S.R. 73/741 intersection will have a significant impact on efforts to facilitate improvements with this Policy Area. The City may wish to explore strategies for facilitating these improvements. Access management control will be particularly critical at this intersection.





## POLICY AREA 10

### EAST CENTRAL SR 741 NEIGHBORHOODS

This Policy Area represents several mature neighborhoods in the City of Springboro. Access is provided to the three neighborhoods from S.R. 741 to the west and Lytle-Five Points Road to the north. Convenience retail and shopping are a short distance away in Policy Areas 8, the Central S.R. 741 Corridor, and 3, North S.R. 741 Corridor.

A summary of the existing character and issues relating to the East Central S.R. 741 Neighborhoods Policy Area is as follows:

- » The Policy Area is comprised entirely of single family detached residential dwellings averaging 30-40 years in age.
- » The area contains the mature neighborhoods that include the Sycamore Creek, McCray Farms, Tanglewood Creek and a portion of the Woodland Greens neighborhoods.
- » There are sidewalks present in this Policy Area that encourages strong internal connectivity. This connectivity extends to S.R. 741.

- » The eastern side of this Policy Area adjoins a golf course, Sycamore Country Club, and open areas in Clearcreek Township.

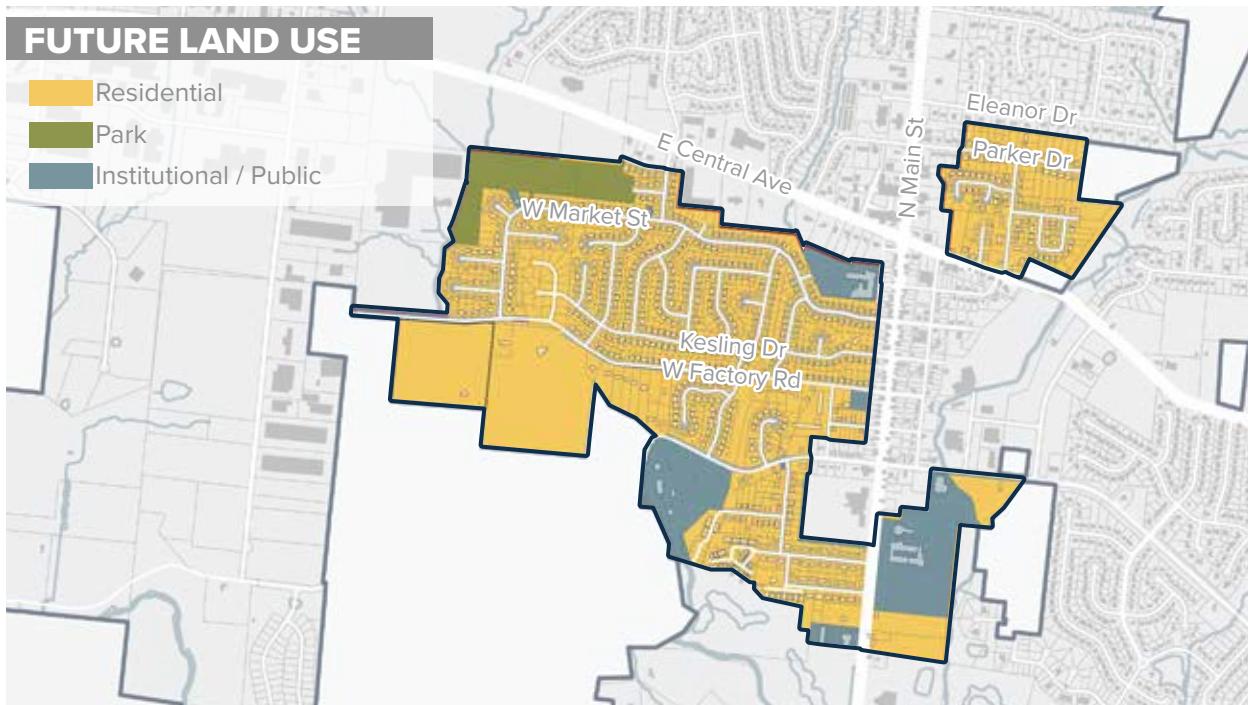
## Preferred Future Characteristics, Actions and Needs

- » Continue to promote the neighborhood character as detached, single-family residential dwellings.
- » Encourage the continued upkeep and maintenance of buildings and properties to maintain property values and the quality of the neighborhoods.

## Implementation Recommendations

Development and redevelopment activities in this Policy Area should emphasize residential uses that conform to the existing character of the neighborhood in which the development or redevelopment is located. Similar building materials, building massing and orientation are essential to the integration of new dwellings. Continuing to support high maintenance standards will be critical for this Policy Area's future success.





## POLICY AREA 11

### CITY CENTER NEIGHBORHOODS

This Policy Area adjoins the Historic Core Policy Area, Policy Area 13, and the SR 73/SR 741, Policy Area 9. The area includes several of the oldest residential neighborhoods in the City built since World War II. Community Park is contained within this Policy Area as well as the former school site located at Florence and North Street. Two unincorporated areas of Clearcreek Township also exist on the eastern side of this Policy Area.

A summary of the existing character and issues relating to the City Center Neighborhoods Policy Area is as follows:

- » The Policy Area includes two nodes physically separated by Policy Area 9 (SR 73/SR 741) and Policy Area 13 (Historic Core).
- » The Policy Area includes predominately

built-out with detached, single family residential dwellings.

- » The area contains the Royal Oaks neighborhood, the first large- scale residential subdivision built outside of the original City plat.
- » Parker Drive homes are the oldest in this Policy Area; they are characterized by brick dwellings older than 50 years in age.
- » A small number of undeveloped infill lots still exist among the single family dwellings.
- » The Meyer Farm subdivision is located in southern portion of this Policy Area.
- » Sawgrass Point, currently under development, lies adjacent to the Royal Oaks Subdivision.
- » Some newer homes are faced with vinyl siding, in contrast with older brick homes.
- » The southern portion of the Policy Area differs from the northern portion of the Policy Area. The southern portion of the

Policy Area contains dwellings largely built in the 1960s, and lacks the brick exteriors that characterize the older areas. The southern portion of the Policy Area is characterized by ranch dwellings built at approximately 4 dwelling units per acre.

- » Community Park is located on S.R. 73 in the western portion of Policy Area.
- » The City's wastewater treatment plant is located in the southern portion of the Policy Area.
- » Clearcreek Elementary School is located on the eastern edge of the policy area.

## Preferred Future Characteristics, Actions and Needs

- » The area has potential for redevelopment in the southwest parts of the policy area, around Royal Oaks subdivision, and in the areas that are currently in the Township.
- » Additionally the former school site on Florence Street should be redeveloped considering its proximity to S.R. 741 and the Historic Downtown.
- » Replace aging infrastructure where necessary to ensure the proper delivery of City services.

## Implementation Recommendations

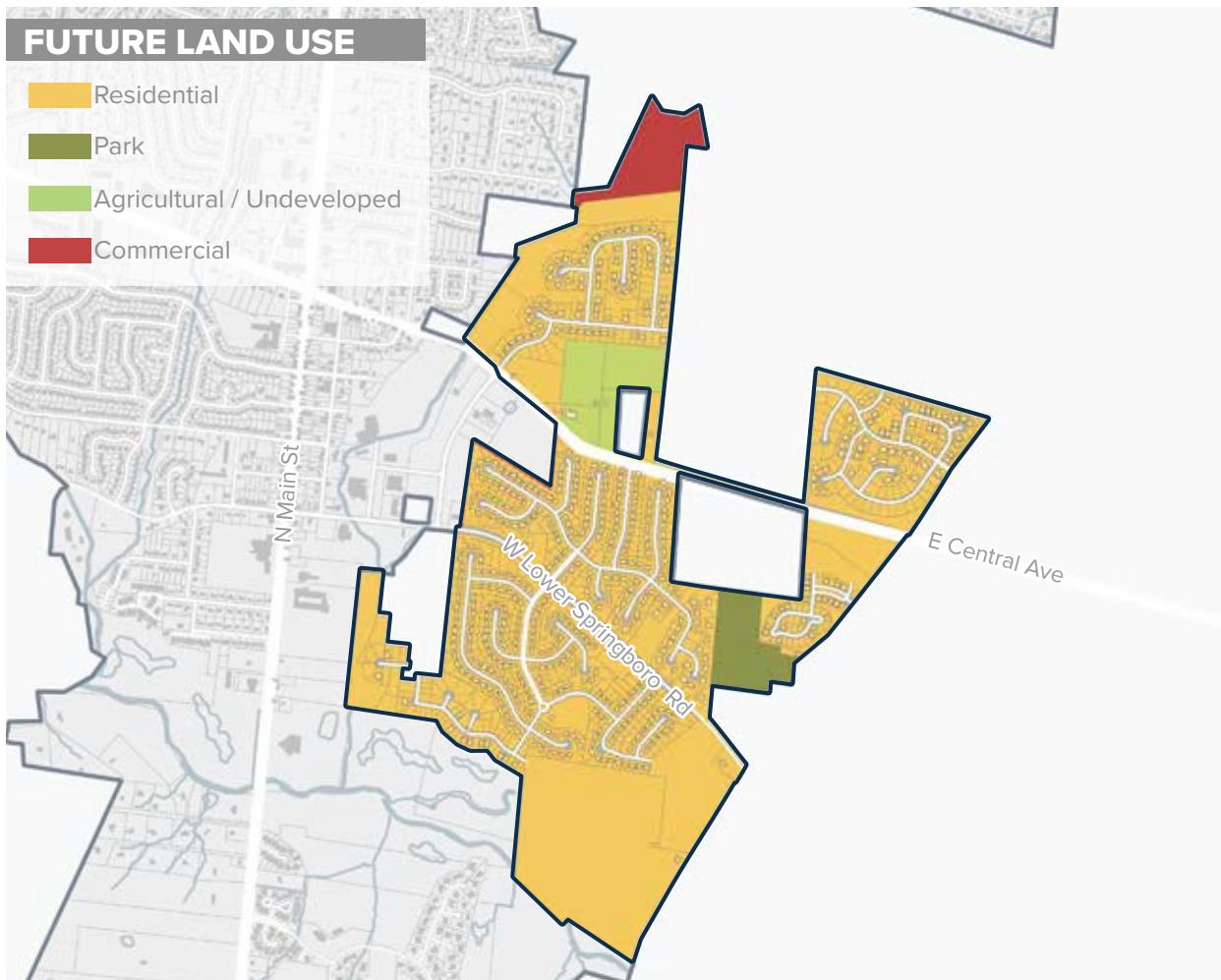
Development and redevelopment activities in this Policy Area should emphasize residential uses that conform to the existing character of the neighborhood in which the development or redevelopment is located.



**COMMUNITY PARK**



**EXISTING SINGLE FAMILY HOME**



## POLICY AREA 12

### SOUTHEAST NEIGHBORHOODS

Located in the southeastern area of the City, this Policy Area includes several newer neighborhoods of Springboro. Bisected by S.R. 73, this area is surrounded on two sides by Clearcreek Township where the development character transitions from smaller residential lots in the City to larger unsubdivided parcels of property and larger lot residential lots. There exists several larger pieces of property that are undeveloped including the recently annexed 73 acre Bailey Property located in the southern portion of this Policy Area on Red Lion-Five Points Road.

A summary of the existing character and issues relating to the Southeast Neighborhoods Policy Area is as follows:

- » The Policy Area includes the following residential subdivisions: Richards Run, North Hills, The Enclave, Fieldstone, Creekside, and The Fairways.
- » This area contains the Bailey Property. That property's development is subject to annexation agreement with Clearcreek Township.
- » The Policy Area's development pattern is primarily detached single family residential; most homes are less than 20 years in age.

- » Sidewalks are readily present in subdivisions, but do not connect to other developments in the city along S.R. 73.
- » Many of the homes are moderately priced in comparison to other newer residential areas of the City.
- » A property at the southwest corner of S.R. 73 and Red Lion-Five Points Road was rezoned in 2002 to PUD Business to accommodate convenience retail uses.
- » Retail use can also be found at the Lovely Farm Market.
- » The Policy Area includes Kessling Park which is owned and operated by the Warren County Park District.

## Preferred Future Characteristics, Actions and Needs

- » Efforts should be made to physically connect the individual subdivisions to other developments and Policy Areas through the construction of sidewalks, trails and bikeways to promote walkability, where they currently do not exist.

## Implementation Recommendations

Improving pedestrian access outside of the individual subdivisions may help overcome the geographic isolation of this Policy Area from the rest of the community. Ensuring high maintenance standards will also be critical for this Policy Area's continued success.

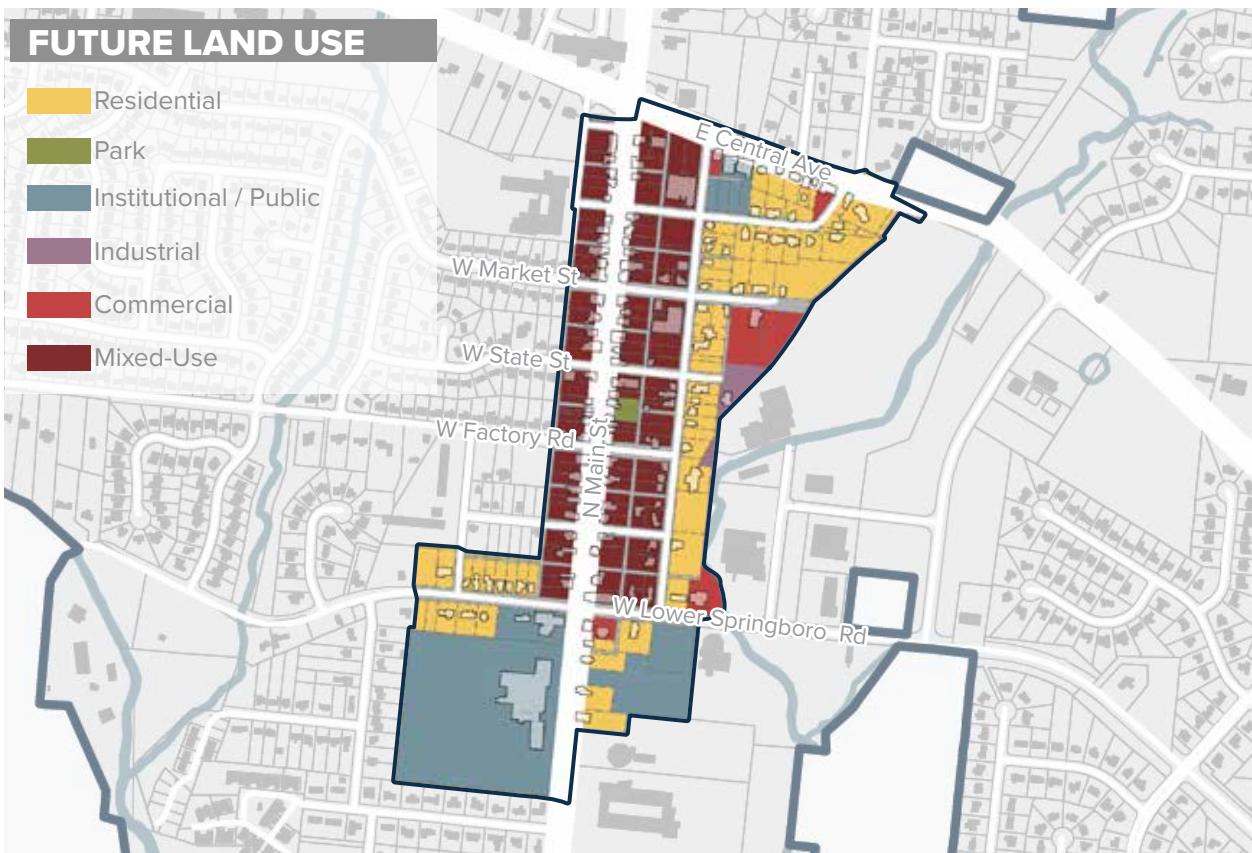
Development and redevelopment activities in this Policy Area should emphasize residential uses that conform to the existing character of the neighborhood in which the development or redevelopment is located.



**EXISTING SINGLE FAMILY HOME**



**LOVELY'S FARM MARKET**



## POLICY AREA 13

### HISTORIC CORE

This Policy Area includes the area most commonly associated with Springboro: the original plat of the City and its largest concentration of pre-Civil War buildings. A large portion of the Policy Area is listed on the National Register of Historic Places and is the subject of a detailed historic preservation review process. This area is also subject to high volumes of peak hour traffic on State Route 741, which diminishes from four to two lanes before entering the Historic Core from the north. Buildings in this Policy Area date between the early 19th and mid 20th Centuries and include a mix of residences, retail businesses, personal service, light industrial and offices uses. Most of the properties are well maintained and have

kept their period architecture. Future uses and development in the Historic Downtown should continue a mixture of land uses that will continue to enhance the walkability and character of the area. Placemaking improvements and an emphasis on improved walkability will help to connect the Historic Downtown with the SR 73/SR 741 Policy Area (9) and expand what the community defines as its downtown.

A summary of the existing character and issues relating to the Historic Core Policy Area is as follows::

- » The Policy Area is largely built out.
- » Portions of the area have local and Federal historic designation.
- » The area is commonly identified by long time residents as the "Identity of Springboro", being the original plat.

- » Rotary Park is located at 320 South Main Street in the Historic Downtown.
- » The area's historic character and intimate scale creates a strong sense of place and supports that the policy area is very walkable.
- » The land use pattern is mixed.
- » Buildings are typically brick and frame construction, and are 2 stories in height. Most buildings are mid-19th century vernacular construction.
- » The area is characterized by alley and on-street parking. There is a perception that adequate parking is not available.
- » The Policy Area is the location of various community events throughout the year.
- » There is a concentration of schools at southern end of Policy Area.
- » The Policy Area is divided by S.R. 741, which is the primary north/south thoroughfare serving the City and surrounding areas.
- » The Policy Area is periodically congested with vehicular traffic, especially during school arrival and release times and at the morning and afternoon rush hours.
- » The area appears to be slowly transitioning from retail and residential uses to office uses.
- » Consider modifying the Zoning Code to promote "form based zoning" to reinforce the character and design of the Historic District. Form based zoning places heavy emphasis on the physical form, features, and visual appearance of buildings and mixed uses over a strong focus on specific land uses by parcel. This type of zoning is most effective in areas where the architectural character is critical to the overall feel, like in the Historic District.
- » Manage traffic through congestion mitigation and improved directional signage for the Richards Run bypass route through Policy Area 14, the South Richards Run Corridor.
- » Maintain the viability of businesses through small business support networks, joint marketing and signage and public education regarding the availability of parking in the Historic Core.
- » Maintain and enhance the community's connectivity to this area, both physically in terms of sidewalks and walking paths, and through promotion of the district to local residents.
- » Maintain the mix of residential, service and retail business uses to ensure a lively, interactive district.
- » Plan for and develop this area as a destination district (e.g. arts district, bridal district, etc.).
- » The City should undergo a specific Downtown Master Planning process to look at strategic planning for the downtown considering land is scarce and it is a delicate blend of activity and history.

## **Preferred Future Characteristics, Actions and Needs**

- » Continue to maintain the historic character and identity of Policy Area through strict enforcement of design guidelines to ensure that property maintenance, infill, and new development are conducive to the historic core design and atmosphere.

## Implementation Recommendations

Education regarding historic preservation techniques, such as is available from the Ohio State Historical Society's Building Doctor program, would also assist efforts to maintain the unique character of this valuable district. Part of the challenge facing this district is the assumption that parking is not available. Although no known study has been conducted regarding the level of use that the existing public parking lot receives, it is likely that this resource is underutilized due to low visitor awareness of its availability and convenience. Installing wayfinding signage that directs visitors to this lot would begin to overcome this perception, as would promotional materials, either independently distributed or printed in a local newspaper, that demonstrate that parking is available. Conducting a downtown master plan could also include a parking study and assessment in addition to being able to look at the downtown in a strategic, detailed manner to create guidance and recommendations for future use and development. Signing and promoting the use of the Richards Run bypass route would also greatly assist efforts to reduce the congestion associated with commutes to schools along S.R. 741.



**DINING & ENTERTAINMENT**



**MIX OF RESTAURANTS AND RESIDENTIAL**



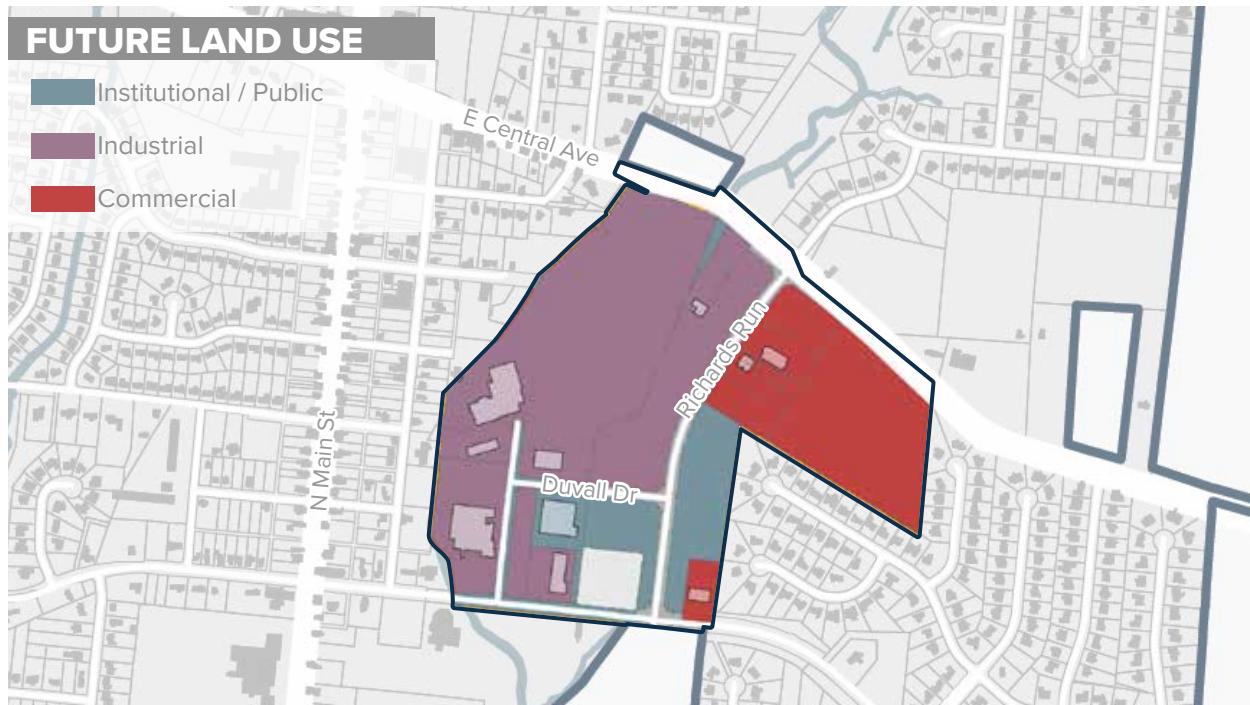
**EXISTING SINGLE FAMILY HOME**



**ROTARY PARK**



**HISTORIC CHARACTER**



## POLICY AREA 14

### SOUTH RICHARDS RUN CORRIDOR

This Policy Area includes a variety of land uses and development issues, ranging from heavy industries to healthcare facilities. The Policy Area also includes a roadway that was designed as a by-pass for the S.R. 741 segment that passes through the Historic Core and to access underutilized land in the area. Portions of this area are also located in the Richards Run flood plain.

A summary of the existing character and issues relating to the South Richards Run Corridor Policy Area is as follows:

- » The Policy Area is characterized by a diverse mix of uses, including the High Concrete plant facility, Mound Steel, the City of Springboro Service/Maintenance Building, undeveloped property, a developing office and business park and a healthcare and rehabilitation center.
- » South Richards Run Road currently serves as a Historic Core (Policy Area 13) by-pass option for peak hour vehicular and heavy truck traffic on S.R. 741.
- » The Policy Area is surrounded by residential uses of varying densities.
- » The Policy Area includes a large portion of the Richards Run floodplain.

## Preferred Future Characteristics, Actions and Needs

- » Encourage the development of larger areas of available property as office.
- » Encourage office infill development around the existing concrete facility.
- » Continue and promote the use of South Richards Run Road as a viable by-pass for the Historic Core to reduce traffic congestion in the Historic Core and SR 73/SR 741 Policy Areas.

## Implementation Recommendations

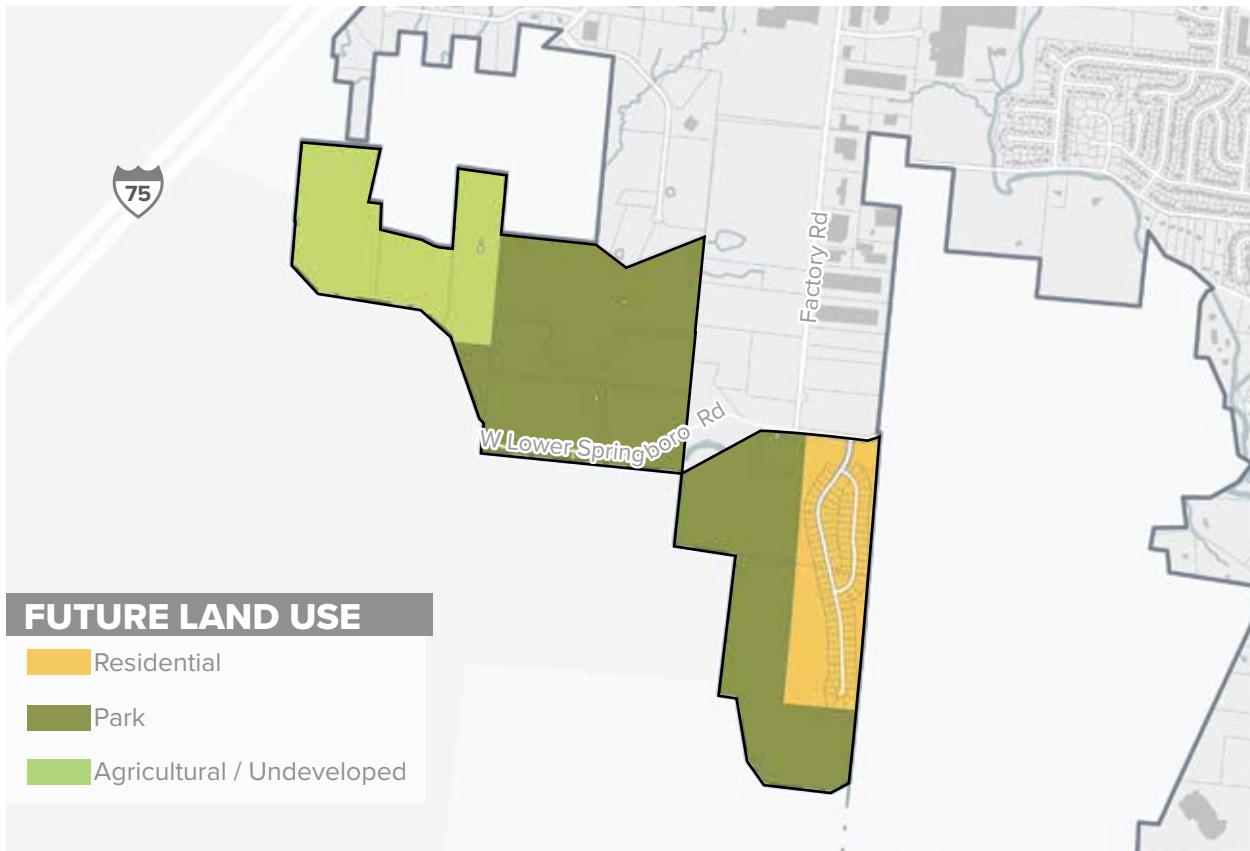
Properties should be encouraged to develop as office type uses with a strong de-emphasis on industrial uses. Because of its relative high visibility on S.R. 73, high landscaping and design standards that coordinate building and lot design in this area are preferable. This area should serve as a transition between the residential uses in Policy Area 12 (Southeast Neighborhoods) and Policy Area 13 (Historic Core).



SMALL OFFICE BUILDING



SMALL OFFICE BUILDING



## POLICY AREA 15

### CLEAR CREEK CORRIDOR

This largely rural Policy Area is located at the southwestern edge of the City and is bordered by Clearcreek Township, Franklin Township and the City of Franklin. With the exception of one recently constructed subdivision, Beck Ridge, this area largely consists of parkland and a wide flood plain associated with Clear Creek and farmland.

A summary of the existing character and issues relating to the Clear Creek Corridor Policy Area is as follows:

- » The Policy Area is characterized by three community parks: E. Milo Beck, Clearcreek, and Hazel Woods Parks.
- » The Policy Area includes the Beck Ridge neighborhood.
- » The area includes the large floodplain and floodway of Clear Creek.
- » The Policy Area is surrounded on three sides by other political subdivisions: Clearcreek Township, Franklin Township and the City of Franklin. These areas are generally lands with large lots and are undeveloped.
- » Limited access to this Policy Area is afforded through a light industrial complex, South Pioneer Drive, in Policy Area 5, and via West Lower Springboro Road.
- » The Policy Area has a rural and visually pleasing atmosphere.
- » The Policy Area also abuts Hazelwoods Park located to the southwest, outside the City corporate boundary.

## Preferred Future Characteristics, Actions and Needs

- » Maintain the park like atmosphere and maintain the quality of the floodplain.
- » Provide for internal and external connectivity with the implementation of bike paths and walking trails.
- » Create a Parks Master Plan to guide future improvements and maintenance of parks not only in this Policy Area, but throughout the City.

## Implementation Recommendations

Development should be limited to conservation or open space type subdivision located south of Lower Springboro Road in order to promote the rural and natural beauty of the area and recognizing limitations to more intensive development (e.g., steep slopes, flood plains). Pedestrian linkages between the Beck Ridge neighborhood, new residential development, and the parks in this area should be promoted.



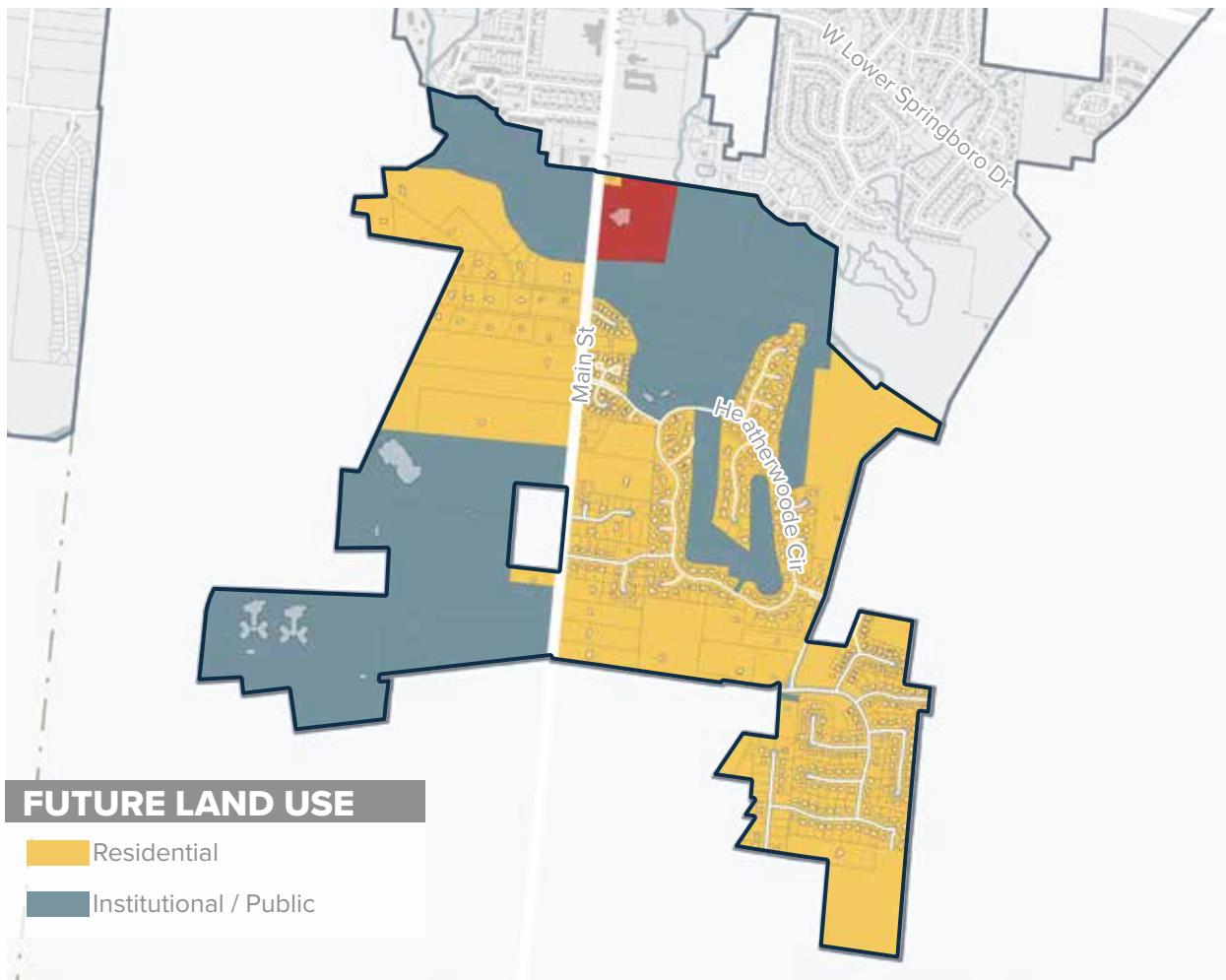
EXISTING SINGLE FAMILY HOME



CLEARCREEK PARK



E. MILO BECK PARK



## POLICY AREA 16

### HEATHERWOODE

This Policy Area includes both pre-annexation lot split homes and some of the City's most recent subdivisions. The area also includes an 18-hole public golf course community, Heatherwoode, and a multi-facility public school complex. The area's character is predominately low- to moderate-density detached residential and includes extensive active and passive open space areas. There are multiple large single family lots that could be redeveloped in the future.

A summary of the existing character and issues relating to the Heatherwoode Policy Area is as follows:

- » The Policy Area is located on the southernmost edge of the City and is bisected north/south by S.R. 741.
- » This Policy Area is the location of the Heatherwoode Golf Club community and the Springboro Public Schools complex (Springboro High School, Springboro Junior High School, Dennis Elementary as well as administrative offices and support facilities).
- » The area is characterized by large lot single family residential uses, particularly along S.R. 741.

- » The area includes the Heatherwoode and Farms of Heatherwoode subdivisions.
- » The Null Log Home, erected in 1798, is located in the policy area on the grounds of the golf course.
- » The northern portions of the Policy Area is within the Clear Creek floodplain/floodway.

## Preferred Future Characteristics, Actions and Needs

- » Improve pedestrian access and encourage safe school access for nearby residents through the establishment of sidewalks and/or walking trails.
- » Continue to promote and maintain the Heatherwoode Golf Course as a valuable local and regional amenity.

## Implementation Recommendations

Development and redevelopment activities in this Policy Area should emphasize residential uses that conform to the existing character of the neighborhood in which the development or redevelopment is located.





## C. CONNECTIVITY

### Challenge and Opportunities

The recommendations recognize that Springboro is a mostly built-out city and that any new development must be fully leveraged to help the city fulfill its vision of the future. The recommendations also recognize that many aging auto-oriented strip developments are likely to be revamped or replaced altogether over the next 20 years. Wright Station is a prime example of changing tastes in commercial development.

### Coordination with Placemaking Recommendations

All of the connectivity recommendations are enhanced and made complete by the incorporation the placemaking recommendations. At the heart of connectivity recommendations is the concept that streets and pathways are city's most used, most visible, and most valuable public spaces. A community is judged by the appearance of its streets and the activity of pedestrians and bicyclists visible therefrom. People in cars look for other people walking or bicycling as a sign that a place is safe, thriving, and worth a visit.

### General Approach to Improving Connectivity

There are three themes that thread through all the connectivity goals: 1) change the character and development along the major roadways so people drive at the desired speeds; 2) create pedestrian scale environments through new projects and redevelopment; and 3) create direct connections for people on foot and bike to encourage alternative forms of transportation and reduce motorized traffic.

The city should undergo the development of an update to the 2001 Mobility Master Plan for the community that will explore the goals and recommendations in more detail.

### Connectivity Goals

The following six connectivity goals are explored in more detail on the following pages:



**STRENGTHEN  
PEDESTRIAN  
CONNECTIONS  
BETWEEN WRIGHT  
STATION AND  
THE HISTORIC  
DOWNTOWN**



**CONSTRUCT  
ALTERNATIVE  
ROUTES FROM  
NORTH TO SOUTH  
PORTIONS OF  
SPRINGBORO**



**ESTABLISH  
PEDESTRIAN  
AND BICYCLE  
CONNECTIONS  
BETWEEN  
NEIGHBORHOODS**



**ENHANCE  
PEDESTRIAN  
CONNECTIONS  
TO ADJACENT  
RESIDENTIAL  
AREAS**



**NEW WALKABLE  
MIXED-USE  
NEIGHBORHOODS**



**ENHANCE THE  
CHARACTER  
OF THE MAJOR  
ROADWAYS TO  
SLOW DOWN  
TRAFFIC**

## 1. Strengthen pedestrian connections between Wright Station and the historic downtown.

Even though the distance between Wright Station and the historic downtown is not significant, the nature of the route itself inhibits pedestrian travel. The only current pedestrian link is through the Main Street / Central Avenue intersection, which can be intimidating to pedestrians. With the Wright Station Development and planned improvements to the public realm in the area, the ultimate goal is to create a walkable center for the community that creates a synergy between new development and the historic downtown.

The city has been and continues to work to identify improvements that will encourage walkability within the area. The character of the historic district encourages a mixture of uses and a more pedestrian-oriented scale of street.

This recommendation focuses on continued efforts to create a richer, more connected commercial and cultural environment.

### City Led Objectives

- » Provide a new pedestrian crossing on Central Avenue at Edgebrook Drive to create a pedestrian link between the Wright Station Area and the south side of SR 73 and the neighborhoods around the Historic Core.
- » Enhance downtown alley parallel to Main Street to foster development of the back lots of Main Street businesses
- » Complete the sidewalk system and add bike lanes on Edgebrook Dr and create

additional points of entry from Edgebrook Dr into Wright Station

- » Open the mid-block pedestrian alleyways that lead from Main Street to the alley to access the new developments.
- » Create a Downtown Master Plan that can formally design and plan for connectivity improvements in the downtown area.

### Development Driven Objectives

- » Encourage mixed-use development on the former school site on Florence Drive.
- » Foster updating and upgrading the apartments and town homes on Edgebrooke Drive.
- » Promote development of alley facing “back lots” of the commercial properties on Main Street.

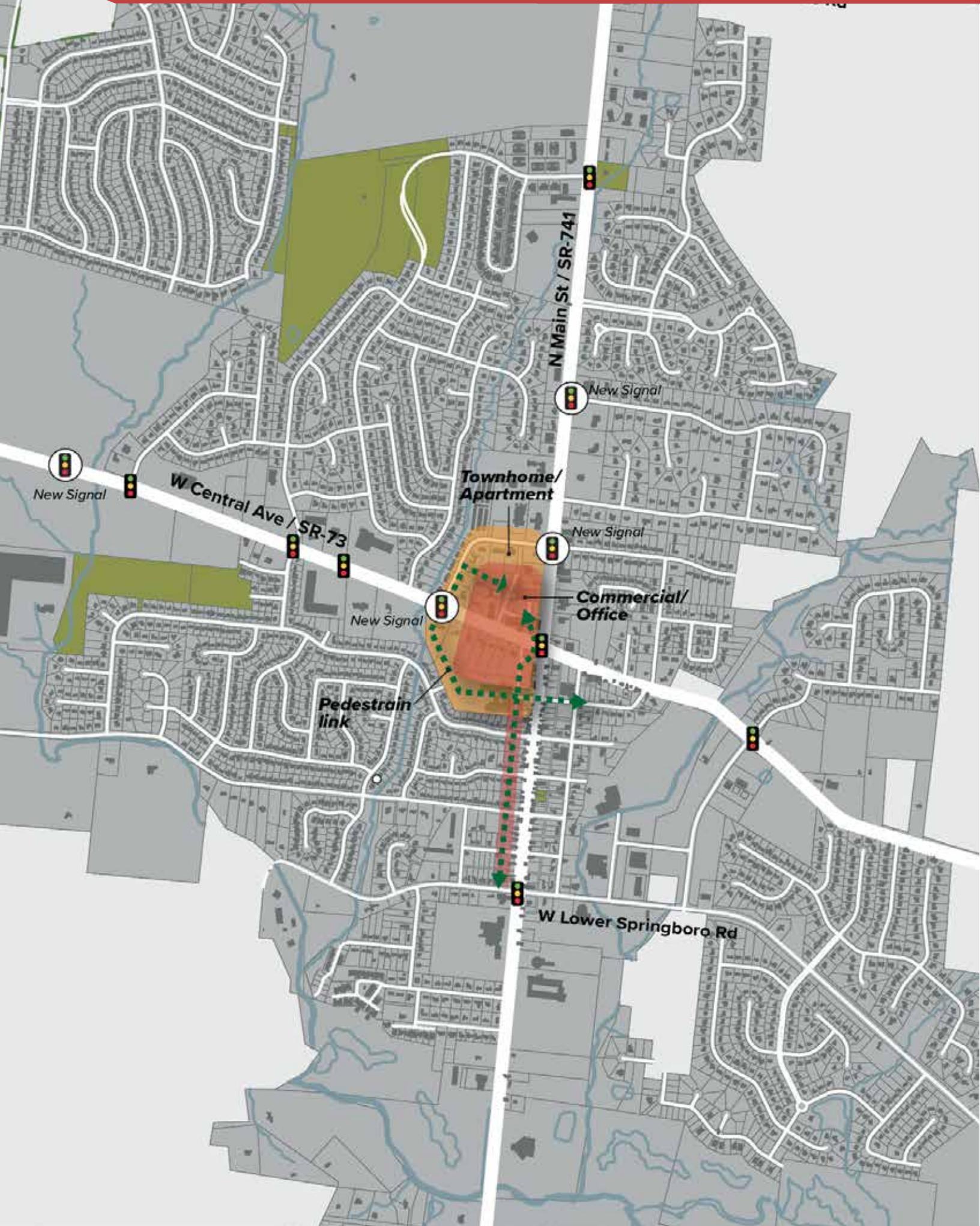


### MULTI-USE PATH



### PUBLIC ART IMPROVEMENTS

# STRENGTHEN PEDESTRIAN CONNECTIONS



## 2. Construct alternative routes from north to south portions of Springboro.

The historic downtown creates a bottleneck between destinations on the south side of the Springboro and population centers on the north side of town. Physically, it is not possible to add any more roadway through the downtown without destroying the historical character. During the morning rush hour, travel times from the residential areas on the north side of town to the schools and other destinations on the south side of town can vary wildly from 12 to 26 minutes if routing through the downtown.

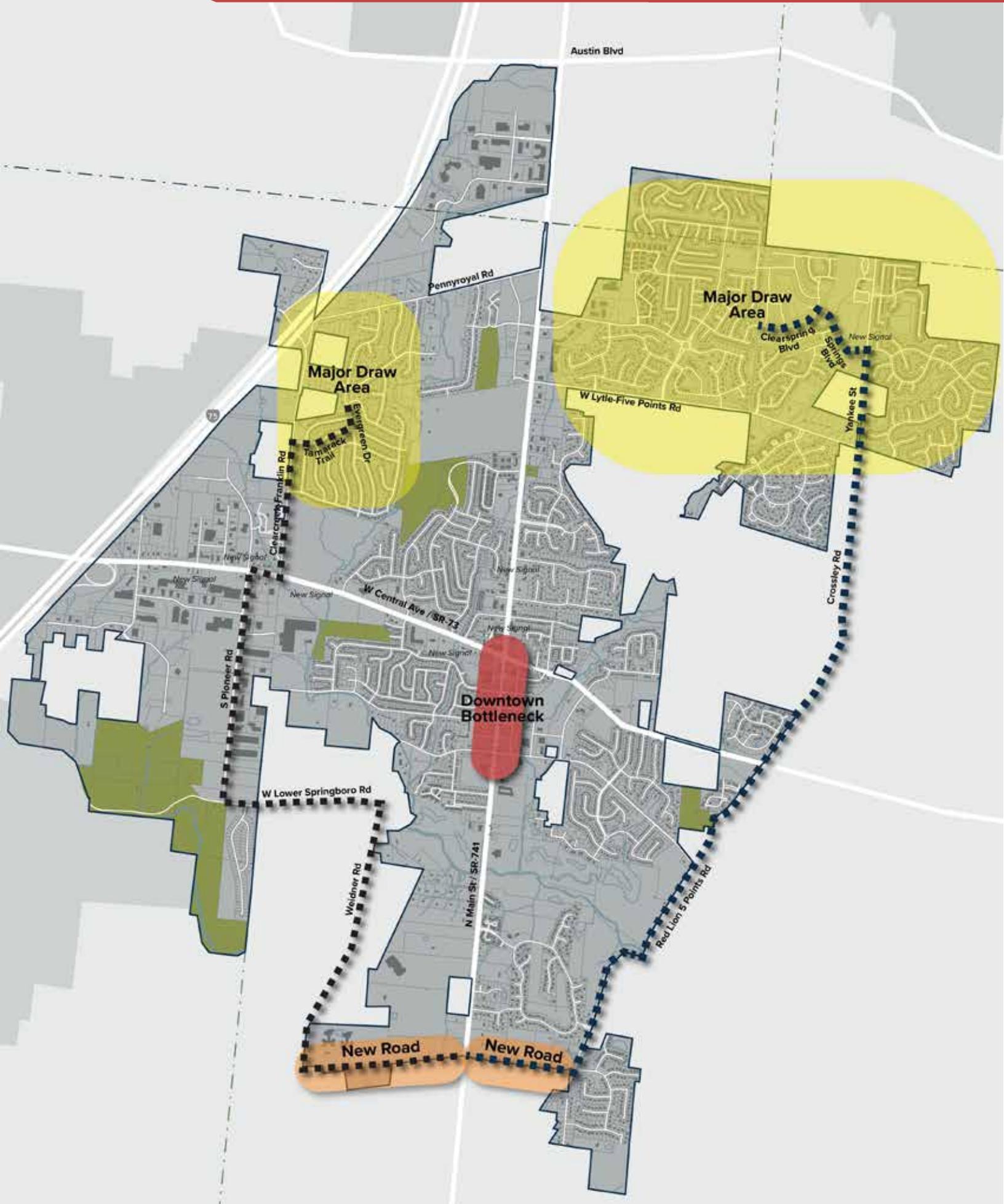
An alternative route on the east side of town provides more consistent times of 14 to 18 minutes but results in significant cut through traffic on residential roadways. The alternative route on the west side requires going significantly south of the schools before looping back north.

A proposed east-west road south of the downtown between Weidner Rd and Red Lion 5 Points Road would relieve pressure through the historic downtown and cut-through traffic in the Heatherwoodes neighborhood. Dispersing traffic across three routes will result in more predictable travel times and provide multiple options in case one route is temporarily blocked.

### City Led Objectives

- » Coordinate with ODOT, county, and townships officials on alignment and to bring sub-standard built roadways segments up to best safety practices
- » Implement traffic calming measures on the current cut-through route of Edinburgh Drive, Heatherwoode Circle, and Balcombe Way from Edinburgh Dr to Red Lion 5 Points Rd.
- » Provide new traffic signal with pedestrian crosswalk at South Main Street and the new road
- » Update the Mobility Master Plan (2001).
- » Coordinate with schools on future school sitings that do not negatively impact traffic in the Historic District on S.R. 741.

# CONSTRUCT ALTERNATIVE ROUTES



### 3. Establish pedestrian and bicycle connections between neighborhoods.

Springboro's irregular border, township islands, and natural physical barriers isolate many neighborhoods from each other and key City amenities. The result is a transportation system that promotes unnecessary motor vehicle traffic within the neighborhoods and on the busy thoroughfares. The existing transportation network actively discourages pedestrian and bicycle travel.

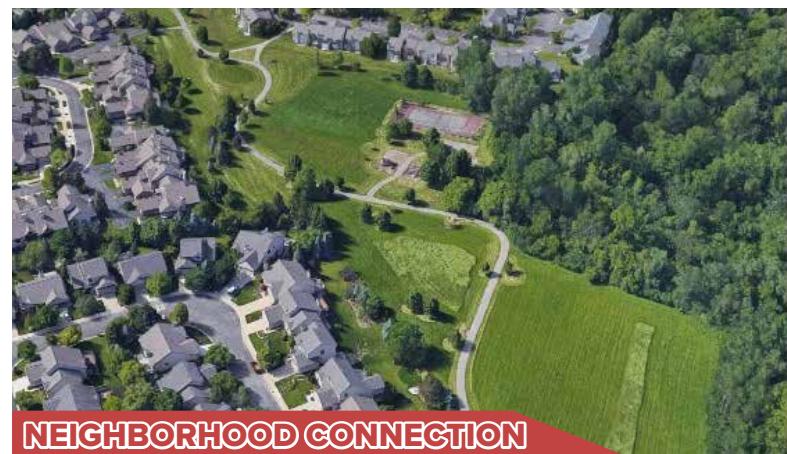
A number of pedestrian and bicycle links are proposed. Three of them provide access across Twin Creek which has only one non-accessible pedestrian connection between Pennyroyal Road and West Central Ave. Another four are focused on tying in the North Hills and The Enclave neighborhoods. The last, focus on tying the McCray Farms neighborhood to the Springbrook neighborhood and North Park. All these links will become park to the low-stress bicycle route system.

#### **City Led Objectives**

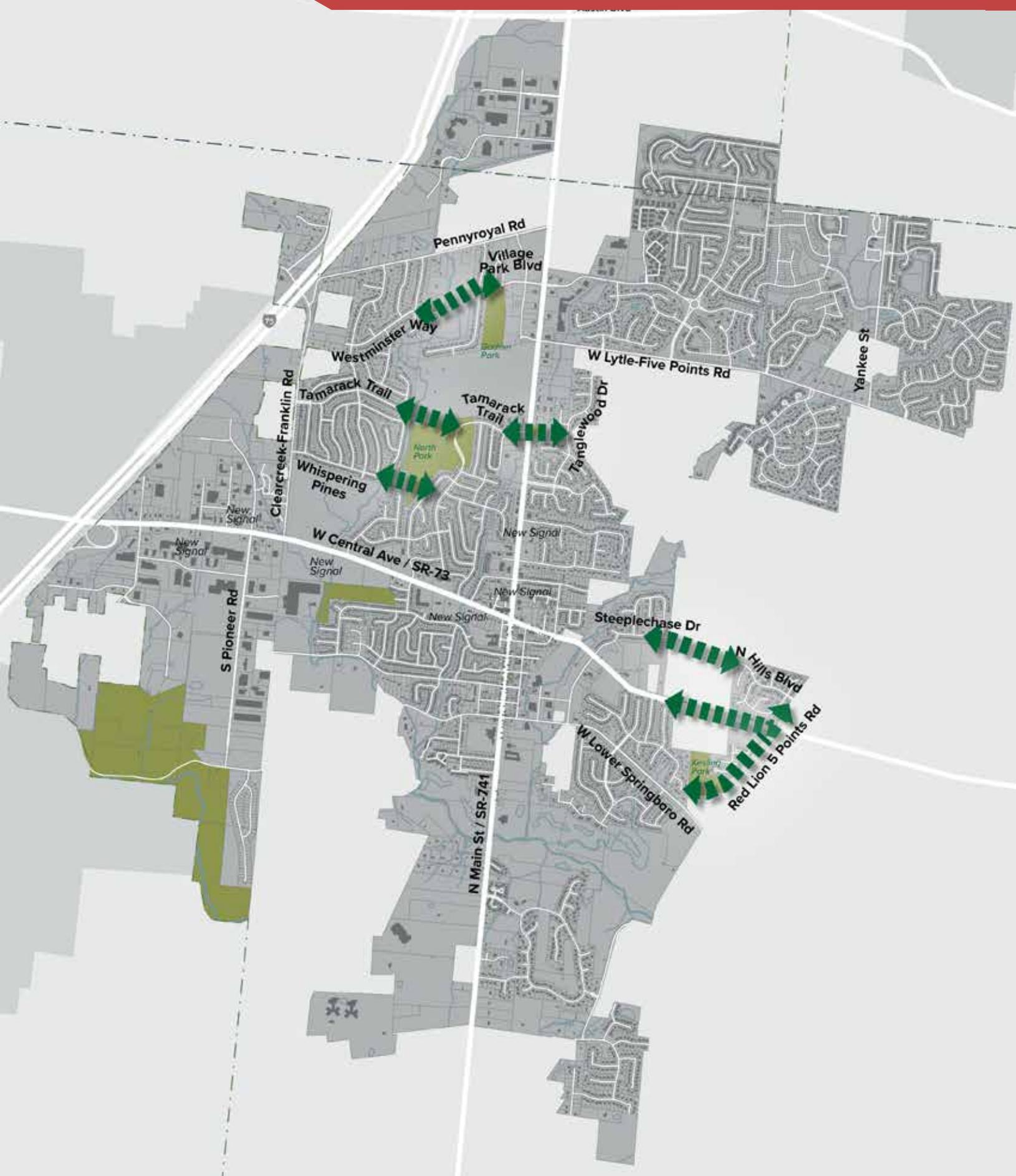
- » Obtain ROW for the proposed connectors through currently developed areas
- » Coordinate with the county and township on connectors outside of the City and along roadways
- » Construct Shared Use pathways on City property and easements

#### **Development Driven Objectives**

- » Work with developers to provide all-season accessible pedestrian and bicycle links through open space concurrent with development



# ESTABLISH CONNECTIONS BETWEEN NEIGHBORHOODS



## 4. Enhance pedestrian connections to adjacent residential areas.

There are many small commercial and service areas that have do not have a direct connection to their surrounding residential neighborhoods. Many of these developments support the type of commercial activity that is suitable for walking and bicycling trips from home. And as some of these developments are currently served by traffic signals; they hold the potential to serve as key links between neighborhoods bisected by the busy state routes. The result is an environment that promotes unnecessary motor vehicle traffic within the neighborhoods and on the busy thoroughfares.

In addition, many of these retail establishments are set back from the roadway. The result is poor visibility from the roadway as well as a wide-open transportation corridor that encourages high speeds. The proposed approach reorients the existing strip development to the street and establishes attractive pedestrian connections from adjacent residential areas and improves cross corridor pedestrian connectivity.

### City Led Objectives

- » Obtain ROW and/or coordinate with property owners to create pathway connections

### Development Driven Objectives

- » Update zoning so that when areas redevelop, retail areas are placed closer to the roadway, parking is located in back, and pedestrian connections lead directly from the sidewalk and the neighborhoods into the heart of the development
- » Create outdoor café seating adjacent to the pedestrian and bicycle connectors



**SIMPLE PATH CONNECTION**

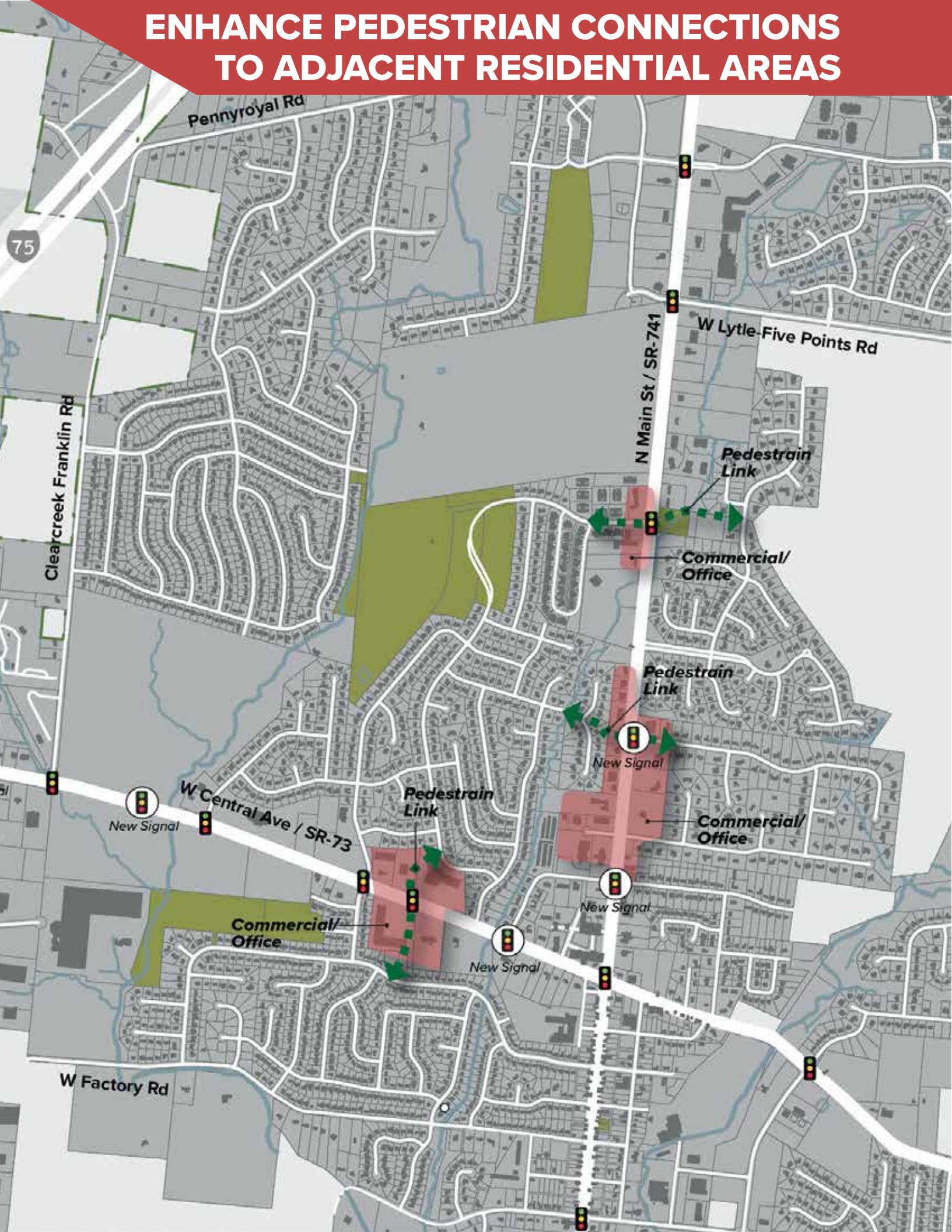


**MULTI-USE PATH**



**SAFE CROSSINGS**

# ENHANCE PEDESTRIAN CONNECTIONS TO ADJACENT RESIDENTIAL AREAS



## 5. New walkable mixed-use neighborhoods.

The large undeveloped parcels known as Eastbrook Farm, the Easton Farm and the Easton property on S.R. 73 provide amazing opportunities to create the type of places that the residents of Springboro said they would like to see. These parcels are also linked by the proposed Central Greenway from the City's 2020 Pedestrian and Bicycle Plan. The result is the city has the opportunity to create "ten minute" neighborhoods where many daily errands may be done more conveniently by walking or bicycling than by driving. And by linking these developments with the Central Greenway, the amenities of the new developments will also easily accessible by existing neighborhoods.

The proposal would establish commercial nodes along the state routes. These neighborhood commercial cores would be comprised of a few blocks of retail, dining, cafés, pubs, and office space fronting on the main roads. Surrounding the commercial core will be single-family housing options that mirror surrounding development. Each development should have a distinct identity and character.

Tying all the different land uses and the neighborhoods together is the Central Greenway where the pedestrian and bicycle links are the centerpiece of development with parklets and outdoor cafés fronting the greenway. Where the greenway meets the state routes, safe, comfortable, convenient, and highly visible pedestrian crossings are provided to cross the busy roads.

### City Led Objectives

- » Promote the guidelines and zoning site plans that promote walkable and bikeable communities
- » Require the incorporation of the Central Greenway in new development

### Development Driven Objectives

- » Incorporation of the Central Greenway as the centerpiece of new ten-minute neighborhoods

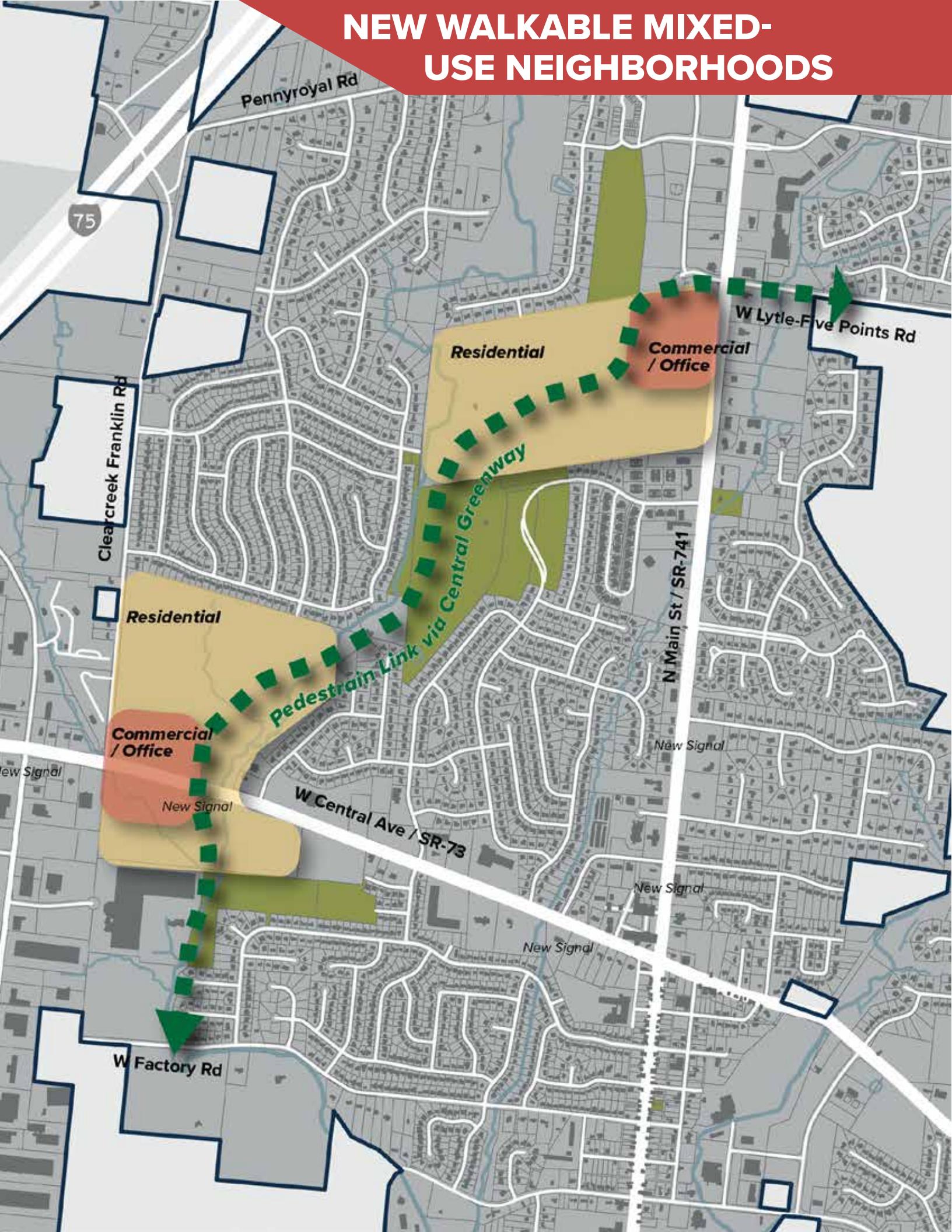


MULTI USE PATH EXAMPLE



OUTDOOR SEATING

# NEW WALKABLE MIXED-USE NEIGHBORHOODS



## 6. Enhance the character of the major roadways to slow down traffic.

State Routes 73, and 741 both connect and divide Springboro. One only has to look at Main Street/SR-741 immediately north and south of Central Ave/SR-73 to understand the impact that number of lanes, traffic speed, the proximity of buildings, block size, and the presence of pedestrians have on roadways character. North Main Street and Central Ave will continue to separate neighborhoods and diminish the desired small town feel unless they are fundamentally changed.

There is a myth that speed equates efficiency in transportation. The reality is that a slower and more consistent speed is the most efficient and safest approach. At slower speeds, a driver's awareness expands laterally, allowing them to spot pedestrians, bicyclists, and retail destinations along the roadway. The impact of slower speeds on travel time is typically grossly over estimated. The two mile stretch of Central Ave from I-75 east to Main Street or Main Street from Downtown north to Pennyroryal Rd will only take a minute longer to drive at 30 mph vs the current 45 mph and 35 mph speed limits. Signalized intersections have a much more significant impact on travel time than does the speed limit.

It is proposed that the city work to enhance the character of the major roadways and use the design the road environment to manage motor vehicle speeds. This in combination with many of the previous connectivity goals will have a noticeable impact on the character of the state routes making for a safer, more attractive environment that supports all means of travel.

### City Led Objectives

- » Place street trees every 30 – 40 feet between the sidewalk and the roadway.
- » Provide 8' wide sidewalks along the Arterial roadways and 6' wide sidewalks along the Collector roadways.
- » Periodically provide amenities such as shaded rest areas with benches and water fountains.
- » Provide pedestrian scale lighting fixtures that provide bright and uniform lighting for personal security and visibility at crosswalks.
- » Incorporate "Green Street" elements such as rain gardens
- » Provide landscaped medians wherever possible.
- » Incorporate best practices for pedestrian safety at all mid-block and signalized crosswalks.
- » Use access control/management strategies to minimize the number of curb cuts. This will be done in coordination with new development and redevelopment plans. Access management reduces the number of curb cuts and thus the potential conflict points between cars, trucks, bikes, and pedestrians.

### Development Driven Objectives

- » Update zoning so that when areas redevelop, the commercial structures are placed closer to the roadway.



## D. PLACEMAKING

The collection of needs and ideas that Springboro residents shared have been combined with research and on-site analysis of Springboro's arts, culture, and public space opportunities. Focusing on public art in key locations is an impactful way to make sure Springboro residents are seeing improvements. These recommendations have been filtered into five creative placemaking ideas: The following sub-section details each of these ideas. Each sub-section will highlight the project goals it will achieve and an explanation of why this approach fits its selected location and how it can energize and build momentum for future placemaking.

### Types of Placemaking and Public Art

The following offers inspiration for what could be implemented



**PUBLIC FURNITURE**



**ARTISTIC BENCH**

### MURALS ON BUILDINGS



## 1. Historic District Placemaking and Public Art

Local business owners in the Historic District have led the effort in creating programmed events that bring residents into the district on a recurring basis. Recurring programmed events have become a critical component in development strategy and revitalization. As sidewalks begin to fill again with people meandering and lingering, placemaking and public art add another layer of vitality by helping shape the physical and social character of the district.

Project goals this initiative achieves:

- » Explore and demonstrate how creative placemaking can support and enhance Historic District programming
- » Discover new partnerships to enhance programming and art activities
- » Investigate art and placemaking in private developments
- » Identify key near - and long-term actions to create opportunities to activate and enliven public spaces throughout the Historic District with arts and cultural activities

Project goals this initiative achieves:

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- » Investigate art and placemaking in private developments
- » Identify key near - and long-term actions to create opportunities to activate and enliven public spaces throughout the Historic District with arts and cultural activities



## 2. Wright Station Placemaking and Public Art

As Wright Station continues to develop, this will become a natural gathering space for people due to the mix of shops and restaurants, greenspace, programmed events, and can therefore play an important role in placemaking initiatives and public art. Placemaking and public art opportunities should require strategic public/private partnerships and collaboration that prioritize community benefit and economic success.

Project goals this initiative achieves:

- » Explore and demonstrate how creative placemaking can support and enhance Wright Station development
- » Discover new partnerships to maximize opportunities for public art and placemaking at Wright Station
- » Illustrate how public art and placemaking can create a sense of place, community identity, and contribute to Wright Station branding

### Types of Placemaking and Public Art

The following offers inspiration for what could be implemented





**ARTFEST ON MAIN**



**POP UP SHOW**

### 3. Wright Station Programming

With Wright Station in its early stages of development, the role of programming will be crucial to attract attention to the location. Creating events that become a fixture in the cultural life of Wright Station will foster strong social capital and lay the foundation for repeated visits. The experiences offered through diverse programming at Wright Station will be fundamental to Wright Station becoming a multi-amenity destination

Project goals this initiative achieves:

- » Explore and demonstrate how creative placemaking can support and enhance Wright Station development
- » Discover new partnerships to maximize opportunities for public art and placemaking at Wright Station
- » Illustrate how public art and placemaking can create a sense of place, community identity, and contribute to Wright Station branding

#### Types of Programming

The following offers inspiration for diverse programming - attracting a broad range of audience



**BBQ FEST**



**WARPED WING**

## 4. Encourage Walking Between the Historic District and Wright Station

The context of downtown Springboro is high-density. In most American cities, the place where most people are likely to walk is downtown. More people walking about and enjoying sidewalk space creates a livelier city and is the first step towards stronger, connected neighborhoods. Creating a comfortable walking corridor using signage, color, and recurring programming will be key to linking these two destinations.

Project goals this initiative achieves:

- » Improve the pedestrian experience with aesthetics and more active storefronts that encourage walking along state route 741 between the Historic District and Wright Station
- » Uncover new partnerships that enhance the walking experience between the Historic District and Wright Station
- » Identify low-cost, quick + lite actions to create opportunities that encourage more people to walk between the Historic District and Wright Station

### Types of Low-Cost, Quick+Lite Actions that Encourage Walking

The following offers inspiration that can be done on a cost-effective, quick to implement timeline



SEATING



ACTIVE STOREFRONTS



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## **SECTION 6**

# **IMPLEMENTATION STRATEGY**



# Implementation Strategy

This Master Plan's recommendations found in Section 5 should guide future decision making for City staff and elected officials at a variety of scales. This section highlights the key implementation tasks that correspond to recommendations of the plan. These recommendations are broken down into five categories: Land Use and Development, Parks, Connectivity, Placemaking, and Policy. Each implementation recommendation includes a description and a priority level.

Implementation is perhaps the most critical part of this plan. Identifying these initiatives is the first step in realizing the vision established by all of the work, input, and effort put into this Master Plan. Prioritizing the right partnerships, and personnel who can assist in the various implementation tasks is also critical. The Planning Commission should work with city staff, elected officials, and citizens to drive the implementation of this plan. This combined strategy covers approximately the next 10 years of life in Springboro.

Recommendation	Description
<b>Land Use and Development</b>	
<b>Develop Design Guidelines</b>	The City will develop design guidelines to guide character and features for new development and redevelopment projects within the city.
<b>Develop Design Guidelines</b>	The City will develop design guidelines to guide character and features for new development and redevelopment projects within the city.
<b>Parks &amp; Connectivity</b>	
<b>Create a Parks and Recreation Master Plan</b>	Residents of Springboro have made it clear how much they enjoy the parks in Springboro. Creating a master plan would allow the city to establish a vision for improving the parks that are such a strong asset for the city.
<b>Continue to Implement the Bicycle and Pedestrian Master Plan</b>	The City should continue to implement the vision laid out in the Bicycle and Pedestrian Master Plan (2020).
<b>Update the Mobility Master Plan</b>	The City should update the 2001 Mobility Master Plan that incorporates the Bicycle and Pedestrian Master Plan (2020) with other transportation recommendations.
<b>Placemaking</b>	
<b>Implement Placemaking Initiatives in Wright Station and Historic Downtown</b>	Continue to implement placemaking initiatives, as defined in this plan, in the Wright Station and Historic Downtown areas of the city that celebrate the history and promote a sense of community in the City.
<b>Develop an Improved Community Gateway at I-75 and SR-73 to tie into the rest of Springboro</b>	Establishing a gateway into Springboro will help better define Springboro as a unique place and build more pride in the community.
<b>Policy</b>	
<b>Update the Springboro Zoning Code</b>	Updating the zoning code will better guide the city towards the vision created by the community in this plan and will better inform developers of the desires of the community related to land uses and character.



SPRINGBORO  
MASTER PLAN ➞

**City of Springboro**  
320 West Central Avenue, Springboro, Ohio 45066  
Planning Commission Meeting  
Wednesday, December 8, 2021

**I. Call to Order**

Chair Becky Iverson called the Springboro Planning Commission Meeting to order at 6:00 p.m. at the Springboro Municipal Building, Council Chambers, 320 West Central Avenue, Springboro, Ohio.

Present: Becky Iverson, Chair; Chris Pearson, Vice-Chair, Robert Dimmitt, Steve Harding, Mike Thompson, Mark Davis and John Sillies.

Staff Present: Dan Boron, City Planner; Elmer Dudas, Development Director, Ann Burns, Planning Commission Secretary.

**II. Approval of Minutes**

**A. November 10, 2021 Planning Commission Minutes**

Ms. Iverson asked for corrections or additions to the minutes. There were none

*Mr. Harding motioned to approve the November 10, 2021 Planning Commission minutes. Mr. Dimmitt seconded the motion.*

**Vote:** Sillies, yes; Dimmitt, yes; Harding, yes; Pearson, yes; Davis, yes; Thompson, yes; Iverson abstained. (6-0)

**III. Agenda Items**

**A. Final Review, Revision to Approved Site Plan, 465 Victory Drive, commercial building under construction for B&E Plumbing**

**Background Information**

This agenda item is a request for a review to an approved site plan for B&E Plumbing located at 465 Victory Drive. A site plan for the construction of a 3,605-square foot commercial building was approved by the Planning Commission, subject to staff comments, at their August 18, 2021 meeting.

Once construction commenced for the site the applicant, Ben Hood, property owner, identified an error in the site plan, specifically the placement of the building relative to the 50-foot setback on the west property line. The subject property is located in the ED, Employment Center District. The ED provides for the establishment of setback lines at the time of the site plan review process in order to provide property owners the maximum flexibility during the site plan review process. In this case the applicant is requesting that the west setback line be changed to 30 feet.

Adjacent land uses include Victory Wholesale to the east, to the south a large commercial building fronting South Pioneer Drive and occupied by Paper Systems and Trebnick Systems, to the west Total Cable Solutions, Inc., and to the north on the north side of Victory Drive, Hausfeld Classics and a commercial

building owned and occupied by Sunstar along with an access drive to their campus to the north. Adjacent zoning is ED District for all adjacent properties.

### **Staff Recommendation**

City staff recommends approval of the revision to the approved site plan subject to the applicant continuing to work with City staff on comments identified as part of the August 18, 2021 Planning Commission approval.

### **Discussion:**

Mr. Ben Hood from B & E Plumbing was in attendance to discuss his revised site plan.

Mr. Boron referred to the staff comments explaining that once construction commenced for the site the property owner identified an error in the site plan, specifically the placement of the building relative to the 50-foot setback on the west property line. This revision is simply to re-establish setbacks for the proposed building.

Mr. Hood noted there was a mistake made by the civil engineer and this revision will correct the setback issue.

Mr. Boron noted that the current 30 Ft. setbacks are acceptable.

Ms. Iverson asked for a motion to approve the revision to the approved site plan for 465 Victory Drive, B & E Plumbing.

*Mr. Pearson motioned to approve. Mr. Harding seconded the motion.*

**Vote:** Davis, yes; Sillies, yes; Dimmitt, yes; Pearson, yes; Iverson, yes; Harding, yes; Thompson, yes (7-0)

- A. Preliminary Review, Final Development Plan, 765 West Central Avenue (SR 73), accessory building for LaComedia Dinner Theater

### **Background Information**

This agenda item was filed by App Architecture, Englewood, on behalf of LaComedia Dinner Theater, property and business owner, located at 765 West Central Avenue (SR 73). The applicant is proposing the construction of a two-story, 16,200-square foot accessory building on the southeast corner of the 5.04-acre site. The building will be used to house costumes in a climate controlled environment and provide for other theater storage.

The dinner theater has been in operation since 1975. There is no record of the theater's original approval; that predated the City's incorporation in 1987 and our present Building Department (plans were reviewed by Warren County prior to that date). The property was rezoned to PUD, Planned Unit Development, in September 1997 to provide flexibility to the property owner to develop the property without great flexibility in light of setbacks. No record of the setback has been found by City staff. In 1999 the Planning Commission approved plans for the expansion of the theater building, and later the construction of a dormitory.

The proposed accessory building will be located in the vicinity of two accessory buildings that will be removed. The proposed building will be as close as 6 feet from the east property line and 15 feet from the south property line. These figures are similar to other buildings on the site including the main theater building relative to the east property line (less than 5 feet), and the dormitory relative to the south property line (15 feet).

Adjacent land uses include the recently completed McDonald's at 775 West Central Avenue to the northwest; on the north side of West Central Avenue the two shopping centers owned by Edwards Furniture on either side of Tahlequah Trail, the shopping center at 625-725 West Central Avenue that includes the former and newly renovated Kroger; and to the south, fronting and with access from Pleasant Valley Drive, a number of commercial and light industrial buildings including VST (650 Pleasant Valley Drive).

### Staff Comments

City Staff has the following comments regarding the proposed accessory structure:

1. Relocate the existing private utilities from underneath the proposed building, for safety and maintenance reasons.
2. Provide the setback from the attached external stairs to the lot lines.
3. Provide a 10-foot sanitary sewer easement along the rear property line.
4. Provide water and sewer lateral information. Water to be minimum 1" K copper with remote reader inside of building and sanitary sewer lateral to be min 6" SDR 35 or SCH 40 material at 2% slope minimum.
5. Conduct a field survey to verify all utilities, elevations and contours. Elevations and contours shall be based upon USGS datum and identify the benchmark utilized.
6. Provide the pervious/impervious change on the site due to the new building and associated construction. Storm water on the additional impervious areas to be detained.
7. The Clearcreek Fire District has no comments at this time.

### Discussion:

Mr. Tim Bement, with App Architects, and Mr. Dave Gabert, M & M Construction were present to discuss their application for the accessory structure.

Mr. Boron referred to the staff comments explaining that the applicant is proposing the construction of a two-story, 16,200-square foot accessory building on the southeast corner of the 5.04-acre site. The building will be used to house costumes in a climate controlled environment and provide for other theater storage.

Mr. Bement stated they have reviewed the staff comments and have no issues with them

Mr. Pearson felt this was a great opportunity and a huge asset to the area to store this priceless collection of civil war era costumes.

Ms. Iverson clarified this would be voted on at the January meeting.

Mr. Boron explained that the applicant will be working with the staff on some engineering questions and if those are resolved, will come back for a vote in January.

Mr. Dudas added that staff will be working on matching the property lines with Warren County records and will need to determine if the dedication of right-of-way will be needed.

M. Bement noted that they have an engineer working on these issues and hope to resolve it by the January or February meeting.

#### **IV. Guest Comments**

There were none.

#### **IV. Planning Commission and Staff Comments**

Mr. Boron stated that the new schedule proposes the January meeting to be the third Wednesday, rather than the second, and also changing the Friday deadlines to noon rather than the end of business. The next Planning Commission meeting is scheduled for January 19, 2022 and the deadline is Friday, December 27, 2021.

#### **Adjournment**

*Mr. Harding motioned to adjourn the December 8, 2021 Planning Commission Meeting at 6:20 pm  
Mr. Davis seconded the motion.*

**Vote: Sillies, yes; Dimmitt, yes; Pearson, yes; Harding, yes; Davis, yes; Thompson, yes (6-0)**

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Becky Iverson, Planning Commission Chairperson

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Dan Boron, Planning Consultant

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Ann Burns, Planning Commission Secretary